

Committee of Adjustment

City of Ottawa | Ville d'Ottawa  
Comité de dérogation

January 15, 2023

Re: 7 Starwood Road, Ottawa, Ontario

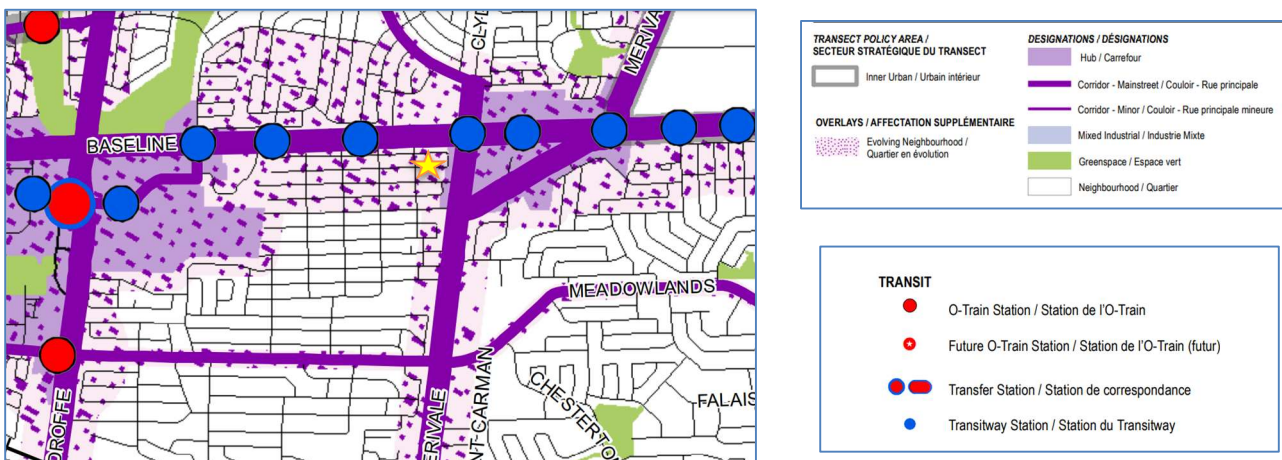
The following information is provided in support of the application for a minor variance to the Committee of Adjustment to allow for the construction of a new detached dwelling unit at 7 Starwood Rd., Ottawa, ON. The property is located in the St. Claire Gardens (City View) neighborhood, in very close proximity to Clyde Ave., Merivale Rd. and Baseline Rd., in the Outer Urban Transect (College Ward 8) of the City of Ottawa.

1. The Site

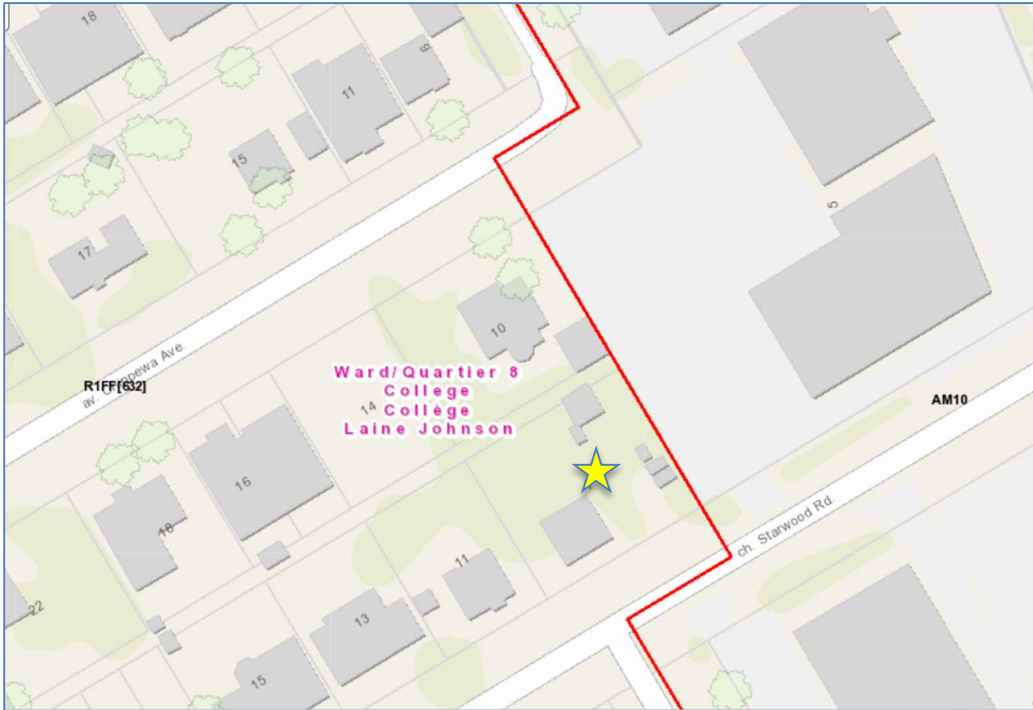
The subject property at 7 Starwood is 842.5m<sup>2</sup> in area and has 30.67m (100 ft) of frontage on Starwood Rd. The subject property is Lots 1889, 1890, 1891, and 1892 on Registered Plan 375. It is located in Ward 8 (College Ward) in the Outer Urban Transect in the City of Ottawa Official Plan (Schedule B3). Within the Outer Urban Transect, the property is designated with a neighborhood and an ‘evolving neighborhood’ overlay. There is no Secondary Plan for this area. It is zoned under By Law 2008-250, Residential First Density, Exception Zone 632 (R1FF [632]). The properties along Clyde Ave. immediately adjacent are zoned Arterial Main Street, Subzone 10 (AM10).

