

**Subject: Zoning By-law Amendment-City-Owned Lands for Housing  
Developments**

**File Number: ACS2024-PDB-PSX-0038**

**Report to Planning and Housing Committee on 5 November 2024**

**and Council 13 November 2024**

**Submitted on October 30, 2024 by Derrick Moodie, Director, Planning Services,  
Planning, Development and Building Services**

**Contact Person: Wendy Tse, Planner III (A), Development Review All Wards**

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**Ward: City Wide**

**Objet: Modification du *Règlement de zonage*: Terrains appartenant à la Ville  
pour la construction de logements**

**Dossier : ACS2024-PDB-PSX-0038**

**Rapport au Comité de la planification et du logement**

**le 5 novembre 2024**

**et au Conseil le 13 novembre 2024**

**Soumis le 30 octobre 2024 par Derrick Moodie, Directeur, Services de la  
planification, Direction générale des services de la planification, de  
l'aménagement et du bâtiment**

**Personne ressource : Wendy Tse, urbaniste (i), Examen des demandes  
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**Quartier: City Wide**

**REPORT RECOMMENDATIONS**

- 1. That Planning and Housing Committee recommend Council approve an amendment to Zoning By-law 2008-250 for 2 Pretty Street, as shown in Document 1, from L1 to R4M[xxx1] with exceptions to enable residential development, on a parcel owned by the City of Ottawa, as detailed in Document 2.**
- 2. That Planning and Housing Committee recommend Council approve an amendment to Zoning By-law 2008-250 for part of 40 Beechcliffe Street as shown in Document 3, from R3Z[937] and R2M to R4M[xxx2] with exceptions to enable residential development, on a vacant parcel owned by the City of Ottawa, as detailed in Document 4.**
- 3. That Planning and Housing Committee recommend Council approve an amendment to Zoning By-law 2008-250 for 261A Hinchey Avenue, as shown in Document 5, from R4UB to R4UB[xxx3] with exceptions to enable residential development, on a vacant parcel owned by the City of Ottawa, as detailed in Document 6.**
- 4. That Planning and Housing Committee recommend Council approve an amendment to Zoning By-law 2008-250 for 185 Hawthorne Avenue, as shown in Document 7, from R3P to R4UB[xxx4] with exceptions to enable residential development, on a vacant parcel owned by the City of Ottawa, as detailed in Document 8.**
- 5. That Planning and Housing Committee recommend Council approve an amendment to Zoning By-law 2008-250 for 2548 Cl  roux Crescent, as shown in Document 9, from R2N to R4M[xxx5] with exceptions to enable residential development, on a vacant parcel owned by the City of Ottawa, as detailed in Document 10.**
- 6. That Planning and Housing Committee recommend Council approve an amendment to Zoning By-law 2008-250 for 2060 Lanthier Drive, as shown in Document 11, from IG7 H(21) and IG[1608] H(21) to GM H(21) [xxx6] with exceptions to enable mixed use development, on a vacant parcel owned by the City of Ottawa, as detailed in Document 12.**
- 7. That Planning and Housing Committee approve the Consultation Details Section of this report be included as part of the ‘brief explanation’ in the Summary of Written and Oral Public Submissions, to be prepared by the**

Office of the City Clerk and submitted to Council in the report titled, “Summary of Oral and Written Public Submissions for Items Subject to *the Planning Act* ‘Explanation Requirements’ at the City Council Meeting of November 13 subject to submissions received between the publication of this report and the time of Council’s decision.

## RECOMMANDATIONS DU RAPPORT

1. Que le Comité de la planification et du logement recommande au Conseil municipal d’approuver la modification à apporter au *Règlement de zonage* (n° 2008-250) pour le 2, rue Pretty, selon les modalités reproduites dans la pièce 1, pour passer de la zone L1 à la zone R4M[xxx1], en apportant des exceptions pour permettre d’aménager des logements sur une parcelle appartenant à la Ville d’Ottawa, selon les modalités précisées dans la pièce 2.
2. Que le Comité de la planification et du logement recommande au Conseil municipal d’approuver la modification à apporter au *Règlement de zonage* (n° 2008-250) pour la partie du 40, rue Beechcliffe représentée dans la pièce 3, afin de passer de la zone R3Z[937] et de la zone R2M à la zone R4M[xxx2], en apportant des exceptions pour permettre d’aménager des logements sur une parcelle inoccupée appartenant à la Ville d’Ottawa, selon les modalités précisées dans la pièce 4.
3. Que le Comité de la planification et du logement recommande au Conseil municipal d’approuver la modification à apporter au *Règlement de zonage* (n° 2008-250) pour le 261A, avenue Hinchey, selon les modalités précisées dans la pièce 5, afin de passer de la zone R4UB à la zone R4UB[xxx3], en apportant des exceptions pour permettre d’aménager des logements sur une parcelle inoccupée appartenant à la Ville d’Ottawa, selon les modalités précisées dans la pièce 6.
4. Que le Comité de la planification et du logement recommande au Conseil municipal d’approuver la modification à apporter au *Règlement de zonage* (n° 2008-250) pour le 185, avenue Hawthorne, selon les modalités indiquées dans la pièce 7, pour passer de la zone R3P à la zone R4UB[xxx4], en apportant des exceptions pour permettre d’aménager des logements sur une parcelle inoccupée appartenant à la Ville d’Ottawa, selon les modalités précisées dans la pièce 8.

5. **Que le Comité de la planification et du logement recommande au Conseil municipal d'approuver la modification à apporter au *Règlement de zonage* (n° 2008-250) pour le 2548, croissant Cléroux, selon les modalités indiquées dans la pièce 9, pour passer de la zone R2N à la zone R4M[xxx5], en apportant des exceptions pour permettre d'aménager des logements sur une parcelle inoccupée appartenant à la Ville d'Ottawa, selon les modalités précisées dans la pièce 10.**
6. **Que le Comité de la planification et du logement recommande au Conseil municipal d'approuver la modification à apporter au *Règlement de zonage* (n° 2008-250) pour le 2060, promenade Lanthier, selon les modalités indiquées dans la pièce 11, pour passer de la zone IG7 H(21) et de la zone IG[1608] H(21) à la zone GM H(21) [xxx6], en apportant des exceptions pour permettre d'aménager des immeubles polyvalents sur une parcelle inoccupée appartenant à la Ville d'Ottawa, selon les modalités précisées dans la pièce 12.**
7. **Que le Comité de l'urbanisme approuve l'intégration de la section Détails de la consultation du rapport dans le cadre de la « brève explication » du Résumé des mémoires déposés par écrit et de vive voix, à rédiger par le Bureau du greffier municipal et à soumettre au Conseil municipal dans le rapport intitulé « Résumé des mémoires déposés par écrit et de vive voix par le public sur les questions assujetties aux "explications obligatoires" de la *Loi sur l'aménagement du territoire* à la réunion tenue par le Conseil municipal le [date de la réunion du Conseil à laquelle la question sera débattue] », sous réserve des mémoires qui seront déposés entre la publication de ce rapport et la date à laquelle le Conseil municipal rendra sa décision.**

## **EXECUTIVE SUMMARY**

### **Staff Recommendation**

The proposed development of the subject sites for housing is in keeping with the direction of the Official Plan. The Zoning By-law amendments will permit the advancement of a number of housing initiatives by the City of Ottawa. Given the applicable Official Plan policies and compatibility of the uses to their areas, the Zoning By-law Amendments are recommended for approval.

Planning staff recommend approval of the Zoning By-law amendments for the following sites to facilitate residential development, which will include new affordable housing:

1. 2 Pretty Street
2. part of 40 Beechcliffe Street
3. 261A Hinchey Avenue
4. 185 Hawthorne Avenue
5. 2548 Cl  roux Crescent
6. 2060 Lanthier Drive

The sites are recommended to be rezoned with exceptions for the elimination of minimum parking requirements and to provide more flexibility within their zone provisions to make the sites market ready and accelerate housing development in accordance with the City's housing supply targets. The modifications will allow for flexibility in housing type and form to maximize development while still ensuring the scale of development is appropriate.

### **Applicable Policy**

The proposed zone amendments align with Official Plan Section 4.2 policy, to prioritize affordable housing and add housing stock that will achieve targets the City has committed to through the Municipal Housing Pledge, the Ontario-Ottawa Agreement, the Housing Accelerator Fund and the City's 10-Year Housing and Homelessness Plan. Additionally, staff will use a toolkit of incentives, including alternative development standards and application processing priority to align with this policy.

### **Other Matters**

Upon zoning approval, the City of Ottawa intends to use the disposals process in accordance with the City policies and procedures to select a development proponent for each site. This process will define the development of each site, including requirements for the provision of affordable housing and number of units. Developments that meet the threshold for site plan control approval will be required to submit plans and studies to support the planning application. The City may also require site-specific studies, at its discretion, as part of the disposals process.

### **Public Consultation/Input**

Pursuant to Section 11.4, Policy 3 of the Official Plan, alternative notification for a Zoning By-law amendment was undertaken.

## BACKGROUND

The proposed Zoning By-Law Amendments are to amend the existing zoning for the following sites:

1. 2 Pretty Street
2. part of 40 Beechcliffe Street
3. 261A Hinchey Avenue
4. 185 Hawthorne Avenue
5. 2548 Cléroux Crescent
6. 2060 Lanthier Drive

The amendments will allow for residential development on the lands in order to increase the housing supply in Ottawa and to support the City's targets for delivery of all types of housing, including affordable housing, in accordance with the [2031 Municipal Housing Pledge](#), [the Ontario-Ottawa Agreement](#) and the [Housing Accelerator Fund \(HAF\)](#). In total, these six sites will have the potential to accommodate a minimum of 60 residential units.

