



MODERN DESIGNS  
INTELLIGENT PLANNING

October 29, 2024

## **PLANNING RATIONALE**

### **RE: 28 Gwynne Avenue**

**Committee of Adjustment**  
Received | Reçu le

**2024-11-01**

**City of Ottawa | Ville d'Ottawa**  
**Comité de dérogation**



**Existing House**



**Proposed Single Detached Home**

### **Introduction:**

This proposal seeks approval for the construction of a single detached home, designed to meet the needs of a modern family while harmonizing with the surrounding neighbourhood. The plan includes a front facing attached garage and a driveway, offering both convenience and visual consistency within the community.

The inclusion of the attached front garage, which aligns with the intent of the official plan and the zoning by-law, triggers a minor variance. This adjustment is intended to enhance the property's functionality and contributes to the neighbourhood's character.

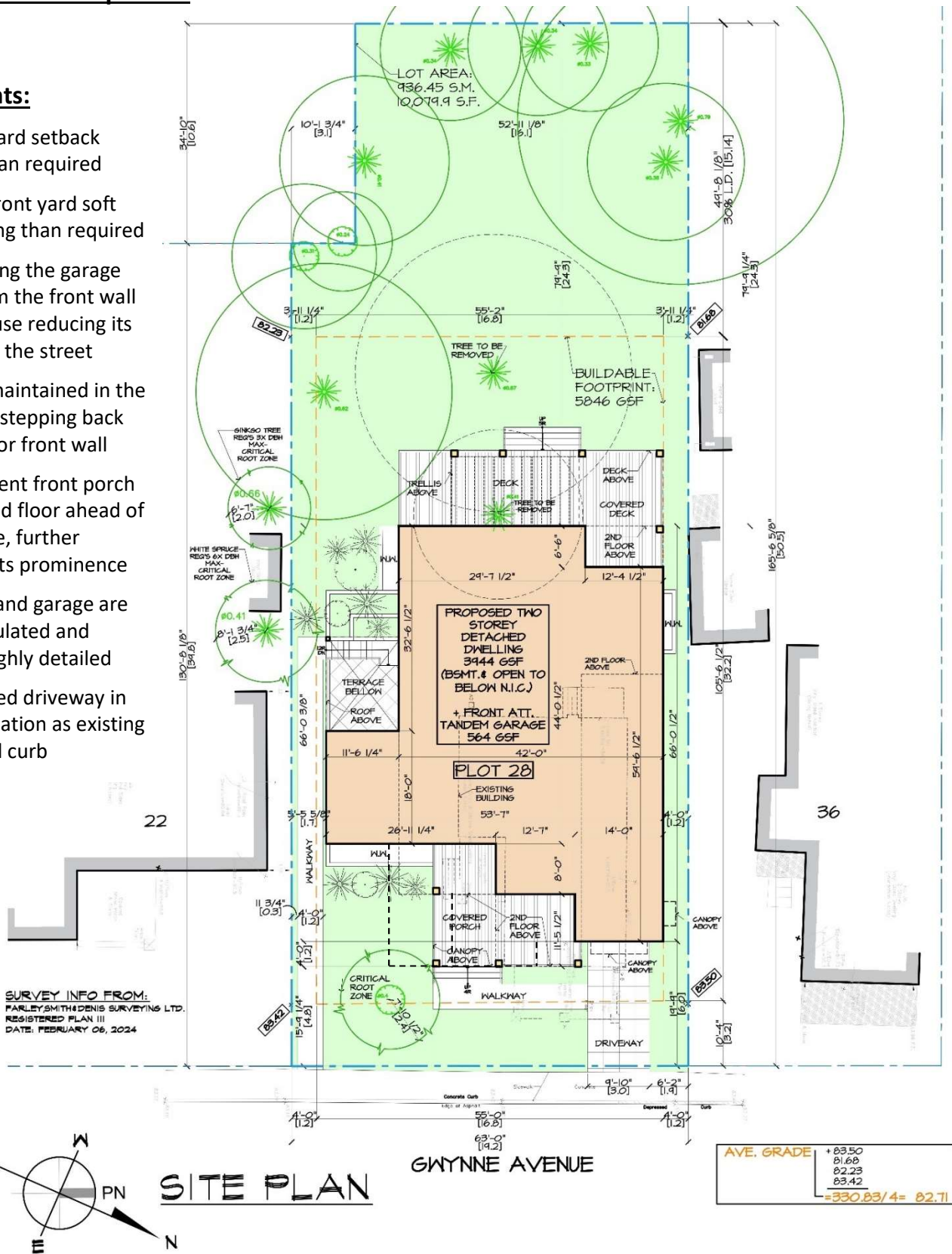
### **Minor Variance Required:**

To permit a front facing attached garage

**Proposed Development:**

**Key Points:**

- 1- Front yard setback further than required
- 2- More front yard soft landscaping than required
- 3- Recessing the garage 1.2 m from the front wall of the house reducing its impact on the street
- 4- Trees maintained in the design by stepping back the exterior front wall
- 5- Prominent front porch and second floor ahead of the garage, further reducing its prominence
- 6- House and garage are well-articulated and equally highly detailed
- 7- Proposed driveway in similar location as existing depressed curb



**Elevations:**



**EAST ELEVATION**

- 1-storey garage
- Occupies ¼ of the width of the house
- Set back from front of house
- Garage door impact reduced with glazing
- Well detailed
- Rich material application



**WEST ELEVATION**



SOUTH ELEVATION



NORTH ELEVATION