One detached home exists on the property, including a large driveway, a metal shed, and a wooden garage. The front and back yards are landscaped, and several trees exist on the property. A wood privacy fence exists on the adjacent commercial property to the east.

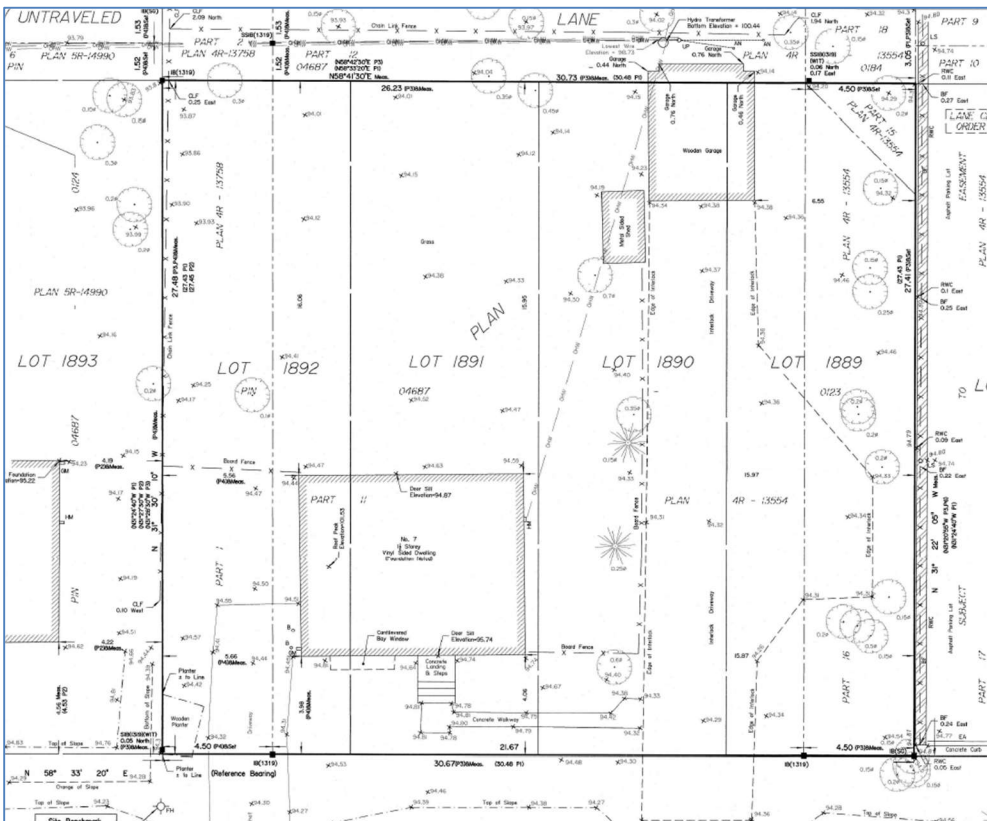
The property is located on a local street (Starwood Rd.) immediately adjacent to the commercial properties fronting on to Clyde Ave., which is classified as an Arterial Roadway on Schedule E (Urban Road Network). The property is also near Baseline Rd., which is also classified as an Arterial Roadway on Schedule E (Urban Road Network). The property is within walking distance (less than 500m) to a variety of uses and amenities on the arterial main street of Clyde Ave. and Merivale Rd. immediately to the east. The west side of Clyde Ave. immediately adjacent to this property is designated as a Hub. The site is served by existing local transit on both Clyde Ave. and Baseline Rd. As indicated on Schedule D (Rapid Transit Network), the subject property is within 500 metres of two future Bus Rapid Transit stations along Baseline Road, which will connect to the future LRT station at Algonquin College.