City Council declared a housing emergency on January 29, 2020 and has approved the Ottawa 10-year Housing and Homelessness Plan 2020-2030, which has a key objective to increase the affordable housing supply.

The City of Ottawa intends to undertake a disposal process to select a suitable nonprofit housing provider or proponent in accordance or as required by the City through the disposals process. This process will identify the following:

- the development and site requirements for each site
- the target number of units, as well as affordable housing units, to be developed
- the development timeline

The approximate number of potential units are provided for the various properties, with the exception of 2060 Lanthier, but will be subject to discussions with the proponent. Additional studies and plans will be provided for any development that will be subject to site plan control approval or as required by the City through the disposals process. For 2060 Lanthier Drive, the Request for Offers is currently underway.

The purpose of the Zoning By-law amendments is to align with the policy direction of the Official Plan, ensure the sites are market ready and to provide flexibility for future

development within the context of the sites in order to support the City's housing targets.

## Résumé

### Recommandation du personnel

Le projet proposé pour l'aménagement des sites visés afin de construire des logements cadre avec l'orientation du Plan officiel. Les modifications à apporter au *Règlement de zonage* permettront de faire avancer un certain nombre d'initiatives de construction de logements de la Ville d'Ottawa. Compte tenu des politiques applicables du Plan officiel et de la compatibilité des aménagements avec les secteurs en cause, on recommande d'approuver les modifications à apporter au *Règlement de zonage*.

Le personnel des Services de planification recommande d'approuver les modifications à apporter au *Règlement de zonage* pour les sites suivants afin de permettre d'aménager des habitations, dont de nouveaux logements abordables :

1. le 2, rue Pretty;
2. une partie du 40, rue Beechcliffe;
3. le 261A, avenue Hinchey;
4. le 185, avenue Hawthorne;
5. le 2548, croissant Cléroux;
6. le 2060, promenade Lanthier.

On recommande de rezoner les sites en prévoyant des exceptions pour éliminer le nombre minimum de places de stationnement obligatoire et pour offrir une plus grande souplesse dans les dispositions du zonage pour que les sites soient prêts à bâtir et pour accélérer l'aménagement de logements conformément aux cibles fixées par la Ville pour l'offre de logements. Ces modifications apporteront la souplesse voulue dans le type et la forme de logements afin de maximiser ce projet d'aménagement tout en veillant à ce que l'envergure des travaux d'aménagement soit appropriée.

### Politiques applicables

Les modifications que l'on propose d'apporter au *Règlement de zonage* concordent avec la politique de la sous-section 4.2 du Plan officiel, pour prioriser le logement abordable et pour augmenter le parc de logements afin d'atteindre les cibles que la Ville

s'est engagée à atteindre dans le cadre de la Garantie municipale du logement, de l'Accord Ontario-Ottawa, du Fonds pour accélérer la construction de logements et du Plan décennal de logement et de lutte contre l'itinérance de la Ville. En outre, le personnel fera appel à une trousse de programmes d'incitation, dont les différentes normes d'aménagement et l'attribution des priorités dans le traitement des demandes pour respecter cette politique.

### **Autres questions**

Dès l'approbation du zonage, la Ville d'Ottawa a l'intention de faire appel au processus d'aliénation, conformément à ses politiques et procédures, afin de sélectionner le promoteur de l'aménagement de chaque site. Ce processus permettra de définir l'aménagement de chaque site, dont les exigences à respecter pour offrir des logements abordables et le nombre de logements à construire. Dans les travaux d'aménagement qui atteignent le seuil de l'approbation de la réglementation du plan d'implantation, il faudra soumettre des plans et des études pour justifier la demande de planification. La Ville pourra aussi, à sa discrétion, exiger des études propres au site dans le cadre du processus d'aliénation.

### **Consultation et commentaires du public**

Conformément à la politique 3) de la sous-section 11.4 du Plan officiel, nous avons lancé un autre avis pour une modification à apporter au *Règlement de zonage*.

### **CONTEXTE**

Les modifications que l'on propose d'apporter au *Règlement de zonage* visent à changer le zonage existant des sites suivants :

1. le 2, rue Pretty;
2. une partie du 40, rue Beechcliffe;
3. le 261A, avenue Hinchey;
4. le 185, avenue Hawthorne;
5. le 2548, croissant Cléroux;
6. le 2060, promenade Lanthier.

Ces modifications permettront d'aménager des logements sur ces terrains afin d'accroître l'offre de logements à Ottawa et d'étayer les cibles de la Ville dans la



réalisation de tous les types de logements, dont le logement abordable, conformément à la [Garantie municipale du logement 2031](#), à l'[Accord Ontario-Ottawa](#) et aux exigences du [Fonds pour accélérer la construction de logements \(FACL\)](#). Dans l'ensemble, ces six sites permettront d'aménager au moins 60 logements.

Le Conseil municipal a déclaré l'état d'urgence du logement le 29 janvier 2020 et a approuvé le [Plan décennal de logement et de lutte contre l'itinérance 2020-2030](#), dont l'objectif consiste essentiellement à accroître l'offre de logements abordables.

La Ville d'Ottawa a l'intention de mener un processus d'aliénation afin de sélectionner un organisme fournisseur de logements à but non lucratif compétent ou un promoteur immobilier conformément à ses exigences dans le cadre de ce processus, qui permettra :

- de recenser les exigences de l'aménagement et de chacun des sites;
- de fixer le nombre cible de logements, dont les logements abordables, à aménager;
- d'établir le calendrier d'aménagement.

Nous avons prévu le nombre approximatif de logements potentiels pour les différentes propriétés, à l'exception du 2060, promenade Lanthier. Nous tiendrons toutefois des discussions avec le promoteur. Il faudra établir d'autres études et plans pour les travaux d'aménagement qui feront l'objet de l'approbation de la réglementation du plan d'implantation ou qui répondront aux exigences de la Ville dans le cadre du processus d'aliénation. Nous avons déjà lancé la Demande d'offres pour le 2060, promenade Lanthier.

L'objectif des modifications du *Règlement de zonage* consiste à respecter la politique-cadre du Plan officiel, à s'assurer que les sites sont prêts à aménager et à offrir une marge de manœuvre pour les travaux d'aménagement projetés dans le contexte des sites afin de permettre d'atteindre les cibles de logements de la Ville.

## **DISCUSSION**

### **Public consultation**

Pursuant to Section 11.4, Policy 3 of the Official Plan, alternative notification for a zoning by-law amendment was undertaken. Bilingual signs were posted on the individual sites. Newspaper ads were published in one English and one French

language daily newspaper on October 12, 2024, prior to the scheduled public meeting. Notice will also be given on Ottawa.ca through the meeting agenda.

For this proposal's consultation details, see Document 3 of this report.

## **2 Pretty Street (Document 1)**

The subject site is located adjacent to Pretty Street to the northeast, Orville Street to the northwest and Meach Private to the southwest. It currently contains the former Pretty Street Community Centre which contained one dwelling unit (both of which are now vacant), a surface parking lot and a landscaped space. This site is approximately 100 metres northeast of Stittsville Main Street, which is identified as a Transit Priority Corridor in the Official Plan. Within 200 metres walking distance are the Ralph Street Park and the Village Square Park, as well as multiple commercial uses and services along Main Street. Immediately to the southwest is a four storey stacked townhouse development and to the north, east and south are detached residential dwellings.

Within the Official Plan, the site is located in the Suburban (West) Transect as shown on Schedule A and designated Evolving Neighbourhood in Schedule B5. This site is currently zoned L1 and is proposed to be rezoned to R4M[xxx1].

The proposed amendment will permit townhouses, stacked townhouses or a low-rise apartment up to four storeys in height. The site is anticipated to accommodate up to ten units. The site-specific zoning proposes to amend the minimum lot area, rear yard setback and a number of other development requirements. Minimum resident and visitor parking space rates are proposed to be eliminated.

## **40 Beechcliffe Street (Document 3)**

The subject site is located adjacent to Beechcliffe Street to the west and is set back from the Woodroffe Avenue right-of-way by a depth that ranges from 25.6 metres to approximately 38.5 metres. This area will abut the future Knoxdale LRT Station and Woodroffe Avenue is currently a bus Transitway. This site is currently vacant, with a paved pathway providing a pedestrian connection from the southeast corner of Beechcliffe Street to Knoxdale Road. This site is primarily surrounded by low-rise residential development, with limited small-scale commercial uses facing Woodroffe Avenue. The Nepean Sportsplex is approximately one kilometre to the south along Woodroffe Avenue and Algonquin College and a wide range of surrounding mixed uses are located approximately 1.5 kilometres to the north.

Within the Official Plan, the site is located in the Outer Urban Transect as shown on Schedule A and designated Evolving Neighborhood in Schedule B3. Currently, this site is zoned R3Z[937] with a portion at the south end zoned R2M and are proposed to be rezoned to R4M[xxx2]. A portion on the east side of the property will remain the existing zoning as it will be transferred to the LRT.

Proposed amendments will permit townhouses, stacked townhouses or a low-rise apartment up to four storeys in height. The site is anticipated to accommodate approximately 30 to 40 units. The site-specific zoning proposes to eliminate the minimum lot area, rear yard area requirements and amend a number of other development requirements. Minimum resident and visitor parking space rates are proposed to be eliminated.