**Perspectives:**





## **Zone Performance Standards and Analysis:**

Areas of compliance are noted with a green checkmark (✓) and areas of non-compliance are indicated with a red (✗).

<b>R1QQ Zone (MNO)</b>	<b>Provision</b>	<b>Provided</b>	<b>Compliance</b>
<b>Permitted Uses</b> S.155(1)	<ul style="list-style-type: none"> <li>- Bed and breakfast</li> <li>- Detached dwelling</li> <li>- Diplomatic mission</li> <li>- Group home</li> <li>- Home-based business</li> <li>- Home-based daycare</li> <li>- Park</li> <li>- Retirement home, converted</li> <li>- Secondary dwelling unit</li> <li>- Urban agriculture</li> </ul>	<ul style="list-style-type: none"> <li>- Detached dwelling</li> <li>- Secondary dwelling unit</li> </ul>	✓
<b>Conditional Permitted Use</b> S.155(2)	Rooming house if it is located on a lot fronting on and having direct vehicular access to an Arterial or Major Collector Road.	n/a	✓
<b>Minimum Lot Width</b> S.156, Table 156A	12 m	19.2 m	✓
<b>Minimum Lot Area</b> S.156, Table 156A	360 m <sup>2</sup>	936.42 m <sup>2</sup>	✓
<b>Maximum Building Height</b> S.156, Table 156A	8.5 m	8.13 m	✓
<b>Minimum Front Yard Setback</b> S.156, Table 156A	3 m	4.8 m	✓
<b>Minimum Rear Yard Setback</b> S.156, S.144, Table 144A	30% of the lot depth = 15.14 m	24.3 m	✓
<b>Minimum Interior Side Yard Setback</b> S.156, Table 156A	1.2 m	1.2 m and 1.7 m	✓
<b>Maximum Lot Coverage</b> S.156, Table 156A	n/a	25.46%	✓
<b>Front Yard Landscaping</b> S.139, Table 139(1)	40% Minimum Aggregated Soft Landscaped Area	71.62%	✓
<b>Minimum Landscape Strip</b> S.139(2) (c)	0.15 m	1.9 m	✓
<b>Driveway</b> S.139(2) (a) (i)	within the Mature Neighbourhoods Overlay, a driveway is only permitted where in accordance with the confirmed Streetscape Character Analysis and Table 140B; and where permitted, the maximum width is as per Table 139(3);	Individual Single Driveway	✓
<b>Maximum Width of an Individual Single Driveway</b> S.139, Table 139(3)	3 m	3 m	✓