City of Ottawa Official Plan – Outer Urban Transect, Land Use Designation ‘Neighborhood’, with ‘Evolving Neighborhood Overlay’ (Schedule B3), immediately adjacent Hub and Main Street corridor.

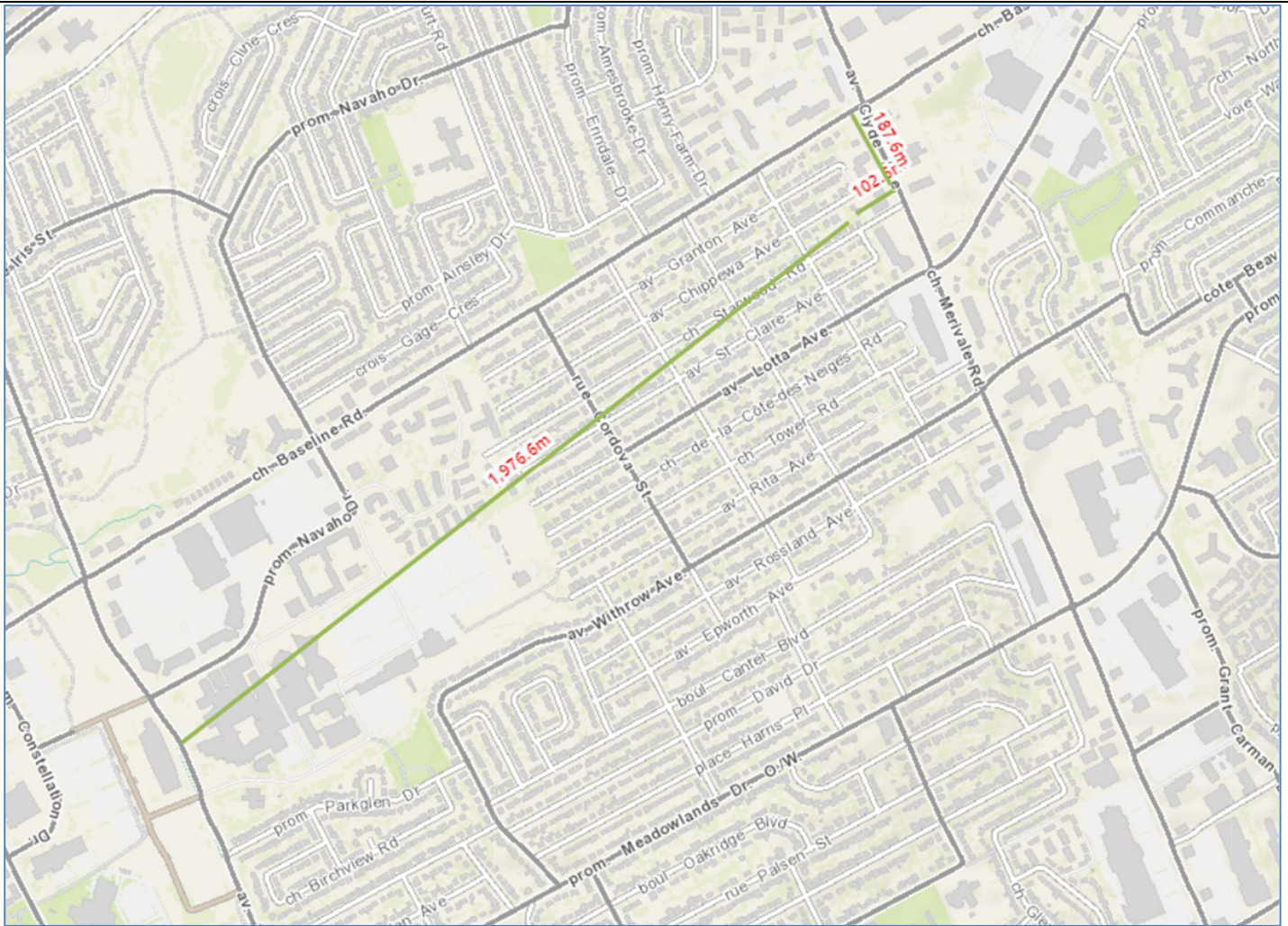


GeoOttawa, showing R1FF [632] zone and adjacent AM10 zone.