### **261A Hinchey Avenue (Document 5)**

The subject site is located at the intersection of Hinchey Avenue and Bullman Street. As this site has been a road allowance, the existing neighbours at 261 and 263 Hinchey Avenue currently use the subject site to access parking on their properties. This site is in a primarily residential neighbourhood, with surrounding low-rise residential development ranging from two to three storeys in height. Within 150 metres to the north is Scott Street, which is lined with a range of mixed uses and there are several local parks in the vicinity. Scott Street is evolving, with a range of new, higher density mixed-use developments approved or constructed recently. Additionally, Tunney's Pasture LRT Station is approximately 600 metres to the northwest.

This site is anticipated to accommodate a small low-rise development, up to three storeys in height. This property will also accommodate driveway access for the adjacent properties to the north and south (261 and 263 Hinchey Avenue respectively), as the subject site has been a road allowance which provided legal access to these properties. As part of the road closure, the City has committed to maintaining an easement for driveway access to rear yard parking for both properties.

Within the Official Plan, the site is located in the Inner Urban Transect as shown on Schedule A and designated Evolving Neighborhood in Schedule B2. It is also designated a Low-Rise Neighbourhood in the Scott Street Secondary Plan. Currently, this site is zoned R4UB and is proposed to be rezoned to R4UB[xxx3].

Due to the constrained site conditions and proximity to amenities, services and the LRT, the minimum resident and visitor parking space rates are proposed to be eliminated and

amenity space is proposed to be provided through balconies, terraces and/or rooftops. This site is anticipated to provide up to approximately four units.

### **185 Hawthorne Avenue (Document 7)**

The subject site is located immediately southwest of Highway 417 and the Lees Avenue eastbound off-ramp. Though the site has frontage on Hawthorne Street, a portion also extends west with approximately 13 metres of frontage on Concord Street South. The site currently contains a short driveway on Hawthorne Street and a landscaped area. This site is surrounded to the west and south by a mix of two to three storey residential dwellings, including detached and semi-detached dwellings, townhouses and multi-unit low-rise apartments. Immediately to the north and east, the site is bounded by Highway 417 and its eastbound off-ramp. The Lees LRT Station on Line 1 is approximately 700 metres to the east. Bus service connecting to the station and beyond runs along Lees Avenue. The site is also approximately 400 metres from Springhurst Park, which provides access to the network of trails and parks along the Ottawa River. The general required setback for development from Highway 417 and the ramp is 14 metres.

Within the Official Plan, the site is located in the Inner Urban Transect as shown on Schedule A and designated Evolving Neighborhood in Schedule B2. The site is also within to the Old Ottawa East Secondary Plan. Currently, this site is zoned R3P and is proposed to be rezoned to R4UB[xxx4].

Proposed amendments will permit townhouses, stacked townhouses or a low-rise apartment up to four storeys in height. The site is anticipated to accommodate up to 10 units. The site-specific zoning proposes to eliminate the minimum lot area, rear yard area requirements and amend a number of other development requirements. Minimum resident and visitor parking space rates are proposed to be eliminated.

### **2548 Cléroux Crescent (Document 9)**

The subject site located in Blackburn Hamlet, with frontage on Cléroux Crescent and is currently vacant with a short existing driveway. The Blackburn Hamlet Bypass abuts the site to the south. It is surrounded by a mix of low-rise residential uses, including detached homes, townhouses and low-rise apartments, as well the Greenbelt. This site is approximately 1.5 kilometres from Blackburn Hamlet's commercial area, offering services and amenities.

Bus service currently runs along Innes Road, less than 400 metres from this site, providing access to Blair LRT Station and to Orleans to the east. A future grade separated Transitway is planned along the Blackburn Bypass, immediately south of this

site, with a planned stop approximately 450 metres via a planned multi-use pathway. The nearby Greenbelt provides access to a network of trails, and there are painted bike lanes along Innes Road connecting to the Blackburn Hamlet commercial area to the west and Orleans to the east.

Within the Official Plan, the site is located in the Outer Urban Transect as shown on Schedule A and designated Neighborhood in Schedule B3. Currently, this site is zoned R2N and is proposed to be rezoned to R4M[xxx5].

The proposed amendment will permit townhouses, stacked townhouses or a low-rise apartment up to four storeys in height. The site is anticipated to accommodate approximately ten units. The site-specific zoning proposes to amend the minimum lot area, rear yard setback and a number of other development requirements. Minimum resident and visitor parking space rates are proposed to be eliminated.

### **2060 Lanthier Drive (Document 11)**

The subject site is located in Orleans, at the southern terminus of Lanthier Drive and with frontage on the future extension of Vanguard Drive. The site is currently vacant. It is surrounded by a mix of retail and commercial uses as well as light industrial uses.

A planned grade separated Transitway will be located south of the site, providing access to the Blair LRT station. Innes Road, less than 350 metres to the north, is identified as a Transit Priority Corridor in the OP. Parkland dedication and a multi-use pathway will be provided on the site as part of the disposal.

Within the Official Plan, the site is located in the Outer Urban Transect as shown on Schedule A and designated Evolving Neighborhood in Schedule B3. Currently, this site is zoned IG7 H(21) and IG[1608] H(21) and is proposed to be rezoned to GM H(21) [xxx6].

This site is anticipated to accommodate a mixed-use development up to seven storeys in height. Due to the proximity to a range of amenities, services and transit, required minimum resident and visitor parking space rates are proposed to be eliminated; however, the proponent may choose to provide parking on-site to meet the needs to residents, visitors, employees and commercial patrons.

### **Planning Rationale**

Section 4.2 of the Official Plan speaks to prioritizing affordable housing, coordinating with goals of the City's 10-Year Housing and Homelessness Plan, with a toolkit of

incentives including alternative development standards and application processing priority. Section 4.2.1 of the Official Plan speaks to flexibility and diversity of housing options throughout the city and supports the 'missing middle' housing of mid-density, low-rise multi-unit housing. This section also speaks to appropriately balancing the value to the public interest of development application requirements against the impact to housing affordability. Section 4.2.2 references targets within the City's 10- Year Housing and Homelessness Plan, which the proposed rezoning of these will assist the City in reaching.

In reference to Section 4.2, the Department has prioritized the review and timelines associated with these proposed developments, and also reviewed the proposed amendments through the lens of considering some flexibility when applying development standards. This includes reducing the rear yard setback, eliminating the rear yard area requirement, providing flexibility in the location of the required amenity area and reducing the lot width and eliminating the minimum lot area for townhouse units.

Section 4.2.3 of the Official Plan recognizes that many individuals may not constitute nor form part of a household and may rely on long-term housing other than the traditional dwelling unit. This section provides for housing options by allowing in any zone where residential uses are permitted alternative accommodating housing forms, including a residential care facility. Therefore, this use is proposed to be added as a permitted use in all zones.

The 185 Hawthorne Street site is subject to the Old Ottawa East Secondary Plan. It is designated Neighbourhood Low-Rise and located within Policy Area 4, - Old Ottawa East Residential Neighbourhoods – including Archville, Spenceville and Rideau Gardens, the three policies relevant to the zone amendment are the following:

- 4) Building heights within the Neighbourhood Low-rise area will not exceed four storeys.
- 23) Maintain the general character of these neighbourhoods as expressed by the existing zoning.
- 25) Intensify these neighbourhoods primarily at their edge, as a transition to denser development sites and where they abut an arterial or collector road.

Notwithstanding this, building heights will be maintained at low-rise levels overall.

The proposed amendment will allow the potential for up to a four-storey apartment building as a proposed use, which is consistent with intensification at the edge of the

community and the low-rise building height is maintained. Within the context of these policies and the others noted above, the proposed zone amendment for this site is appropriate.

The 261A Hinchey Street site is located within the Scott Street Secondary Plan. It is shown as a street allowance, but within an area designated as Low-Rise Neighbourhood. The proposed amendment is in keeping with the direction of the Secondary Plan with the building height limited to three storeys or 11 metres.

To provide flexibility for the development form and number of units, the proposed zoning amendments eliminate the minimum parking requirements for both residents and visitors on all sites. Despite this, parking may be provided by proponents to suit the development or if, through the RFP process, the City identifies a need. Section 4.1.4 of the Official Plan speaks to the potential for minimum parking requirements to be reduced or eliminated where the site is in proximity to rapid transit in order to minimize asphalt and driveways. Section 5 references an urban built form with reduced or no parking provided in Hubs and Corridors in the Outer Urban and Suburban Transects, with priority on walking, cycling and transit. Section 6.3 speaks to neighbourhoods enabling a transition over time towards less automobile-dependent development and a distribution of densities so that those highest are close to rapid transit and major neighbourhood amenities. All the sites are within close proximity to rapid transit or a Transit Priority Corridor, as required by Section 4.1.4. For sites which will require site plan control approval, a notice on title indicating that parking may not be provided on site will be included as a condition. It is noted that 261A Hinchey Street and 185 Hawthorne Avenue are within Area X of Schedule 1A of the Zoning By-law and Section 101 (3) indicates that no off-street motor vehicle parking is required to be provided for the first twelve dwelling units of a residential use.

Where a proposed development will trigger site plan control approval (eg. greater than ten units or more than four storeys), a review of the site design and technical components will be conducted prior to development. As well, during the RFP process, the City may request additional plans and/or studies or impose development requirements, including the provision of parking, as needed, for individual sites.

### **Provincial Planning Statement**

The proposal is consistent with the 2020 Provincial Policy Statement and 2024 Provincial Planning Statement. Staff note that the 2024 Provincial Planning Statement came into force on October 20, 2024, and as of that date, replaced the 2020 Provincial Policy Statement.

