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# **1660 Merivale Road**

## **Applications for Site Plan Control and Minor Variance**

### **Planning Rationale**

**February 2025**

**Prepared by:**

**Parsons Inc.**

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## 1.0 INTRODUCTION AND SITE DESCRIPTION

This Planning Rationale has been prepared by Parsons Inc. on behalf of Harnois Énergies in support of Minor Variance and Site Plan Control Approval applications for the property at 1660 Merivale Road (“site”) in the City of Ottawa (“City”) (**Figure 1-1**). Previous applications and meetings related to this proposed development are listed below. The submission comments received are provided in **Appendix B**.

- Phase 1 Pre-Consultation meeting – May 26, 2023
- Phase 2 Pre-Consultation meeting – November 6, 2023
- Phase 3 Pre-Consultation submission – March 14, 2024

A Minor Variance and Site Plan Control Approval are being sought at this time to enable the proposed redevelopment of a portion of the existing gas bar on the property. This Planning Rationale has been prepared as a supporting document to these applications and is intended to assist the City of Ottawa in evaluating the proposal in the context of applicable Provincial and Municipal land use policies. The proposed developed is being supporting by additional plans and studies submitted under separate cover.

**Figure 1-1. Site Location**





## 2.0 SITE DESCRIPTION AND SURROUNDING USES

The site is located at 1660 Merivale Road in the City of Ottawa, legally described as PART OF LOT 30 CONCESSION 1 (RIDEAU FRONT). The rectangular shaped property has a total site area of approximately 6,603.3 m<sup>2</sup> (0.66 ha), and frontages of 91.61m along Merivale Road, 72.72m along Viewmount Drive, and 91.49m along Glenmanor Drive. Vehicle access to the property is provided through two all-movement driveways from Merivale Road and one all-movement driveway from Viewmount Drive (**Figure 2-1, Figure 2-2**). A pedestrian pathway is provided from Glenmanor Drive to the existing gas bar.

The site currently contains a gas bar featuring a pumping island and two buildings, one containing a car wash and one containing a convenience store. Fuel storage tanks are located in the northeast portion of the property. Parking areas are provided along the north and west property boundaries and to the east of the convenience store, and a storage shed and waste area are located in the west portion of the property. A large grassy landscaped area with large mature trees is located at the south of the property.

The surrounding land uses are depicted in **Figure 2-3** and are as follows:

- **North** – Viewmount Drive; a restaurant and parking area associated with Merivale Mall across Viewmount Drive
- **South** – Low-density residential (single and semi-detached buildings)
- **East** – Merivale Road; Merivale Intermediate/High School across Merivale Road
- **West** – Glenmanor Drive; low-density residential (single and semi-detached buildings) across Glenmanor Drive

Figure 2-1. View from Merivale Road facing Northwest (Google Streetview 2023)



Figure 2-2. View from Viewmount Drive facing South (Google Streetview 2023)