Lots 1889-1892, Registered Plan 375 (City of Ottawa) (Surveyors Real Property Report, AOV, November 2023)





*Amenities less than 200m along Clyde Ave. (restaurants, retail, medical services) and Bus Rapid Transit within 200 – 300m. Less than 2km to Algonquin LRT station.*

Immediately to the west and north of the subject site the neighborhood is characterized as a stable low-rise residential neighborhood, with traditionally very large, serviced lots (100’ frontage) with moderately sized homes (1000 – 1800 sq ft). In the past 10 years the neighborhood has been characterized by a significant amount of re-generation and renewal with the construction of many much larger detached homes and more recently semi-detached homes on 50’ lots (1800 – 3000 sq ft). In recent history, most new construction has been done on 50’ lots through minor variances to the zoning.





*7 Starwood Rd: Facing north toward subject site, showing the existing dwelling and driveway access to rear yard parking.*



*Facing south from the subject site toward St. Clair Gardens Rd. and the rear of the commercial plaza.*



*Entrance to Starwood at Clyde Ave., immediately adjacent to 7 Starwood Rd.*



*Facing west from subject site on Starwood Rd.*



*39 Starwood Rd: 2021 construction, 45' wide lot, 4100 sq ft. Double driveway, attached garage.*





14 Chippewa: 2 detached homes on approx. 50' frontage lots. One with front yard parking, one with rear yard parking.



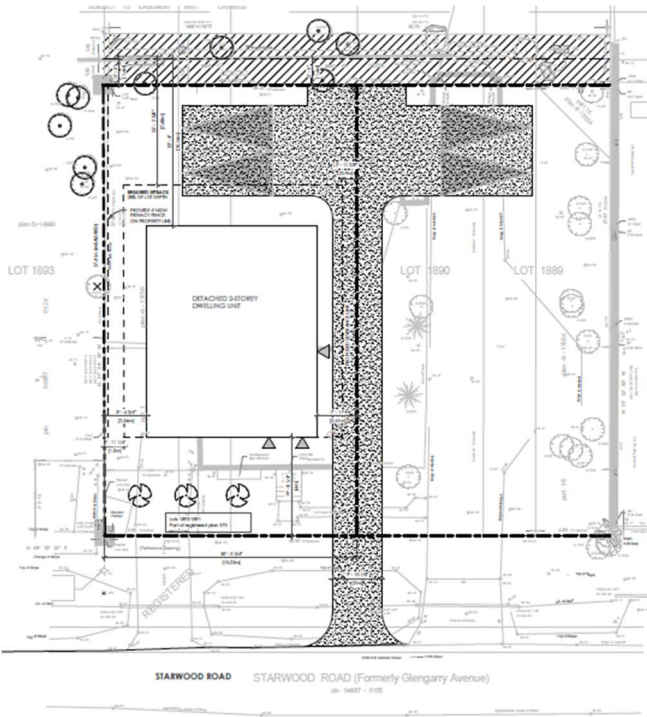
37 Lotta: Example showing variety of housing size in existing neighborhood and driveway access to rear yard parking.