## **RURAL IMPLICATIONS**

N/A

## **COMMENTS BY THE WARD COUNCILLOR(S)**

### **2 Pretty Street-Councillor Glen Gower**

The Councillor is supportive of the application related to this report.

### **40 Beechcliffe Street – Councillor Sean Devine**

While I am generally supportive of the Zoning By-Law Amendment as proposed for 40 Beechcliffe, I offer my support with some considerations for future planning. It is concerning to me that, until only recently, no housing was being considered for this greenspace at all, which will include the future of site of the proposed Knoxdale Station on LRT Stage 3. And so I am grateful that Housing Solutions and Investments supported the process that will result in getting at least some housing on this municipally-owned land. But it is critical that we add more density to a future LRT Stage 3 corridor than what is being proposed for this parcel. The Zoning By-Law Amendment proposed for 40 Beechcliffe is appropriate, and I welcome the new housing. I look forward to seeing how future transportation plans might be adapted to better meet our density targets along this corridor.

### **261A Hinchey Avenue-Councillor Jeff Leiper**

I'm delighted to see that this City owned site – in an area well served by LRT, active transportation, community resources and services, and shops - has been identified as a property that can be developed with affordable housing, in accordance with the Housing Accelerator Fund (HAF) Municipal Land Strategy. I have reviewed the proposal with staff and am fully supportive of these minor zoning changes.

### **185 Hawthorne Avenue-Councillor Shawn Menard**

The Councillor is aware of the application related to this report.

### **2548 Cléroux Crescent-Councillor Laura Dudas**

The Councillor is aware of the application related to this report.



**2060 Lanthier Drive-Councillor Catherine Kitts**

Since being alerted to the existence of this city-owned parcel, I recognized this site held important opportunities for our community.

One of my early concerns was unlocking the Vanguard Drive Extension to support future development. Too often, I have seen transportation planning as an afterthought in the development of my rapidly growing community. The Vanguard Drive Extension will serve as an important east/west connector for many new residential parcels, and I was thrilled that \$2.5 million was secured to advance road construction through the province's Building Faster Fund.

This funding allows for a unique opportunity to develop affordable housing in Ward 19.

I believe general mixed-use is an appropriate rezoning to allow for residential development in this otherwise commercially dominated area. However, I must express reservations regarding the elimination of minimum parking space requirements for residents and visitors. The planning rationale points to the site's proximity to the future Cumberland BRT, a hypothetical and aspirational project since the late 90s with no funding attached. While I, too, hope to see modal shift in ward 19, Orléans South-Navan still faces significant gaps in transit and transportation infrastructure. I caution that removing parking requirements without sufficient alternatives simply doesn't align with the current context of the area.

I look forward to seeing this development take shape, which will be a positive contributor to the city's overall affordable housing targets and want to thank all involved who brought this project forward for consideration to the province.

**LEGAL IMPLICATIONS**

As City-initiated rezoning applications, there is no right of appeal should one or more of the proposed zoning amendments not be adopted. With the passage of Bill 185, as amended, a zoning by-law amendment is only subject to appeal by "specified persons", essentially utility providers and government entities.

**FINANCIAL IMPLICATIONS**

There are no direct financial implications

## **ASSET MANAGEMENT IMPLICATIONS**

There are no servicing constraints identified for the proposed rezoning at this time. Servicing capacity requirements to be confirmed at time of site plan.

## **ACCESSIBILITY IMPACTS**

Accessibility will be considered at subsequent approvals.

## **TERM OF COUNCIL PRIORITIES**

These projects address the following Term of Council Priorities:

- Has affordable housing and is more liveable for all

## **APPLICATION PROCESS TIMELINE STATUS**

This application (Development Application Number: D02-02-24-0050) was processed by the "On Time Decision Date" established for the processing of Zoning By-law amendment applications.

## **SUPPORTING DOCUMENTATION**

Document 1	Location Map / Zoning Key Map-2 Pretty Street
Document 2	Details of Recommended Zoning-2 Pretty Street
Document 3	Location Map / Zoning Key Map-Part of 40 Beechcliffe Street
Document 4	Details of Recommended Zoning-Part of 40 Beechcliffe Street
Document 5	Location Map / Zoning Key Map-261A Hinchey Avenue
Document 6	Details of Recommended Zoning-261A Hinchey Avenue
Document 7	Location Map / Zoning Key Map-185 Hawthorne Avenue
Document 8	Details of Recommended Zoning-185 Hawthorne Avenue
Document 9	Location Map / Zoning Key Map-2548 Cléroux Crescent
Document 10	Details of Recommended Zoning-2548 Cléroux Crescent
Document 11	Location Map / Zoning Key Map-2060 Lanthier Drive
Document 12	Details of Recommended Zoning-2060 Lanthier Drive

Document 13 Consultation Details

## **CONCLUSION**

The proposed developments are in keeping with the direction of the Official Plan. The Zoning By-law amendments will permit the advancement of a number of housing initiatives by the City of Ottawa. Given the applicable Official Plan policies and compatibility of the uses to their areas, the Zoning By-law Amendments are recommended for approval.

## **DISPOSITION**

Office of the City Clerk, Council and Committee Services to notify the owner; applicant; of City Council's decision

Ottawa Scene Canada Signs, 13-1920 Merivale Road, Ottawa, ON K2G 1E8;

Krista O'Brien, Program Manager, Tax Billing & Control, Finance and Corporate Services Department (Mail Code: 26-76) of City Council's decision.

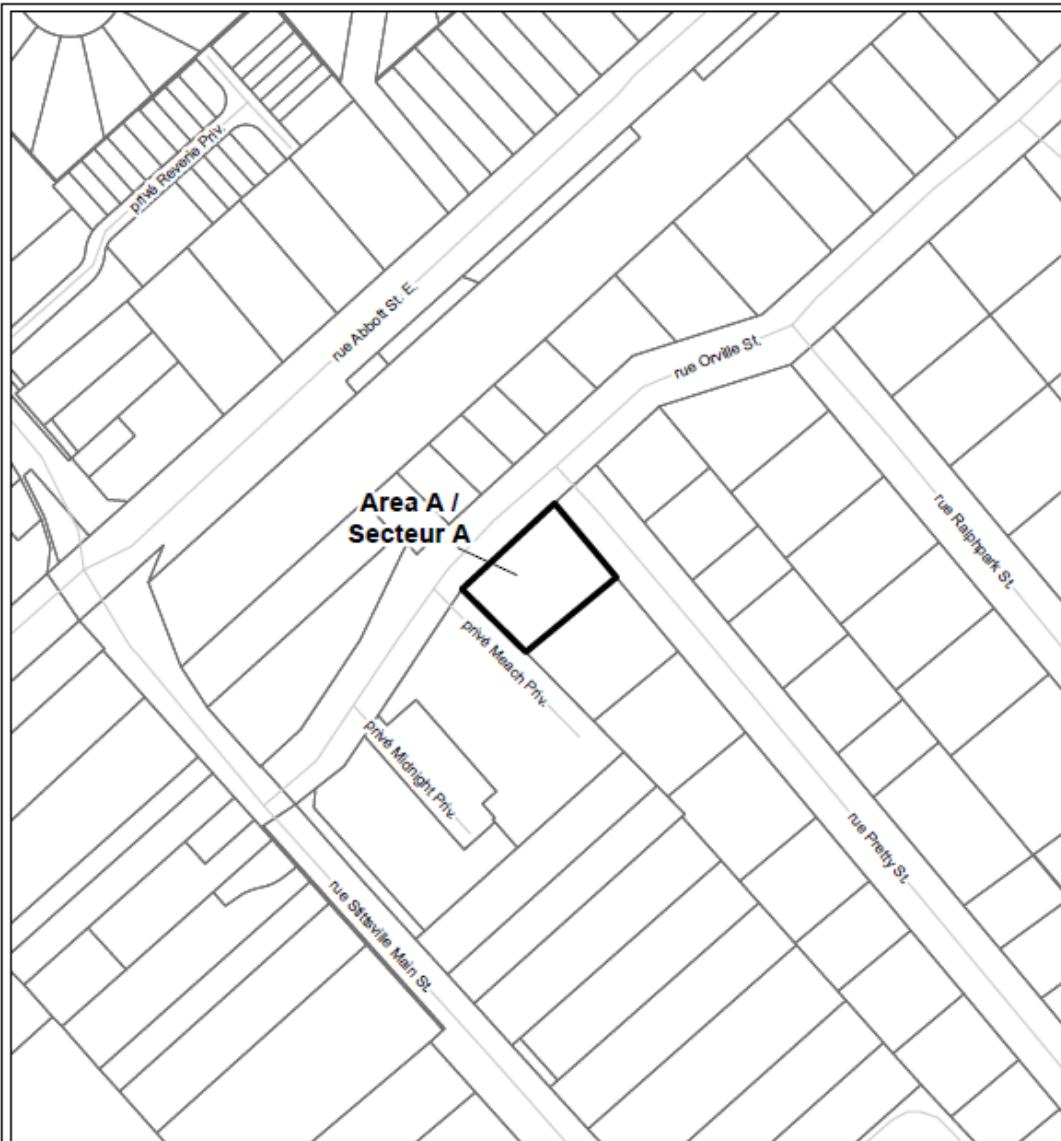
The Planning, Development and Building Services Department will prepare an implementing by-law and forward it to Legal Services.