R1QQ Zone (MNO)	Provision	Provided	Compliance
<b>Minimum Garage Setback</b> S.139(3) (a)	0.6m	1.2 m	✓
<b>Front facing Garage</b> S.139(3) (c)	Within the Mature Neighbourhoods Overlay, no such garage or carport is permitted except subject to the Streetscape Character Analysis and Table 140A	Front Attached Tandem Garage	✗
<b>Maximum Number of Walkways Extend to the Right of Way</b> S.139(4) (f)	One walkway per yard is permitted to extend to the right-of-way in the case of a detached, semi-detached, long semi-detached or townhouse dwelling.	1	✓
<b>Maximum Width of Walkway</b> S.139(4) (c) (ii)	1.2 m	1.2 m	✓
<b>Existing Average Grade</b> S.139(5)	n/a	82.71 m	✓

### Summary of Zone Performance Standards and Analysis:

The proposed detached home with a front facing attached garage is meeting and exceeding all the performance standards as per the zoning bylaw table above. The only non-compliant provision is the front facing garage as determined by the streetscape character analysis.



## **Streetscape Character Analysis (SCA), (By-law 2020-289):**

### **Section 140**

140. (3) A Streetscape Character Analysis (SCA) must be confirmed prior to any application under the Planning Act, building permit application, or approval under the Private Approach By-law, whose approval would permit:

(a) a new residential use building.

140. (6) (a) The Streetscape Character Analysis must document the dominant pattern with respect to:

(i) location and type of driveways;

(ii) location and size of all parking spaces, garages and carports; and,

(iii) orientation of principal entranceways.

140. (6) (b) The attributes listed in subsection 140(6)(a) must be recorded for 21 lots located on the same street as the affected lot, as follows:

(i) the ten lots nearest the affected lot abutting the same side of the street and located within the same block;

(ii) the lot immediately opposite and across the street from the affected lot, and

(iii) the ten lots nearest the lot specified in (ii) herein, located within the same block as the affected lot.

### **Note on the SCA Interpretation:**

HD&P met section 140 in the selection of lots to be analyzed (*see Figure 2*).

The municipal planner's selection is different (*see Figure 5*).

**Streetscape Character Analysis - Prepared By HD&P:**

TABLE 1 GARAGES, CARPORTS & PARKING		HOW MANY LOTS ?
<b>Character Group</b>	<i>Pattern Refer to Table 140(A)</i>	<b>Total</b>
A.No garage or carport is attached to the front façade or corner façade of the dwelling		12
B.Garage or carport is attached to the front façade or corner façade of the dwelling		9

TABLE 2 DRIVEWAY CHARACTER & LEGAL FRONT YARD PARKING		HOW MANY LOTS ?
<b>Character Group</b>	<i>Pattern Refer to Table 140(B)</i>	<b>Total</b>
A.No driveways		0
B.Individual/shared driveway		16
C.Double-wide driveway		5
D.Legal front yard parking		0

TABLE 3 PRINCIPAL ENTRANCEWAY		HOW MANY LOTS ?
<b>Character Group</b>	<i>Pattern Refer to Table 140(C)</i>	<b>Total</b>
A.Principal entranceway is located on the front façade of the dwelling unit and faces the street		21
B.Principal entranceway is not located on the front façade of the dwelling unit and does not face the street		0