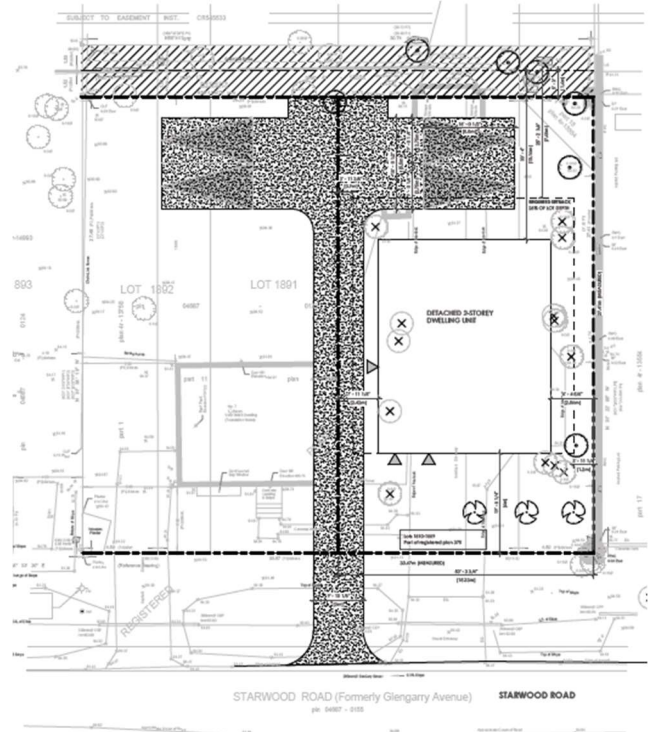


## 2. The Proposal

The applicant is proposing to demolish the existing 2-storey detached dwelling, garage and shed and to construct 2 new detached 2-storey dwellings on the property. Each of the dwelling units will contain 3 units, with on-site surface parking for 2 vehicles and a shared driveway access from the front to the rear yard parking. The site will also include landscaping, tree planting, privacy screen fencing and private amenity areas.



Site Plans for Lot 1889 and 1890



Site Plans for Lot 1891 and 1892





Front Elevation - 7 Starwood Rd. (west)

Front Elevation - 7 Starwood Rd. (east)

The proposed detached homes conform with all required setbacks in the zoning by-law (including the minimum lot coverage) for the site (R1FF [632]). The frontage does not conform with the minimum lot width requirement for 19.5m (64') or minimum lot area 600m<sup>2</sup> (6,458 sq ft). Therefore, Minor Variances to minimum lot area and minimum frontage is required to permit the construction of the two dwelling units.

The impact of allowing the variances will permit the construction of two smaller dwelling units (approximately 1400 sq ft building envelope footprint), whereas one very large dwelling unit could be built as-of-right on the property today (>4300 sq ft building footprint), with a larger overall coverage and development footprint than what is proposed.

The following minor variances are required (under 2 separate applications) to bring the lots in to conformity with ZBL 2008-250 Table 156A (R1FF, Column II and III):

Proposed Lot	Minor Variances Requested
<u>Lots 1889 + 1890</u>	(A) Column II: Minimum Lot Width: <del>19.5 m</del> <b>15.3m</b> (B) Column III: Minimum Lot Area: <del>600 m<sup>2</sup></del> <b>419.9m<sup>2</sup></b>
<u>Lots 1891 + 1892</u>	(A) Column II: Minimum Lot Width: <del>19.5 m</del> <b>15.3m</b> (B) Column III: Minimum Lot Area: <del>600 m<sup>2</sup></del> <b>421.5m<sup>2</sup></b>



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### 3. Planning Policy Conformance

#### **City of Ottawa - Official Plan (By Law: 2021-386)**

The subject property is in the City of Ottawa, Outer Urban Transect Area, designated Neighborhood, and is a lot on a plan of Subdivision. Section 5, Table 7 - *Minimum and Maximum Height Overview Based on Official Plan Policy*, characterizes the heights for Neighborhoods in the Outer Urban Transect as Low-rise, specifying “no minimum [height] and generally, zoning will permit at least 3 storeys but no more than 4 storeys.” Whereas Table 7 notes that the immediately adjacent Hub designation allows for Low-rise, Mid-rise and High-rise building, specifying a “minimum 3 storeys and maximum 40 storeys”. This property being located at the edge condition of the Evolving Neighborhood overlay makes it an excellent candidate to allow for slightly more density in order to transition from the stable low-rise neighborhood to the increased density in the Hub area, with a minimum of 3 storeys.

Section 5.3 – *Outer Urban Transect* outlines the intentions of the Outer Transect Neighborhood, “...the evolution of existing neighbourhoods is expected to be extremely gradual within a fundamentally suburban pattern, with more substantial changes confined to a set of strategic locations, unless the site is close to new Hubs or Corridors that are serviced by rapid transit.” The subject property is located adjacent to a hub and near rapid transit.

This neighborhood would be described as Classic “Post War”, and the City of Ottawa’s Official Plan describes the intent to move toward 15-minute neighborhoods.