Legal Services, City Manager's Office to forward the implementing by-law to City Council.

Planning Operations, Planning Services to undertake the statutory notification.

**Document 1 Location Map / Zoning Key Map-2 Pretty Street**

For an interactive Zoning map of Ottawa visit [geoOttawa](http://geoOttawa)



		LOCATION MAP / PLAN DE LOCALISATION ZONING KEY PLAN / SCHÉMA DE ZONAGE	
D02-02-24-0050	24-1157-X	<b>2 rue Pretty Street</b>	
I:\CO\2024\ZKP\Pretty_2		 Area A to be rezoned from L1 to R4M[xxx1] Le zonage du secteur A sera modifié de L1 à R4M[xxx1]	
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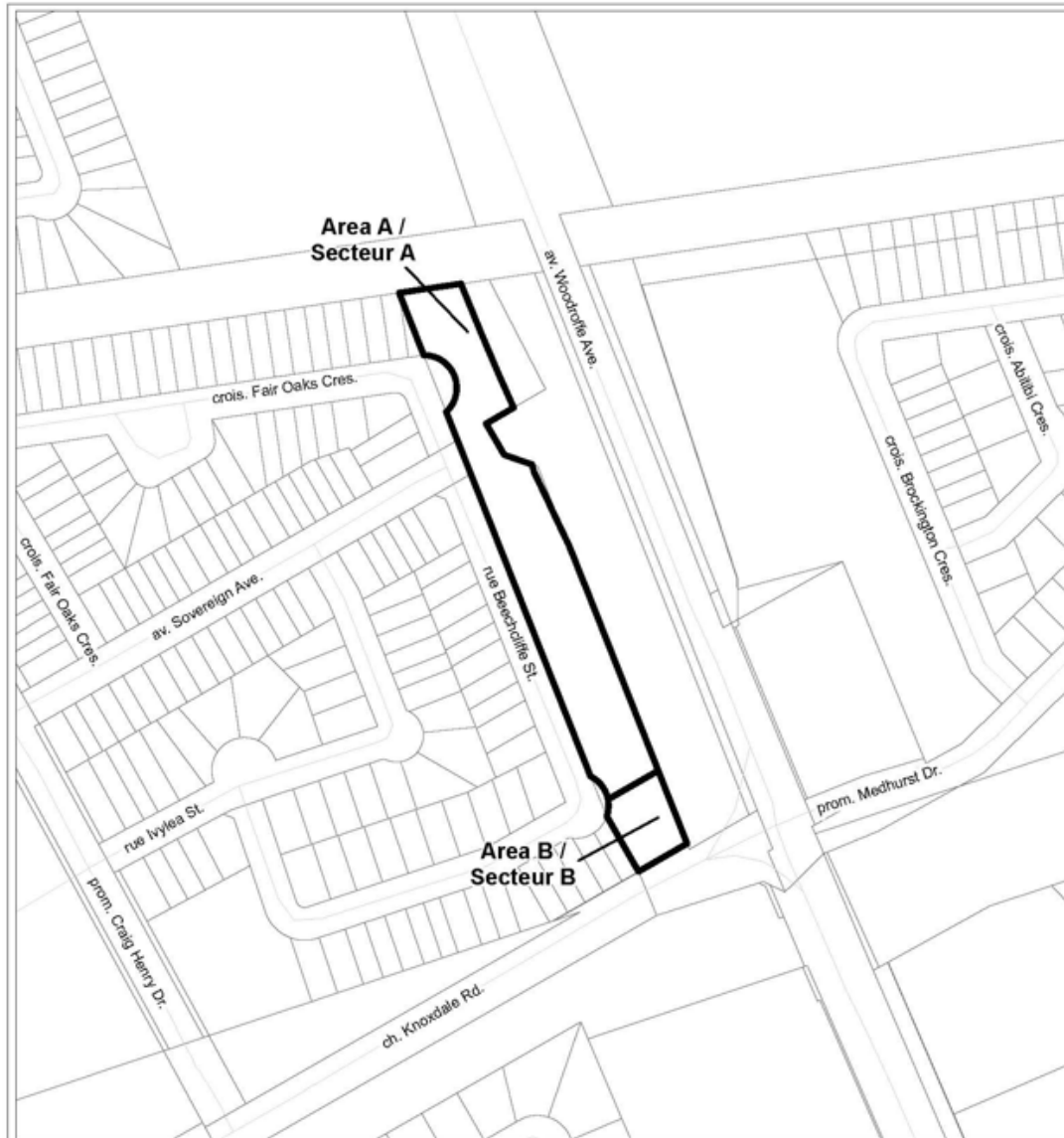
## Document 2 Details of Recommended Zoning-2 Pretty Street




The proposed change to the City of Ottawa Zoning By-law No. 2008-250 for 2 Pretty Street:

1. Rezone the lands as shown in Document 1
2. Add a new exception xxx1 to Section 239 – Urban Exceptions with provisions similar in effect to the following:
  - a. In Column I, Exception Number, add the text “xxx1”
  - b. In Column II, Applicable Zones, add the text “R4M [xxx1]”
  - c. In Column III, Exception Provisions - Additional land uses permitted, add the text ‘residential care facility’
  - d. In Column V, Provisions, add the text:
    - i. Section 101-Minimum Parking Space Rates does not apply
    - ii. Section 102 Minimum Visitor Parking Space Rates does not apply
    - iii. The front yard may contain a driveway with a maximum width of 3 metres, a walkway with a maximum width of 1.8 metres, and a garbage enclosure The remainder of the front yard, with the exception of projections permitted in Section 65, must be landscaped with soft landscaping.
    - iv. Minimum rear yard setback: 4.5 metres
    - v. Minimum rear yard area requirement: No minimum
    - vi. Maximum building height for all permitted dwelling types: the lesser of 14.5 metres or 4 storeys
    - vii. Communal amenity area required need not be in the rear yard
    - viii. For townhouses,
      - Minimum lot width: 5m
      - Minimum lot area: No minimum

**Document 3 - Location Map / Zoning Key Map-Part of 40 Beechcliffe Street**

For an interactive Zoning map of Ottawa visit [geoOttawa](http://geoOttawa)



		LOCATION MAP / PLAN DE LOCALISATION ZONING KEY PLAN / SCHÉMA DE ZONAGE	
D02-02-24-0050      24-0994-D		<b>Part of / Partie de 40 rue Beechcliffe Street</b>	
I:\CO\2024\ZKP\Beechcliffe_40		 Area A to be rezoned from R3Z[937] to R4M[xxx2] Le zonage du secteur A sera modifié de R3Z[937] à R4M[xxx2] Area B to be rezoned from R2M to R4M[xxx2] Le zonage du secteur B sera modifié de R2M à R4M[xxx2]	
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#### **Document 4 Details of Recommended Zoning-Part of 40 Beechcliffe Street**




The proposed change to the City of Ottawa Zoning By-law No. 2008-250 for part of 40 Beechcliffe Street:

1. Rezone the lands as shown in Document 3
2. Add a new exception xxx2 to Section 239 – Urban Exceptions with provisions similar in effect to the following:
  - a. In Column I, Exception Number, add the text “xxx2”
  - b. In Column II, Applicable Zones, add the text “R4M [xxx2]”
  - c. In Column III, Exception Provisions - Additional land uses permitted, add the text ‘residential care facility’
  - d. In Column V, Provisions, add the text:
    - i. Section 101-Minimum Parking Space Rates does not apply
    - ii. Section 102 Minimum Visitor Parking Space Rates does not apply
    - iii. The front yard may contain a driveway with a maximum width of 3 metres, a walkway with a maximum width of 1.8 metres and a garbage enclosure. The remainder of the front yard, with the exception of projections permitted in Section 65, must be landscaped with soft landscaping.
    - iv. Minimum rear yard setback: 4.5 metres
    - v. Minimum rear yard area requirement: No minimum
    - vi. Maximum building height for all permitted dwelling types: the lesser of 14.5 metres or 4 storeys
    - vii. Communal amenity area required need not be in the rear yard
    - viii. For townhouses,
      - Minimum lot width: 5m
      - Minimum lot area: No minimum

**Document 5 Location Map / Zoning Key Map-261A Hinchey Avenue**

For an interactive Zoning map of Ottawa visit [geoOttawa](http://geoOttawa)



		LOCATION MAP / PLAN DE LOCALISATION ZONING KEY PLAN / SCHÉMA DE ZONAGE	
D02-02-24-0050	24-1157-X	<b>261A av. Hinchey Avenue</b>	
I:\CO\2024\ZKPI\Hinchey_261A		 Area A to be rezoned from R4UB to R4UB[xxx3] Le zonage du secteur A sera modifié de R4UB à R4UB[xxx3]	
<small>©Parcel data is owned by Teramet Enterprises Inc. and its suppliers. All rights reserved. May not be produced without permission. THIS IS NOT A PLAN OF SURVEY.</small>		Entire map area is affected by the Mature Neighbourhoods Overlay (section 139) / Tout le secteur de la carte est touché par la Zone sous-jacente de quartiers établis (article 139)	
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## Document 6 Details of Recommended Zoning-261A Hinchey Avenue