Figure 1

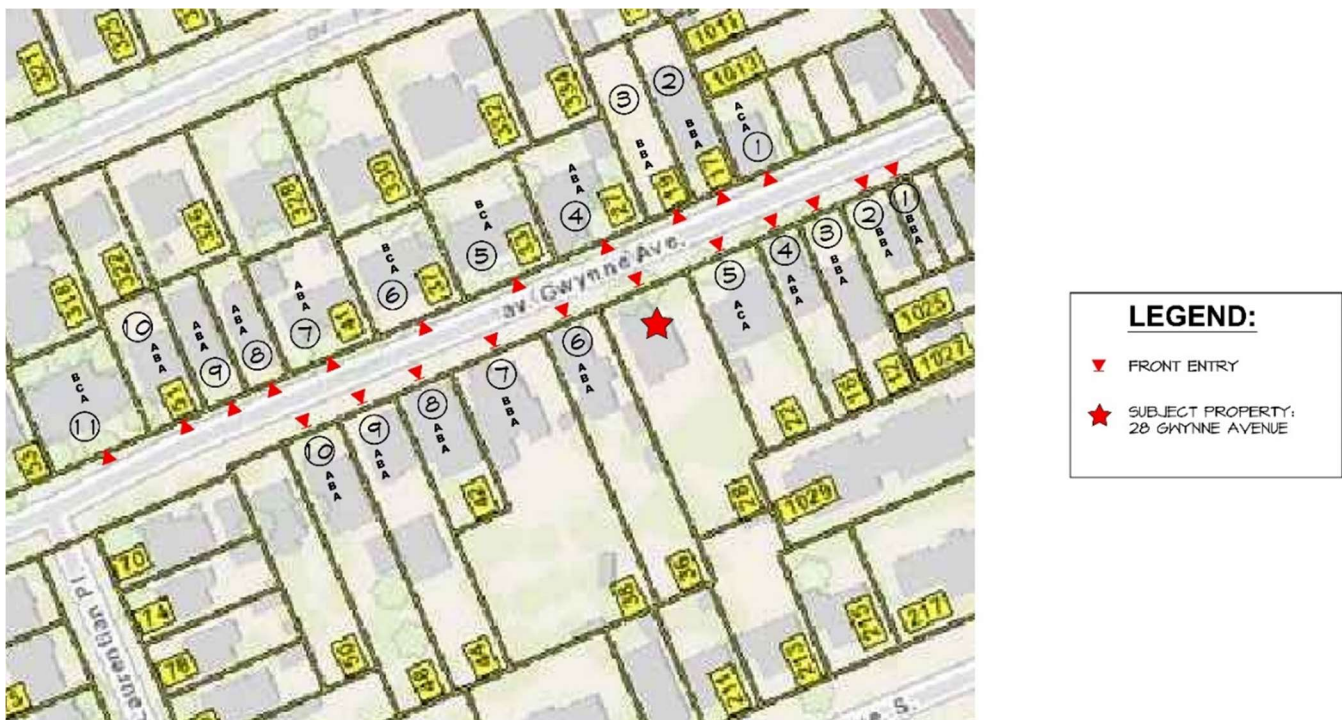


Figure 2



STREETSCAPE OF LOTS ADJACENT TO 28 GWYNNE AVENUE



STREETSCAPE OF LOTS ACROSS FROM 28 GWYNNE AVENUE \*

Figure 3

**Summary of Streetscape Character Analysis - Prepared By HD&P:**

The proposed detached home with a front facing attached garage requires a minor variance, based on the Streetscape Character Analysis (*illustrated in Figures 1, 2 and 3*), which indicates that although a significant number of the 21 surveyed lots have front facing attached garages, they still fall just short by 2 lots of the required majority by a margin of 9 to 12.