Section 5.3.4 – *Provide direction to Neighbourhoods located within the Outer Urban Transect* outlines the city’s intention for the area:

1) *Neighbourhoods located in the Outer Urban area shall accommodate residential growth to meet the Growth Management Strategy as outlined in Section 3. The Zoning By-law shall implement development standards that transition away from a suburban model and move towards urban built forms as described in Table 6 as applicable and that:*

- a) Allows and supports a wide variety of housing types with a focus on lower density missing-middle housing which generally reflects the existing built form context of the neighbourhood, which may include new housing types that are currently not contemplated in this Plan;*
- b) The application, as appropriate, of Zoning By-law development standards to be applied as one lot for zoning purposes to support missing middle housing;*
- c) Generally provides for up to 3 storeys height permission, and where appropriate 4 storeys height permission to allow for ground oriented higher-density Low-rise residential development;*
- d) Provides an emphasis on regulating the maximum built form envelope that frames the public right of way; and*
- e) In appropriate locations, to support the production of missing middle housing, prohibit lower-density typologies.*

Section 3.0 – *Growth Management Framework* in the Official Plan outlines the city’s Growth Management Framework:

The section identifies that “...most growth will occur within the urban area of the City, with a majority of residential growth to be within the built-up area through intensification, increasing over time during the planning horizon.”

Section 3.0’s policy intents are as follows:

- *To provide an appropriate range and mix of housing that considers the geographic distribution of new dwelling types and/or sizes to 2046;*
- *To provide a transportation network that prioritizes sustainable modes over private vehicles, based on the opportunities for mode shifts presented by each transect area context;*
- *To prioritize the location of residential growth to areas with existing municipal infrastructure, including piped services, rapid transit, neighbourhood facilities and a diversity of commercial services;*
- *To reduce greenhouse gas emissions in the development and building sectors and in the transportation network; and*
- *To establish a growth management framework that maintains a greater amount of population and employment inside the Greenbelt than outside the Greenbelt*

The support for urban intensification and variety of housing choices is clear throughout the Official Plan. For example, Section 4.2 - *Support Intensification* outlines that:

*4.2(3) The vast majority of Residential intensification shall focus within 15- minute neighbourhoods, which are comprised of Hubs, Corridors and lands within the Neighbourhood designations that are adjacent to them.*

*4.2(4) Intensification is permitted in all designations where development is permitted taking into account whether the site has municipal water and sewer services. This Plan supports intensification and the approval of applications for intensification shall be in conformity with transect and overlay policies as applicable.*

*4.2(8) Intensification should occur in a variety of dwelling unit floorspace sizes to provide housing choices. Dwelling sizes are categorized into two broad categories, with a range of floorspaces occurring within each category: a) Small-household dwellings ...; and b) Large-household dwellings*



**A. The Requested Minor Variance Maintains the General Intent and Purpose of the Official Plan:**

Lots 1889+1890 and 1891+1892	Maintains the general intent and purpose of the Official Plan
<p>ZBL 2008-250 Table 156A (R1FF, Column II and III):</p> <p>(A) Column II: Minimum Lot Width: <del>19.5m</del> <b>15.3m</b></p> <p>(B) Column III: Minimum Lot Area: <del>600 m<sup>2</sup></del> <b>419.9m<sup>2</sup></b> / <b>421.5m<sup>2</sup></b></p>	<p>Supports the goals of the Official Plan and is consistent with the objectives 4.0, 4.2, 5.3. Supports the gradual move toward 15-minute neighborhoods. Allows for a more compact form of urban form over time. It allows flexibility in housing choices to adapt to a variety of possible uses.</p> <p>The reduction in minimum lot width and lot area, while maintaining all of the required setbacks and coverage allows for a small increase in the number of dwellings on existing city services.</p>

The proposed addition of a dwelling unit (through a reduced minimum lot frontage and lot size) in the existing Outer Urban Transect area will allow for some moderate infill (intensification) in the urban area. While still maintaining a very large lot frontage (50 ft) and lot area (4800 sq ft) on full municipal services. It has regard for the policies throughout the Official Plan, including 3.0, 4.0, 5.0. The subject sites are also within walking distance of many community amenities and transit. This type of infill development helps to support the development of 15-minute, sustainable communities and maintains the general intent and purpose of the Official Plan.

**The proposed minor variances maintain the general intent and purpose of the City of Ottawa Official Plan.**



**City of Ottawa – Consolidated Zoning By-Law (2008-250)**

The property is zoned R1FF – Residential, First Density, Sub-Zone FF, exception zone 632. The proposal meets all other requirements of the R1FF zone and performance standards.