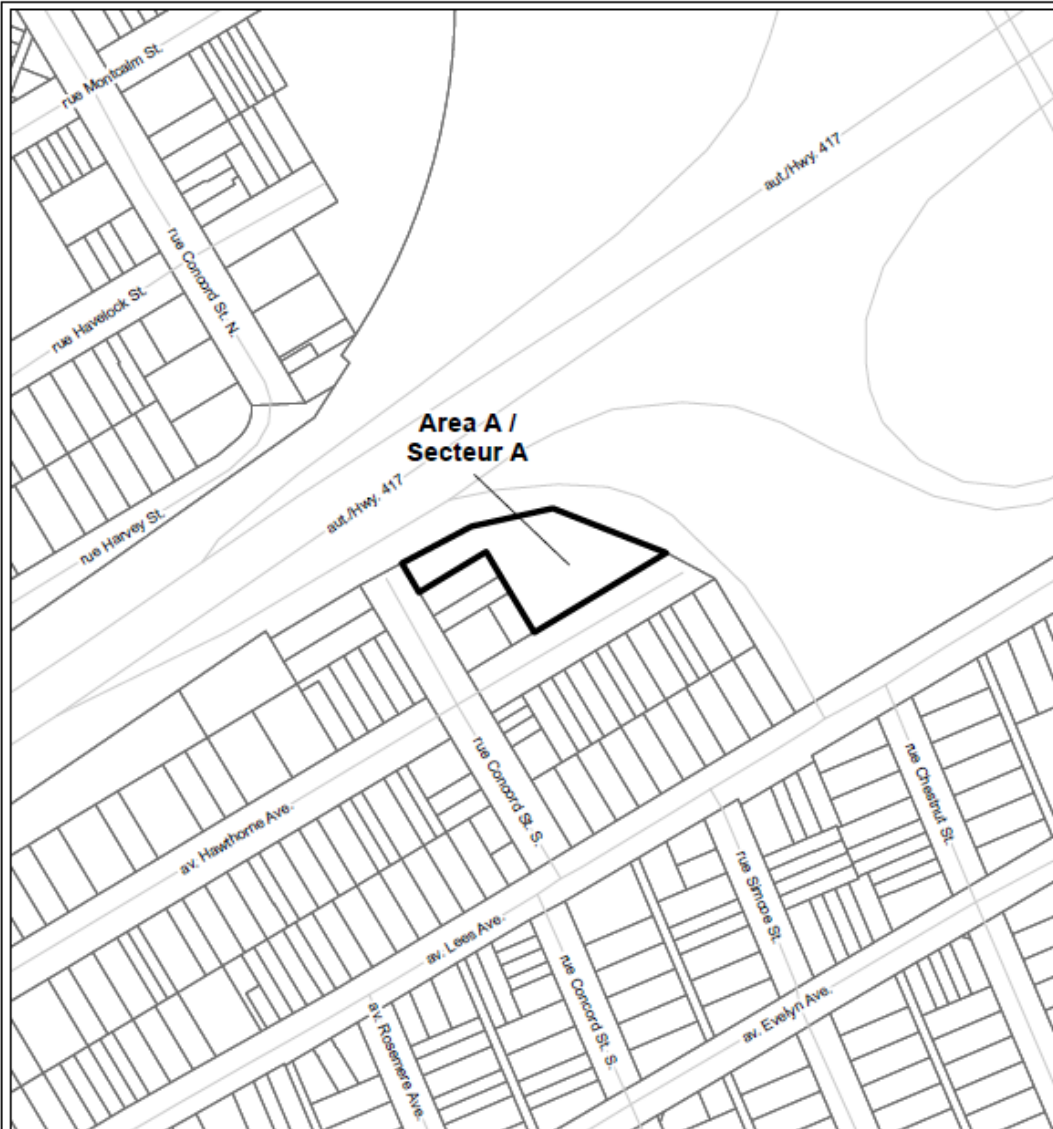
The proposed change to the City of Ottawa Zoning By-law No. 2008-250 for 261A Hinchey Avenue:



1. Rezone the lands as shown in Document 5
2. Add a new exception xxx3 to Section 239 – Urban Exceptions with provisions similar in effect to the following:
  - a. In Column I, Exception Number, add the text “xxx3”
  - b. In Column II, Applicable Zones, add the text “R4UB[xxx3]”
  - c. In Column III, Exception Provisions - Additional land uses permitted, add the text ‘residential care facility’
  - d. In Column V, Provisions, add the text:
    - i. Section 101-Minimum Parking Space Rates does not apply
    - ii. Section 102 Minimum Visitor Parking Space Rates does not apply
    - iii. Minimum rear yard setback: 6 metres
    - iv. Minimum rear yard area requirement: 0
    - v. Maximum building height for all permitted dwelling types: the lesser of 11 metres or 3 storeys
    - vi. Communal amenity area required need not be in the rear yard
    - vii. For townhouses,
      - Minimum lot area: No minimum
    - viii. Minimum of 20% of the lot area must be soft landscaped area
    - ix. Front yard setback must align with the average of the abutting lots’ front yard setback
    - x. Paragraphs 161(15)(b)(iv) (e) and (h) do not apply

- xi. Clause 107(3)(b) does not apply and a driveway may be located in the side yard and rear yard

**Document 7 Location Map / Zoning Key Map-185 Hawthorne Avenue**

For an interactive Zoning map of Ottawa visit [geoOttawa](http://geoOttawa)



		LOCATION MAP / PLAN DE LOCALISATION ZONING KEY PLAN / SCHÉMA DE ZONAGE	
D02-02-24-0050	24-1171-X	<b>185 avenue Hawthorne Avenue</b>	
I:\COI\2024\ZKPI\Hawthorne_185		 Area A to be rezoned from R3P to R4UB[xxx4] Le zonage du secteur A sera modifié de R3P à R4UB[xxx4]	
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REVISION / RÉVISION - 2024 / 10 / 18		Entire map area is affected by the Mature Neighbourhoods Overlay (section 139) / Tout le secteur de la carte est touché par la Zone sous-jacente de quartiers établis (article 139)	

## Document 8 Details of Recommended Zoning-185 Hawthorne Avenue

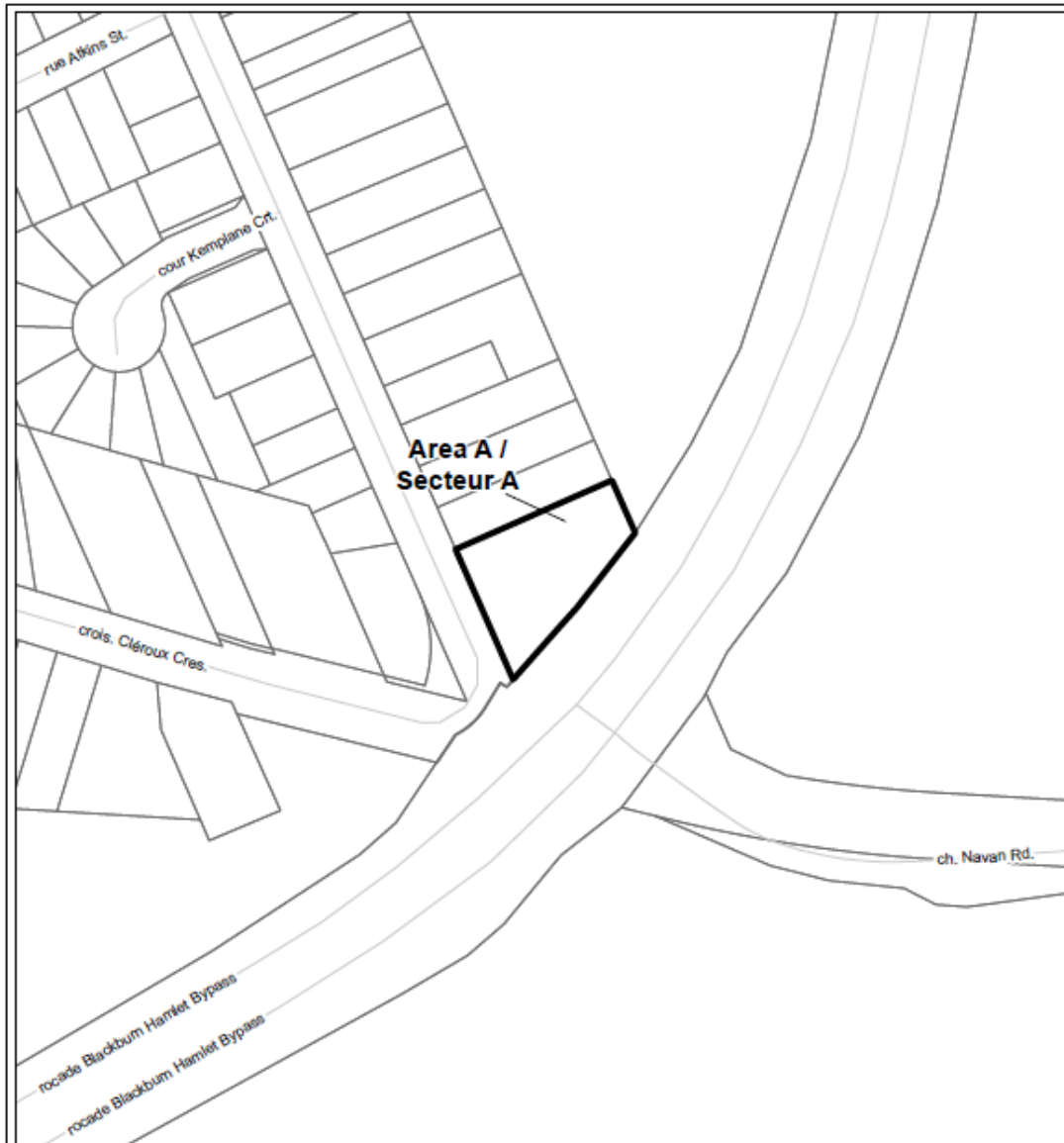
The proposed change to the City of Ottawa Zoning By-law No. 2008-250 for 185 Hawthorne Avenue:




1. Rezone the lands as shown in Document 7
2. Add a new exception xxx4 to Section 239 – Urban Exceptions with provisions similar in effect to the following:
  - a) In Column I, Exception Number, add the text “xxx4”
  - b) In Column II, Applicable Zones, add the text “R4UB[xxx4]”
  - c) In Column III, Exception Provisions - Additional land uses permitted, add the text ‘residential care facility’
  - d) In Column V, Provisions, add the text:
    - i. Hawthorne Avenue is deemed to be the front lot line
    - ii. Section 101-Minimum Parking Space Rates does not apply
    - iii. Section 102 Minimum Visitor Parking Space Rates does not apply
    - iv. Minimum rear yard setback: 4.5 metres
    - v. Minimum rear yard area requirement: No minimum
    - vi. Maximum building height for all permitted dwelling types: the lesser of 14.5 metres or 4 storeys
    - vii. Communal amenity area required need not be in the rear yard
    - viii. For townhouses,
      - Minimum lot area: No minimum
    - ix. Front yard setback must align with the average of the abutting lots’ front yard setback
    - x. Clauses 161(15) (e) and (h) do not apply
    - xi. Endnotes 11 and 12 to Table 162A do not apply.