**Neighbourhood Context:**

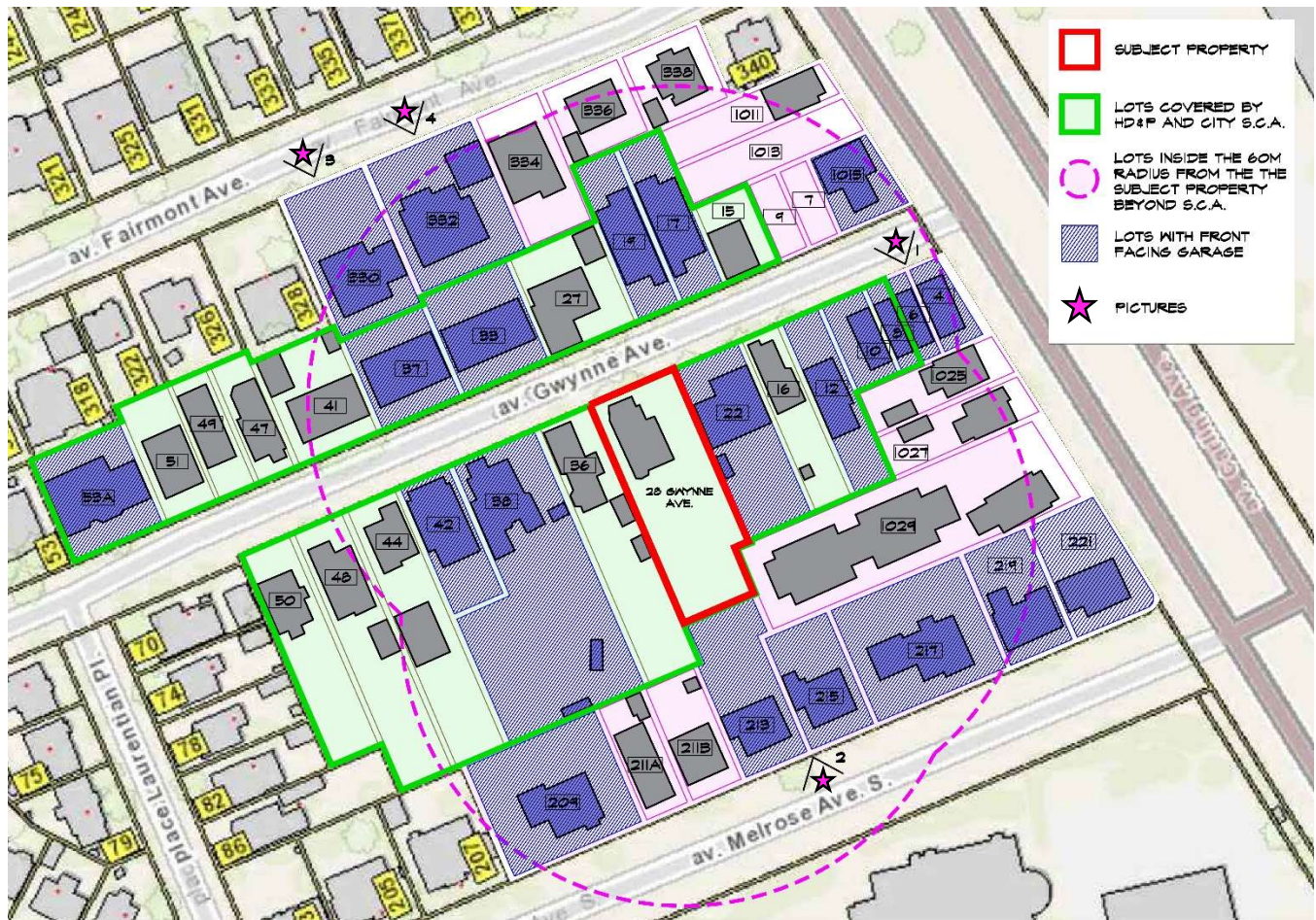


Figure 6

**Figure 6:**

Map of the 60 metre Committee of Adjustment circulation radius, superimposed with the Streetscape Character Analysis map prepared by HD&P, showing the locations of the front facing garages. Within the 60 metre radius, front facing garages are found on a majority of lots (52.5%) which reveals that they are not foreign to the neighbourhood.

	Front Facing Attached Garages	Non Front Facing Attached Garages	Total Lots	Percentage
SCA	9	12	21	43%
CofA 60m RADIUS	21	19	40	52.5%

Figure 7

**Figure 7:**

Tabulates and compares both the Streetscape Character Analysis (SCA) and the 60 metre radius. Although there are, overall, less than 50% front facing garages in the SCA, figure 7 reveals a close mix of front facing garages and non front facing garages throughout the wider neighbourhood.

**Examples of lots with front facing garages within a 60 meter radius of the subject property:**



Figure 8

**1) 4, 6 Gwynne Avenue**

More than half the elevation width  
 Front & centered  
 Plain garage door



Figure 9

**2) 213, 215 Melrose Avenue**

Setback from the front face  
 One storey  
 Plain garage door

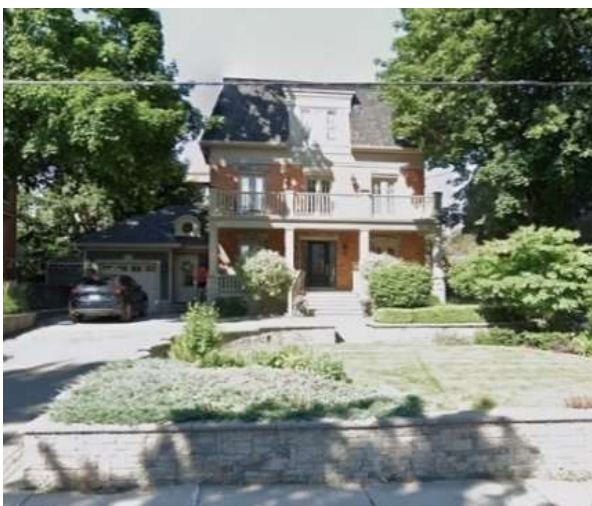


Figure 10

**3) 330 Fairmont Avenue**

Setback from the front face  
 One storey  
 Partially glazed garage door  
 Man door with trim details