Figure 2-3. Surrounding Land Uses



### 3.0 DEVELOPMENT PROPOSAL

#### 3.1 Site Plan Overview

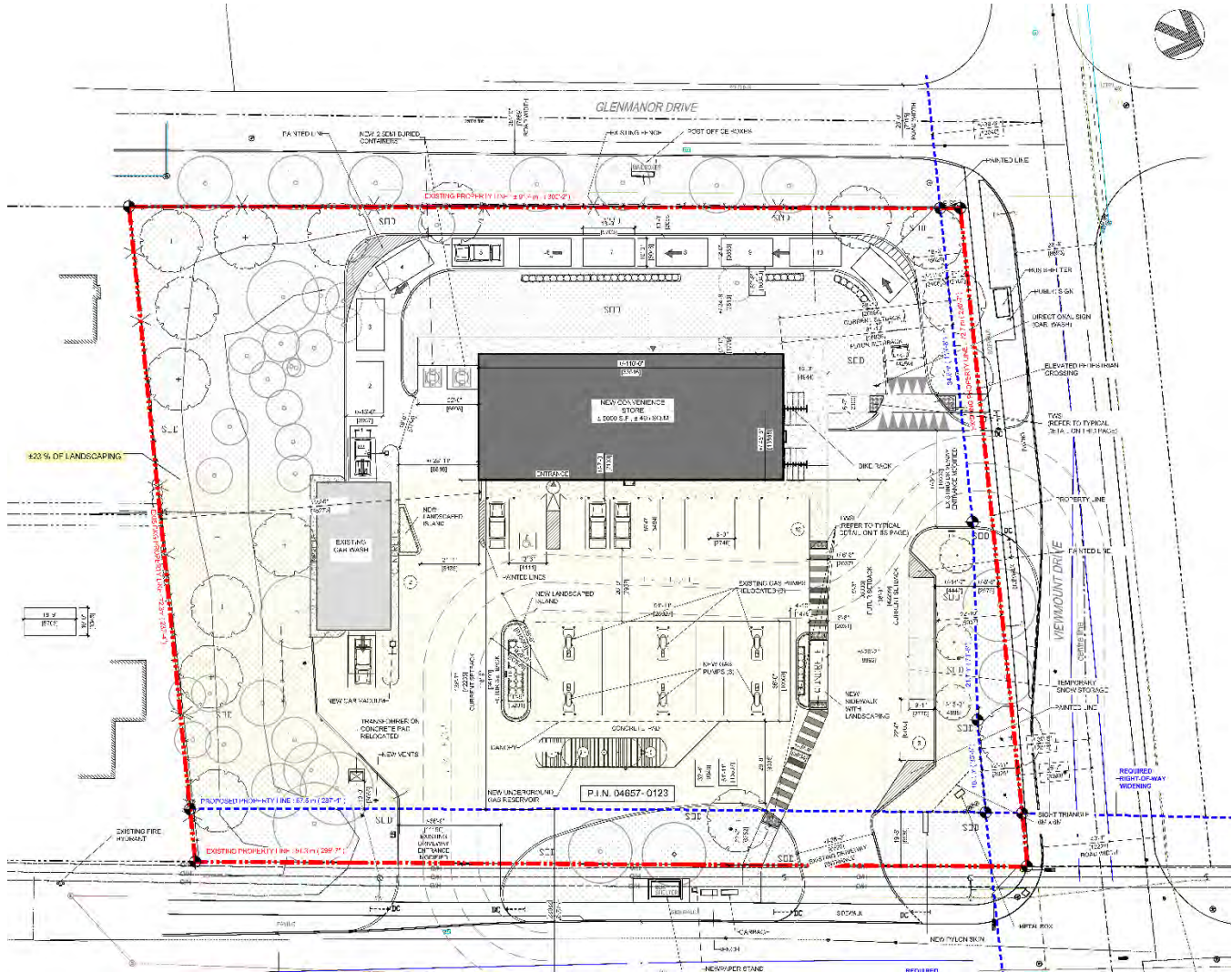
The current development proposal seeks to permit the redevelopment/intensification of the property, which includes the following modifications to the site. There are no proposed changes to the existing car wash use/building.

- Construction of a single-storey commercial main building with a gross floor area of 445 sqm for convenience store uses;
- Repositioning of the car wash queue lane to the west of the property and provision for additional landscaping along Glenmanor Drive;
- Relocation of the pump island to the east side of the property and addition of three new gas pumps;
- Relocation of the existing underground gas tanks between the pump islands and Merivale Road right-of-way;
- New in-ground waste storage containers south of the new building;
- Removal of the existing storage shed;
- Improved pedestrian pathway connections from Viewmount Drive and Merivale Road, through the property, to the new main building;
- Landscaped areas throughout the property, including increased buffer areas with new trees along Merivale Road and Viewmount Drive;
- Modifications to the south-most Merivale Road and Viewmount Drive access points;
- 17 vehicle parking spaces provided throughout the property, inclusive of one barrier-free space;
- 14 bicycle parking spaces located along the Viewmount Drive end of the main building; and
- Right-of-way dedication along Merivale Road and Viewmount Drive.



The proposed Site Plan is shown in **Figure 3-1** and the site statistics are provided on the attached overall Site Plan for the property (**Appendix A**).

**Figure 3-1: Proposed Overall Site Plan**



### 3.2 Proposed Variances

The proposed development requires variances to Zoning By-law 2008-250 to increase the required maximum front yard setback for gas bar uses and the required amount of glazing and customer entrances. The requested variances are as follows:

1. Exception from Section 186.10 (h) where a minimum of 50% of the surface area of the ground floor façade, measured from the average grade up to a height of 4.5 metres, facing a public street must be comprised of transparent glazing and active customer entrance access doors and where the proposal provides 52% glazing and one active customer entrance facing Merivale Road (the Arterial Mainstreet) however 0% glazing or active customer entrances are provided along the Viewmount Drive or Glenmanor Drive public streets.
2. Exception from Section 186.10 (i) where a maximum front yard setback of 3 metres is required to any building containing a gas bar whereas 42.29 metres is proposed.