- xii. Section 107(3)(b) does not apply and a driveway may be located in the side yard and rear yard

**Document 9 Location Map / Zoning Key Map-2548 Cléroux Crescent**

For an interactive Zoning map of Ottawa visit [geoOttawa](http://geoOttawa)



		LOCATION MAP / PLAN DE LOCALISATION ZONING KEY PLAN / SCHÉMA DE ZONAGE	
D02-02-24-0050	24-1157-X	<b>2548 crois. Cléroux Crescent</b>	
I:\COI\2024\ZKP\Cleroux_2548		 Area A to be rezoned from R2N to R4M[xxx5] Le zonage du secteur A sera modifié de R2N à R4M[xxx5]	
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## Document 10 Details of Recommended Zoning-2548 Cléroux Crescent

The proposed change to the City of Ottawa Zoning By-law No. 2008-250 for 2548 Cléroux Crescent:

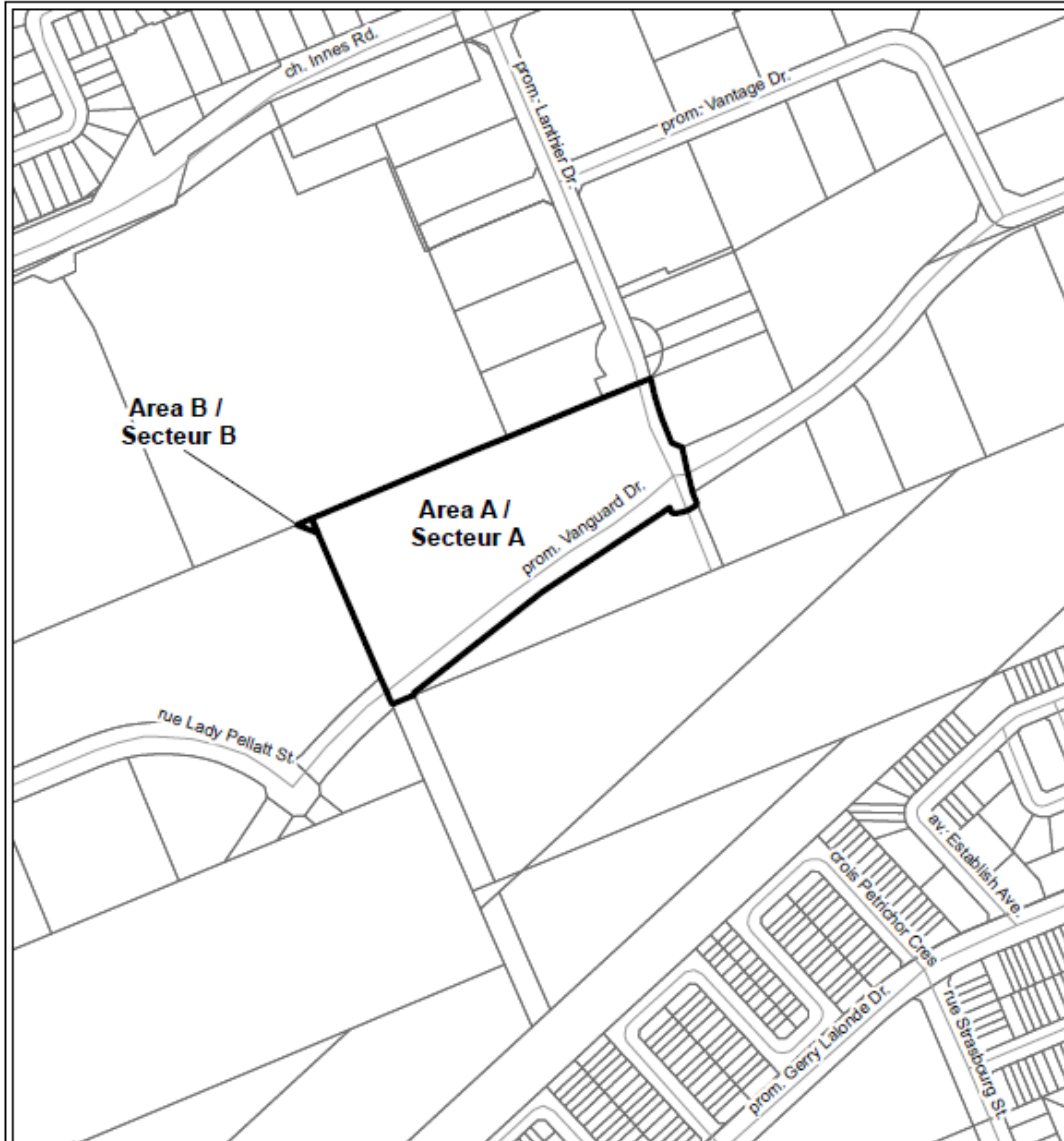
1. Rezone the lands as shown in Document 9
2. Add a new exception xxx5 to Section 239 – Urban Exceptions with provisions similar in effect to the following:
  - a. In Column I, Exception Number, add the text “xxx5”
  - b. In Column II, Applicable Zones, add the text “R4M [xxx5]”
  - c. In Column III, Exception Provisions - Additional land uses permitted, add the text ‘residential care facility’
  - d. In Column V, Provisions, add the text:
    - i. Section 101-Minimum Parking Space Rates does not apply
    - ii. Section 102 Minimum Visitor Parking Space Rates does not apply
    - iii. The front yard may contain a driveway with a maximum width of 3 metres, a walkway with a maximum width of 1.8 metres and a garbage enclosure. The remainder of the front yard, with the exception of projections permitted in Section 65, must be landscaped with soft landscaping.
    - iv. Minimum rear yard setback: 4.5 metres
    - v. Minimum rear yard area requirement: No minimum
    - vi. Maximum building height for all permitted dwelling types: the lesser of 14.5 metres or 4 storeys
    - vii. Communal amenity area required need not be in the rear yard
    - viii. For townhouses,
      - Minimum lot width: 5m



- Minimum lot area: No minimum



**Document 11 Location Map / Zoning Key Map-2060 Lanthier Drive**

For an interactive Zoning map of Ottawa visit [geoOttawa](http://geoOttawa)



		LOCATION MAP / PLAN DE LOCALISATION ZONING KEY PLAN / SCHEMA DE ZONAGE					
2060 promenade Lanthier Drive							
D02-02-24-0050	24-1157-X						
I:\CO\2024\Zoning\Lanthier_2060							
©Parcel data is owned by Teconet Enterprises Inc. and its suppliers. All rights reserved. May not be produced without permission. THIS IS NOT A PLAN OF SURVEY.		<table border="0"> <tr> <td style="width: 20px; height: 15px; border: 1px solid black; margin-right: 5px;"></td> <td>                     Area A to be rezoned from IG7 H(21) to GM[xxx6] H(21)                      Le zonage du secteur A sera modifié de IG7 H(21) à GM[xxx6] H(21)                 </td> </tr> <tr> <td style="width: 20px; height: 15px; border: 1px solid black; margin-right: 5px;"></td> <td>                     Area B to be rezoned from IG[1608] H(21) to GM[xxx6] H(21)                      Le zonage du secteur B sera modifié de IG[1608] H(21) à GM[xxx6] H(21)                 </td> </tr> </table>			Area A to be rezoned from IG7 H(21) to GM[xxx6] H(21) Le zonage du secteur A sera modifié de IG7 H(21) à GM[xxx6] H(21)		Area B to be rezoned from IG[1608] H(21) to GM[xxx6] H(21) Le zonage du secteur B sera modifié de IG[1608] H(21) à GM[xxx6] H(21)
	Area A to be rezoned from IG7 H(21) to GM[xxx6] H(21) Le zonage du secteur A sera modifié de IG7 H(21) à GM[xxx6] H(21)						
	Area B to be rezoned from IG[1608] H(21) to GM[xxx6] H(21) Le zonage du secteur B sera modifié de IG[1608] H(21) à GM[xxx6] H(21)						
REVISION / RÉVISION - 2024 / 10 / 18							

**Document 12 Details of Recommended Zoning-2060 Lanthier Drive**

The proposed change to the City of Ottawa Zoning By-law No. 2008-250 for 2060 Lanthier Drive:

1. Rezone the lands as shown in Document 11
2. Add a new exception xxx6 to Section 239 – Urban Exceptions with provisions similar in effect to the following:
  - a. In Column I, Exception Number, add the text “xxx6”
  - b. In Column II, Applicable Zones, add the text “GM H(21) [xxx6]”
  - c. In Column V, Provisions, add the text:
    - i. Section 101-Minimum Parking Space Rates does not apply
    - ii. Section 102 Minimum Visitor Parking Space Rates does not apply

## **Document 13 – Consultation Details**

### Notification and Consultation Process

Pursuant to Section 11.4, Policy 3 of the Official Plan, alternative notification for a zoning by-law amendment was undertaken. Bilingual signs were posted on the individual sites. Newspaper ads were published in one English and one French language daily newspaper on October 12, 2024 prior to the scheduled public meeting. Notice will also be given on Ottawa.ca through the meeting agenda.