**B. The proposed Minor Variance maintains the general intent and purpose of the zoning by-law:**

Lots 1889+1890 and 1891+1892	Maintains the general intent and purpose of the zoning by-law
<p>ZBL 2008-250 Table 156A (R1FF, Column II and III):</p> <p>(A) Column II: Minimum Lot Width: <del>19.5m</del> <b>15.3m</b></p> <p>(B) Column III: Minimum Lot Area: <del>600 m2</del> <b>419.9m2</b> / <b>421.5m2</b></p>	<p>The reduction in minimum lot width and lot area, while maintaining all of the required setbacks and coverage allows for a small increase in the number of dwellings on existing city services. However, it fully maintains the general intent and purpose of the R1 ZBL.</p> <p>The purpose of the R1- Residential First Density Zone is to: (1) restrict the building form to detached dwellings in areas designated as General Urban Area in the Official Plan; (2) allow a number of other residential uses to provide additional housing choices within detached dwelling residential areas; (3) permit ancillary uses to the principal residential use to allow residents to work at home; (4) regulate development in a manner that is compatible with existing land use patterns so that the detached dwelling, residential character of a neighbourhood is maintained or enhanced; and (5) permit different development standards, identified in the Z subzone, primarily for areas designated as Developing Communities, which promote efficient land use and compact form while showcasing newer design approaches.</p>

The proposed variances continue to restrict the building form to detached dwellings, while allowing additional housing choices, supporting residents to work from home, and regulating development in a manner that is compatible with existing land use patterns, so that the detached dwelling residential character of the neighborhood is maintained.

There are many examples throughout the neighborhood of detached housing in the 1400 sq ft footprint size, on a 50’ frontage lot.

**The proposed minor variances maintain the general intent and purpose of the zoning by-law.**



**C. Variances are desirable for the appropriate development of the property:**

<p><b>Lots 1889+1890 and 1891+1892</b></p>	<p><b>Variances are desirable for the appropriate development of the property</b></p>
<p>ZBL 2008-250 Table 156A (R1FF, Column II and III):</p> <p>(A) Column II: Minimum Lot Width: <del>19.5m</del> <b>15.3m</b></p> <p>(B) Column III: Minimum Lot Area: <del>600 m<sup>2</sup></del> <b>419.9m<sup>2</sup> / 421.5m<sup>2</sup></b></p>	<p>The reduction in minimum lot width and lot area, while maintaining all of the required setbacks and coverage allows for a small increase in the number of dwellings on existing city services.</p> <p>Further, it allows for a more appropriate size of building on the lot of approximately 1500 sq ft, from what is already allowed as-of-right on the property (ex. 4300 sq ft building envelope permitted under the ZBL).</p>

The proposed detached dwellings will allow for appropriate infill in this neighborhood that enhances and provides new opportunities for a different housing form within the urban area on the existing city roads and services. The edge condition of these lots immediately adjacent to the arterial main street of Clyde Ave. provides a unique opportunity where increased density is desirable. The site plan provided demonstrates that the minimum required zoning setbacks can be met (and exceeded), minimum parking requirements can be exceeded, and landscaping and tree planting can also be provided. Privacy screen fencing adjacent to the residential properties to the west is also proposed as part of the landscaping design with the building permit process.