An overview of the site statistics and applicable zoning provisions are provided in **Section 4.3**. The proposed minor variances are further discussed in **Section 5.0**.

## 4.0 PLANNING RATIONALE

This Planning Rationale has been prepared to review the provincial and municipal land use planning policies that guide development on the site. It provides our professional land use planning opinion on compliance with applicable policy and is intended to be of helpful assistant to the City in reviewing this application.

### 4.1 Provincial Policy Statement

The Provincial Policy Statement (PPS), issued under Section 3 of the Planning Act (revised 2020), provides policy direction on matters of Provincial interest and sets the rules for land use planning in Ontario. It includes policies on managing growth, use and management of natural resources, protecting the environment, and public health and safety. The Planning Act requires that decisions affecting planning matters “shall be consistent with” policy statements.

The proposed commercial development aligns with the following policies of the PPS:

#### 1.1.1 Healthy, liveable and safe communities are sustained by:

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;
- e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;

#### 1.1.3.1 Settlement areas shall be the focus of growth and development.

#### 1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- a) efficiently use land and resources;
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
- g) are freight-supportive.

1.1.3.6 New development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.

#### 1.3.1 Planning authorities shall promote economic development and competitiveness by:

- a) providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;
- b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;
- c) facilitating the conditions for economic investment by identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment;

#### 1.7.1 Long-term economic prosperity should be supported by:

- a) promoting opportunities for economic development and community investment-readiness;
- c) optimizing the long-term availability and use of land, resources, infrastructure and public service facilities;

**Planning Response:** The proposal to redevelop/intensify the site supports broad principles of directing growth and development to suitable locations and fostering liveable communities. The development contributes to the long-term economic success of the site and to sustaining healthy, safe and liveable communities through promoting efficient redevelopment of an existing commercial property close to existing residential neighbourhoods. The proximity to nearby residential uses, as well as improved on-site pedestrian pathways and connections to public pathways enable convenient access to the convenience store uses by walking and cycling. The development makes use of existing municipal services currently provided to the site, eliminating the need for costly municipal infrastructure system expansions or updates.

The minor variances proposed permits development that better utilizes the available commercial land while enhancing and expanding landscape areas and preserving flexibility for potential future uses of the property.

The accompanying reports demonstrate that the use can be accommodated without risk to the natural environment or physical environment.

It is our opinion that the development proposal is consistent with the Provincial Policy Statement.

## 4.2 City of Ottawa Official Plan (2022)

The City of Ottawa Official Plan (OP) (2022) provides goals, objectives and policies and outlines a comprehensive land use policy framework that guides the growth and development within the City to the year 2046. The OP is organized into 13 sections, with the applicable sections and relevant planning responses provided below.

**Section 2 – Strategic Directions.** Section 2 of the OP provides ‘The Big Policy Moves’ which propose five broad policy directions that serve as the foundation for the City’s intention of becoming the most liveable mid-sized city in North America. The five Big Policy Moves are:

1. *Achieve, by the end of the planning period, more growth by intensification than by greenfield development.*
2. *By 2046, the majority of trips in the city will be made by sustainable transportation.*
3. *Improve our sophistication in urban and community design and put this knowledge to the service of good urbanism at all scales, from the largest to the very small;*
4. *Embed environmental, climate and health resiliency and energy into the framework of our planning policies;*
5. *Embed economic development into the framework of our planning policies.*

This section also provides ‘Cross Cutting Themes’ which identify themes that span and apply to implementation policies, plans, and other practices required to achieve goals set out in the OP. These themes are essential to the achievement of a liveable city. The Cross Cutting Themes are:

- Intensification
- Economic Development
- Energy and Climate Change
- Healthy and Inclusive Communities
- Gender Equity
- Culture

**Section 3 – Growth Management Strategy.** Section 3 of the OP provides that significant increases in employment are expected within the 2046 planning timeframe and that areas within Outer Urban Corridors will see their role increase significantly as places for employment growth. Further, a policy intent of the growth management strategy is to establish a framework that maintains a greater amount of employment inside the Greenbelt versus outside the Greenbelt. Relevant policies include:

3.1.3) *The urban area and villages shall be the focus of growth and development.*

3.5.2) *Employment uses within Neighbourhoods are service oriented and are limited in size or area. These uses tend to be office-based, institutional or in the retail sector.*

**Section 4 – City-Wide Policies.** Section 4 of the OP provides City-wide policies that are organized into 11 different topics. An overview of the policy topics that are relevant to the proposed development are provided as follows.