### **2 Pretty Street**

Number of Public Comments Received: 7

### **Transportation**

- Vehicles already park where none are permitted, creating sight line issues for drivers when exiting Meach Private onto Orville Street
- Orville Street has no sidewalks and no street parking
- Concerned cars will park in visitor parking spots of the neighboring condo complex
- Concerns regarding the loss of existing parking spaces at 2 Pretty.
- Will further burden an already overloaded Orville Street

#### *Staff response*

The proposed amendment includes removing minimum parking requirements to allow for flexibility during the design process, however the proponent may choose to provide parking. The City may also require parking, and will be determined during the disposal process. .

Orville Street is a local road and has sufficient capacity. With the maximum anticipated ten units, it would generally generate three peak hour vehicles.

### **Urban Form**

- What will the height, building type, and unit orientation be?
- Privacy features between 2 Pretty and adjacent property are requested.

#### *Staff response*

The building typology may be a townhouse, stacked townhouse or low-rise apartment, up to the lesser of four storeys or 14.5 m in height and up to ten units. The building orientation and privacy features are undetermined at this point, but these concerns have been noted.

### **General**

- Concerned about vibrations from the construction of the proposed development compromising structural integrity of neighboring buildings
- Building should be restricted to providing housing to seniors

*Staff response*

Development and construction will be subject to the Building Code and all by-laws in effect.

Staff note the comment regarding providing housing for seniors, however zoning can not dictate occupancy.

**Part of 40 Beechcliffe Street**

Number of Public Comments Received: 15

**Transportation**

- Will the road be widened to support extra vehicles and people?
- Will snow removal be more consistent due to more traffic with more families/children?
- Will there be space for LRT service alongside Woodroffe?
- Site was supposed to be converted into rail network for the Barrhaven extension
- Concerns about eliminating parking requirements and how this might impact on-street parking.
- Shouldn't be built until the Algonquin LRT station is open or bus express service is re-enabled
- Concern with added traffic

*Staff Response*

Additional road right of way is not required.

Snow removal of City streets and sidewalks will continue to be the responsibility of the City.

The proposal includes removing minimum parking requirements to allow for flexibility during the design process. Parking provided will be determined during the disposal process at a future date. The proponent may choose to provide parking. .

**Urban Form**

- Townhomes could be a better option to fit better in with the adjacent neighborhood
- Suggest that there would be an enclosed green space on site
- Site would be better used as a park

- Last consultation with LRT Phase 3 in Feb 2023 had a very different view of the site, with parks and other community items – please provide a consistent message from the city

*Staff Response*

Comments noted

**General**

- Concerned that there was no mention of greenspace/park as suggested with the Barrhaven LRT Planning and Environment Assessment Study
- Concerned about construction noise, pollution and blasting that will affect Beechcliffe/Fair Oaks
- Concern with rodents and flooding during construction
- Concern with property value of residence nearby newly built low-rise apartments
- Concern with location, attractiveness for animals and smell of large garbage bins – townhomes would not have these issues
- Craig Henry in this vicinity is almost exclusively low income housing

*Staff Response*

Construction at this site will be subject to the Noise By-law and all requirements to keep the site clean and safe.

Stormwater management on site to be addressed at site plan stage, pre and post development stormwater flows from site to be the same.

Property values are based on a number of criteria.

Zoning can not dictate occupancy.

**261A Hinchey Avenue**

Number of Public Comments Received: 3

**Urban Form**

- Neighboring property owners would like more information on the proposed housing types for the site

*Staff response*

Proposal has not been determined at this time. The zoning allows for housing form from detached dwellings to a low-rise apartment, it is anticipated that up to four units could be provided.

**Hintonburg Community Association comments**

- Not opposed with application as long as units are affordable and include larger units (3 bedroom units)
- Site has bedrock at and just below surface, could affect cost of excavation
- Amenity area must be provided
- If tree planting is not feasible in rear yard, consider providing in front yard

#### *Staff Response*

Proposed amendment would allow amenity area to be provided through balconies, terraces or rooftops.

Other comments noted

### **185 Hawthorne Avenue**

Councillor Menard hosted a public meeting electronically on October 21, 2024.

Number of Public Comments Received: 16

#### **Transportation**

- Concerned that the developer may not allow for parking spaces
  - o alternate transit is being cut (Bus #55)
  - o many residents do not work downtown so their commutes are mainly by car
  - o lack of suitable parking in area
  - o would lead to street parking
    - blocks traffic
    - hinders garbage and snow removal
- Street is a dead end and is used to dump snow and very difficult to clean – would be more difficult with increased cars and development
- Too many on-street parking

#### *Staff Response*

Proponent may provide parking. It is noted that this site is within Area X of Schedule 1A of the Zoning By-law and Section 101 (3) indicates that no off-street motor vehicle parking is required to be provided for the first twelve dwelling units of a residential use. The subject site is well serviced by pedestrian, cycling and transit infrastructure, and is within 500 metres of Lees O-Train Station. Street parking subject to City By-law.

#### **Greenspace**

- The existing land is a well-maintained green space with multiple mature trees
- Community has lost green spaces in community
- Not 'unused City owned land' – used as a common yard by residents as most houses don't have backyards/greenspaces

- Dog walking
- Children playing
- Students studying
- Nearby daycares and day camps bring children to play – safer because it is at the end of a dead end street

*Staff Response*

Site is not a City park, but understand and note concern

Ministry of Transportation requires a 14m buffer from the property line of MTO land.

**General**

- Specific clarification of exceptions so surrounding residents can comment effectively.
- There are many other actually 'vacant' lots that can be used for this purpose
- The more dense a neighbourhood, the less well people do on various social indicators
- Proposed ZBL change is not consistent with OOESP policies
- Lands are contaminated

*Staff Response*

Comments noted.

Zoning can not dictate occupancy.

Refer to the Planning Rationale above with respect to Secondary Plan analysis.

Site remediation will be completed to Ministry of the Environment, Conservation and Parks standards

**Old Ottawa East Community Association**

- Proposal does not conform with Policies 23 and 25 of the Old Ottawa East Secondary Plan
- Site has long been considered parkland
- Request a parkette if the site is developed
- Of the opinion that some parking is required

*Staff Response*

Refer to the Planning Rationale above with respect to Secondary Plan analysis

Site is not considered a City park

Proponent may provide parking, or if the City identifies a need through the RFP process. It is noted that this site is within Area X of Schedule 1A of the Zoning By-law and would not require off-street parking for the first twelve dwelling units.

### **2548 Cl rroux Crescent**

Number of Public Comments Received: 7

#### **Transportation**

- Increase in traffic is already noticeable from construction of nearby projects, concerned for when they are finished

#### *Staff Response*

With the maximum anticipated ten units, it would generally generate three peak hour vehicles.

#### **Urban Form**

- Requesting proposed development blueprints/site plan
- What type of housing will be on the site and will they be city-owned?
- Already two medium density projects being built on either end of Cleroux
- Housing without underground parking would greatly inconvenience nearby residents
- Currently used as a greenspace for residents from townhouses without yards and dog walkers
- Concerned that the site is currently a wildlife corridor, many animal types have regularly been spotted
- Concern with amendment which would allow anything other than a single detached home, area should have same type of housing rather than a mix
- Concern with respect to decrease in value of existing homes
- Site is too small for multi-unit dwellings

#### *Staff Response*

The building typology may be a townhouse, stacked townhouse or low-rise apartment, up to the lesser of four storeys or 14.5 m in height and up to ten units.

The intent is to dispose of the site to a housing provider.

Site is not a City park.

Current R2N zoning permits duplexes and semi-detached.

Property values are affected by a number of criteria



## General Planning

- Cleroux Cr is a school bus route with a very sharp corner
- Will the concept shown on Page 22 of the [Cumberland Transitway Project](#) still be possible if the residential project goes ahead? (Pedestrian MUP connection to intersection that starts right by the subject site)
- Development should be conditional on completion of the MUP link between Cleroux and Navan to connect the site to the wider active transportation network
- A mixed use zoning with ground floor retail would be ideal for land use flexibility and increasing community usage of the site as it is near a future transit station in the Official Plan
- Water pressure issues, sewer pipes have not been updated and can not accommodate extra development

### *Staff Response*

Comments noted

The multi-use pathway will be co-ordinated and designed to ensure both the pathway and development are feasible. The pathway will be designed to avoid this property.

## **2060 Lanthier Drive**

Number of Public Comments Received: 1

### **General**

- Will the project be able to connect the Avalon West neighborhood to the shops of Innes Road

### *Staff response*

Comment noted-land will be dedicated for the multi-use pathway