**Variances are desirable for the appropriate development of the property.**



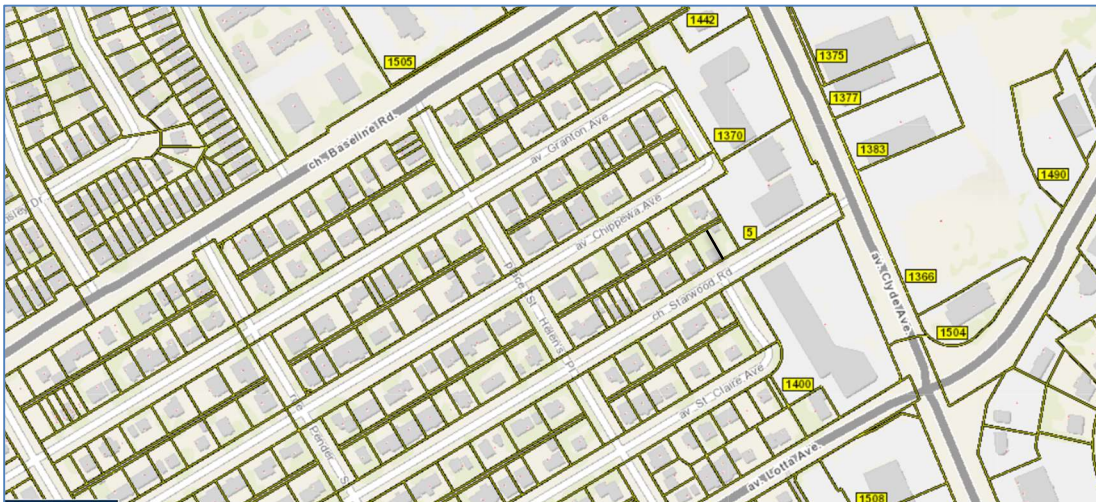


**D. The proposed variances are minor in nature:**

The proposed variances are minor in nature. The minor variances allow for the continued evolution of the neighborhood for housing choices and allows for continued gentle intensification. This type of development is anticipated through the policies of the Official Plan. The impact of the proposed on the lotting fabric of the neighborhood is minimal and in keeping with many examples throughout the area. The size and style of housing is very similar to many examples on Starwood Rd. and Chippewa Ave. existing today.



*Existing lotting fabric (December 2023)*

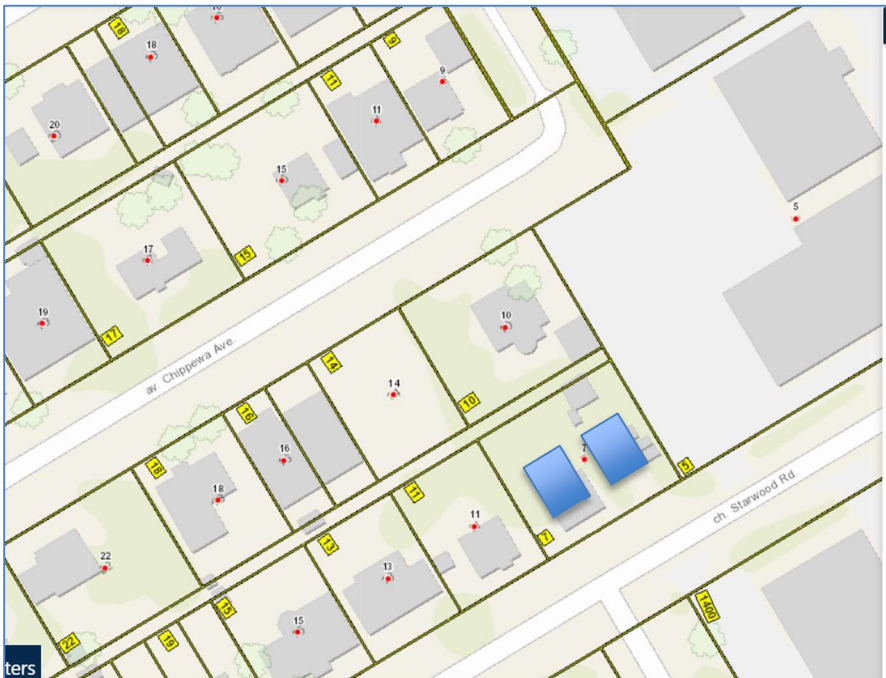


*Proposed lotting fabric with minor variance.*





Existing building sizes (December 2023)



Proposed building sizes (with minor variances permitted)

The proposed variances are minor in nature.

## 4. Conclusion

We request the Committee of Adjustment to approve the **minor variances** to Zoning By-law 2008-250 under **Section 45** of the Planning Act. ZBLA **2008-250 Table 156A (Column II and III) to be modified as outlined below:**

Proposed Lot	Minor Variances Requested
<u>Lots 1889 + 1890</u>	(C) Column II: Minimum Lot Width: <del>19.5 m</del> <b>15.3m</b> (D) Column III: Minimum Lot Area: <del>600 m<sup>2</sup></del> <b>419.9m<sup>2</sup></b>
<u>Lots 1891 + 1892</u>	(C) Column II: Minimum Lot Width: <del>19.5 m</del> <b>15.3m</b> (D) Column III: Minimum Lot Area: <del>600 m<sup>2</sup></del> <b>421.5m<sup>2</sup></b>

The variances are minor in nature, the variances are desirable for the appropriate development of the property, the general purpose and intent of the Zoning By-Law is maintained, and the general intent and purpose of the Official Plan is maintained.

Prepared by:



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