**4.1 Mobility** – Transportation policies within the OP reflect the City’s commitment to more equitable, safe, and healthy communities and increase the reliance of space and cost-effective modes of transportation to accommodate projected



growth. The City intends to take a more deliberate approach to allocation of space for automobiles and prioritize transit and active transportation through: increasing safe and convenient access to transit stops along the Transit Priority Network; using right-of-way widening to improve the public realm in Design Priority Areas; and providing supportive, attractive facilities, including bicycle parking. Merivale Road is designated as a Corridor – Mainstreet within a Design Priority Area per OP Schedule C7A (**Figure 4-2**), and as a Transit Priority Corridor per OP Schedule C2 (**Figure 4-1**). The City intends to minimize the total amount of land used for parking and may permit the reduction or elimination of minimum parking rates in areas designated as *Hubs* and *Corridors* or areas within a 300m radius of a transit stop along a *Transit Priority Corridor*. A transit stop is currently located in front of the property and this site plan provides an enhanced connection from the stop to the commercial uses on the property as well as excess bicycle parking spaces located conveniently beside the main building.

Figure 4-1. Transit Network (Schedule C2)

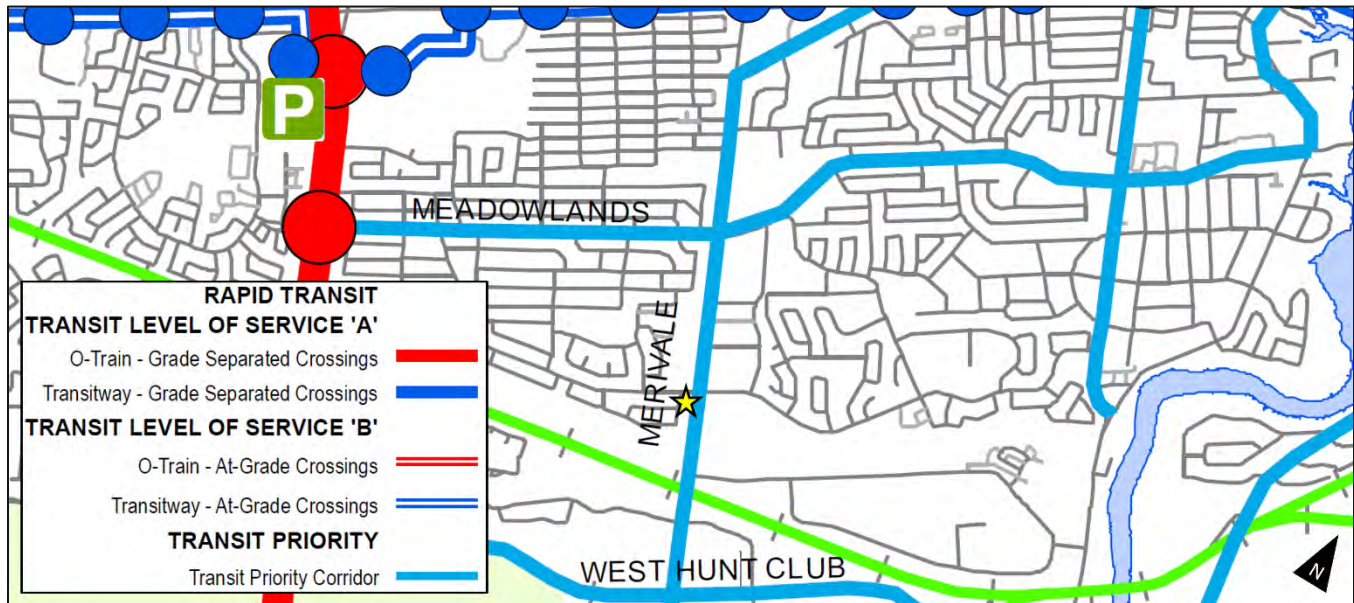
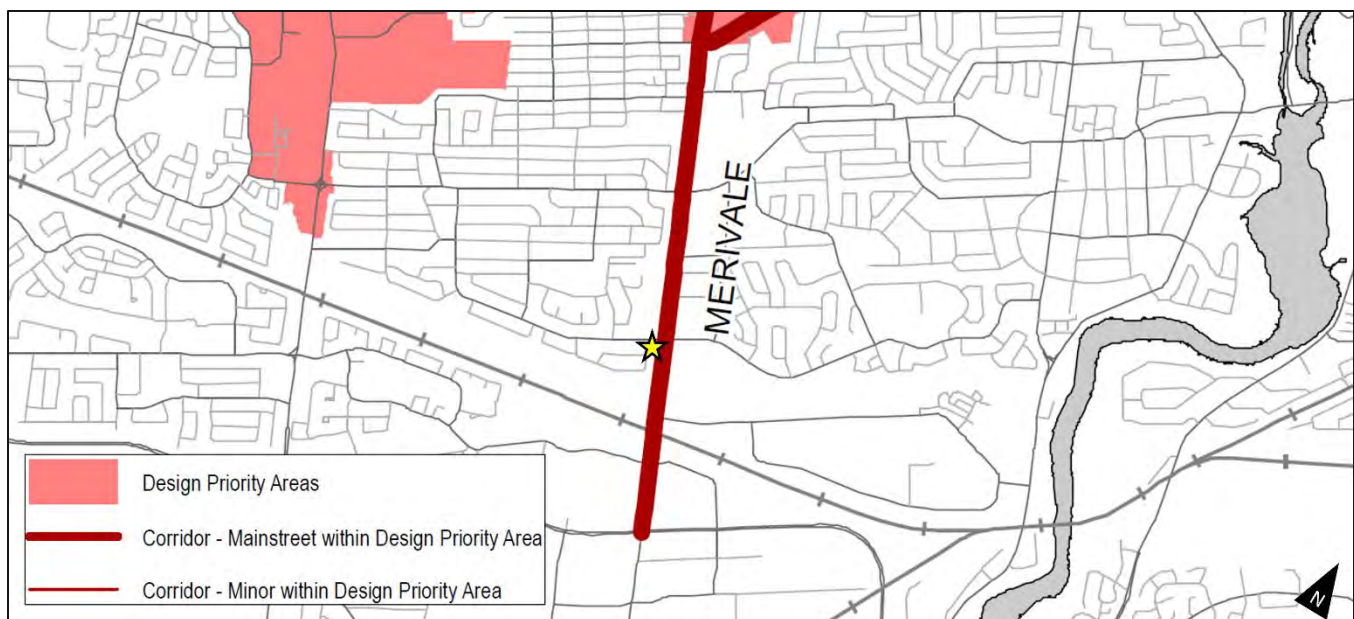