Figure 11

**4) 332 Fairmont Avenue**

Prominent front-facing double garage and ahead of the dwelling's front face  
 One storey  
 Partially glazed garage doors and trim work  
 Double wide driveway

**Figures 8-11:**

Illustrations of front facing-garage lots and their characteristics described. Some are located at the front wall (*figure 8*), ahead of the front wall (*figure 11*), and beyond the front face of the dwelling (*figures 9 and 10*) which have the least prevalence on the street.

**Summary of Neighbourhood Context:**

The neighbourhood context map (*Figure 6*) shows the prevalence of front facing garage lots (52.5%, as tabulated in *figure 7*), in the 60 m Committee of Adjustment circulation radius (see *additional photos in Figures 8, 9, 10 and 11*) as well as HD&P's 21-lot Streetscape Character Analysis. There is a majority of front facing garage lots in the 60 m radius area, which shows they are not foreign to the neighbourhood. The proposed design of the house and garage will positively contribute to the neighbourhood's character in the immediate vicinity. In the proposed application, the garage is one storey, is highly set back from the front face of the dwelling, and is detailed similarly to the rest of the house, minimizing its impact.

### The Four Tests:

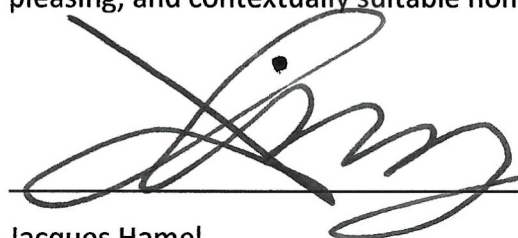
- 1- The proposed detached home with an attached front garage and a driveway meets the intent of the Official Plan as it is a permitted use in the general urban area.
- 2- The proposal meets the intent of the zoning bylaw, and contributes to the neighbourhood's mixed character. The home's design meets or exceeds all the performance requirements for a detached house, and is consistent with the area's established context, blending a modern family home with the surrounding properties.
- 3- The requested variance is minor and maintains alignment with the neighbourhood's dominant streetscape character. The Streetscape Character Analysis reveals that only 2 additional lots would have been needed to meet the dominant pattern requirement (9 out of 21 lots have front-attached garages instead of 11). The deviation is minor and does not significantly impact the overall character of the neighbourhood. Furthermore, the larger streetscape character of 40 lots (60 m radius) has 21 front facing garages which would be the dominant majority.
- 4- The impact of the proposed development is desirable for the neighbourhood. An analysis of neighbouring properties within a 60m radius as well as the streetscape character analysis show a mix of existing front-attached garages and front-prominent garages. The impact of an additional front facing garage would be minimal.

The proposed front facing attached garage is integrated in a way that minimizes its prominence, softened by recessing it from the porch, the second floor, and the other front facing design elements, such as porch columns, walkways, and canopy.

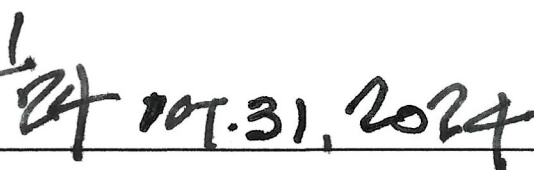
The finer design expression of the garage harmonizes and integrates the garage with the overall dwelling's expression, reducing its prominence, avoiding a harsh or imposing appearance and ensuring the proposed home's positive contribution to the local character.

### Conclusion

In our opinion, the four tests have been satisfied, and we urge the Committee of Adjustment to approve the requested minor variance, as it supports the creation of a family-oriented, aesthetically pleasing, and contextually suitable home.



Jacques Hamel  
Hamel Design & Planning



Date