Figure 4-2. Design Priority Areas (Schedule C7-A)



**4.6 Urban Design** – Urban Design provides form and context to the City's buildings, landscapes, pathways, parks, and streets that connect people to places and the community. The OP identifies three Design Priority Area tiers that set the expectation for public realm investment, place keeping, and built form excellence that contribute to the overall City image. The third tier relates to Mainstreet Corridors which define the image of the City at the local level. These areas are

characterized by neighbourhood commercial streets and are intended to provide a high-quality pedestrian environment. Developments are required to minimize conflict between pedestrians and vehicles and reduce the visual impact of servicing, mechanical, and vehicle-oriented areas from the public realm through the use of landscaping areas and vegetation buffers.

**4.8 Natural Heritage, Greenspace and the Urban Forest** – The OP highlights the benefits of the natural heritage system and greenspace network in providing climate change resiliency and improving community health and wellness. The OP also recognizes the contribution of all natural features and areas to the health of the environment regardless of their inclusion in the natural heritage system. Trees provide many of these desirable benefits but continue to experience pressures from multiple sources including growth and development. Natural heritage policies require new development to maintain the urban forest canopy through preserving space and appropriate soil volumes for mature, healthy trees on private and public property. The proposed landscape plan increases the canopy cover on the site from 16% to 47.1%.

**Planning Response:** The proposed development maintains the Official Plan strategic directions that seek to reduce greenfield development and improve urban and community design. The proposed site plan provides improved pedestrian access to the uses on the property through well-defined pedestrian pathways that connect to public sidewalks as well as the bus shelter/stop located along Merivale Road. The proposed plan also provides increased landscaped areas that accommodate buffering vegetation and trees surrounding the building, the drive-through aisle to the car wash, and along the three street frontages: Merivale Road, Viewmount Drive and Glenmanor Drive. The development is located within the urban area and provides service-oriented employment opportunities while taking advantage of available space in an existing commercial property. Located adjacent to an existing transit stop, the proposed development provides an enhanced pedestrian connection to the stop, parking for bicycles, and sufficient parking for uses proposed on site. The proposed development also accommodates future right-of-way widenings by dedicating unencumbered lands along Merivale Road and Viewmount Drive. The provided landscape plan outlines the plantings accommodated on the site and the contribution of this redevelopment to the city's objective to increase the urban forest canopy.

**Section 5 – Transects.** Section 5 of the OP highlights the organization of the City into six concentric policy areas called transects, which represent different gradations in the type and built environment and planned function of the lands within. The site is located within the *Outer Urban Transect* (**Figure 4-3**). Within this transect, the site is designated Neighbourhood with an *Evolving Neighbourhood Overlay*, and adjacent to a *Corridor – Mainstreet* (**Figure 4-4**).

The *Outer Urban Transect* is comprised of neighbourhoods inside the Greenbelt that represent classic suburban built form. The intentions of OP policies for this transect include:

- Recognizing a suburban pattern of built form and site design that is required to be low-rise within *Neighbourhoods*, and generally mid- to high- rise along *Mainstreets* where property sizes permit adequate transitions to adjacent low-rise areas.
- Enhancing mobility options and street connectivity and taking opportunities to improve convenience for walking, cycling, and public transit, while acknowledging the reality of existing automobile-dependent built form.
- Providing height and parking direction to *Hub* and *Corridor* lands located within the transect, where parking minimums in these areas may be reduced or eliminated and sites of sufficient size that front a *Mainstreet* maintain specific building heights.
- Providing direction to *Neighbourhood* lands within the transect to accommodate residential growth that transitions away from a suburban model towards urban built forms.

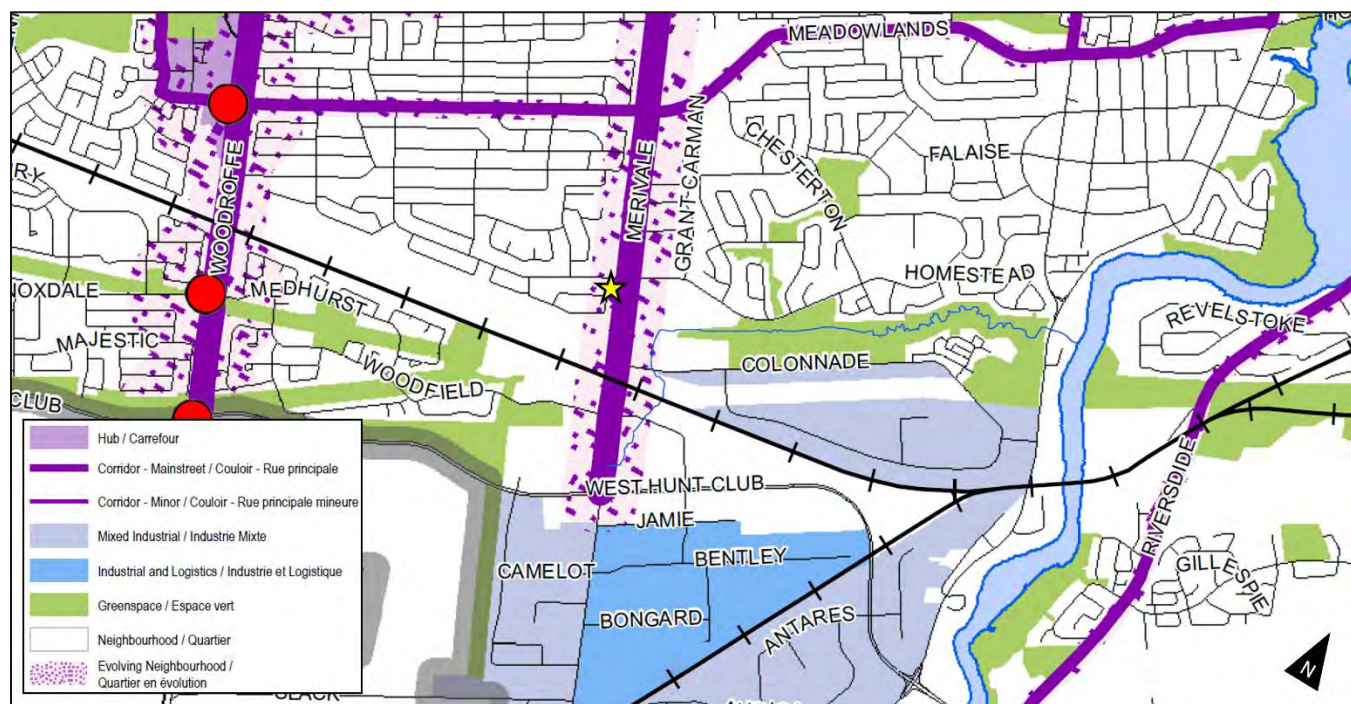
Overlays within the OP are applied to specific lands and are intended to complement underlying designations by providing additional policy direction regarding land uses and built form. The *Evolving Neighbourhood* overlay is applied to areas close to *Hubs* and *Corridors* to signal a gradual character evolution from suburban to urban built forms and functions.



Figure 4-3. Transects Area (Schedule A)



Figure 4-4. Outer Urban Transect Designations (Schedule B3)



**Section 6 – Urban Designations.** Section 6 of the OP provides policy for the different urban designations within the city. The site is located within the *Neighbourhoods* designation, abutting a *Corridor-Mainstreet* designation (**Figure 4-4**).

The *Corridors* designation applies to bands of land along specified streets whose function is to combine higher density development, a greater mix of uses, and higher-level street transit services. Lands within the *Corridor* designation are further categorized as *Mainstreet Corridor* and *Minor Corridor*. The *Neighbourhood* designation applies to contiguous urban areas that represent the heart of communities. While these areas can be varied in their stage of development, the intent of the OP is to guide these areas towards functioning as a 15-minute neighbourhood. Pertinent *Corridor* and *Neighbourhood* policies applicable to the proposed development are provided below.

6.2.1.1) *Corridors* are shown as linear features in the B-series of schedules. The *Corridor* designation applies to any lot abutting the *Corridor*, subject to:



a) Generally, a maximum depth of:

i) In the case of Mainstreet Corridors, a maximum depth of 220 metres from the centreline of the street identified as a Mainstreet Corridor;

6.2.2.1) In the Mainstreet Corridor designation, this Plan shall permit a mix of uses including offices. These uses are permitted throughout the building, however the Zoning By-law may require active commercial or service uses on the ground floor, which include those that support cultural development in order to maintain, extend, or create a continuous stretch of active frontages along a Mainstreet.

6.3.1.2) Permitted building heights in Neighbourhoods shall be Low-rise, except:

a) Where existing zoning or secondary plans allow for greater building heights; or

b) In areas already characterized by taller buildings.

**Planning Response:** The proposed development provides a refreshed opportunity for service and commercial uses along a Mainstreet Corridor in a building that reflects the low-rise, built form of the surrounding Neighbourhood area. The development within walking distance of residential areas while maintaining compatibility with surrounding properties through providing a similar built form and ensuring potential sources of conflict are mitigated. This includes providing sufficient landscape buffering and fencing between the residential areas and maintaining required setbacks for buildings, and waste storage areas. This is further elaborated in supporting studies accompanying this report. The Noise Study completed by Gradient Wind Engineering determined that the proposed development is anticipated to be compatible with existing adjacent noise-sensitive land uses.

The development additionally improves site connectivity to the municipal sidewalk network and to the community through improved pedestrian pathways through the site directly to the main building.

It is our opinion that the development proposal is in general conformance with the associated land use designation policies of the Official Plan.

### 4.3 City of Ottawa Zoning By-law 2008-250

The City of Ottawa Zoning By-law 2008-250 (as amended) is intended to establish and regulate land use by implementing the policies of the City's Official Plan into site-specific development performance standards. The site is currently zoned Arterial Mainstreet Subzone 10 (AM10), as shown in Figure 4-5.

Figure 4-5. Zoning



The intent of the AM zone is to: accommodate a broad range of uses including retail, service commercial, offices, residential and institutional uses in mixed-use buildings or side by side in separate buildings in areas designated Arterial Mainstreet in the Official Plan; and impose development standards that will promote intensification while ensuring that

they are compatible with the surrounding uses. The AM zone permits the following non-residential uses with the existing and proposed uses bolded and underlined in the list below:

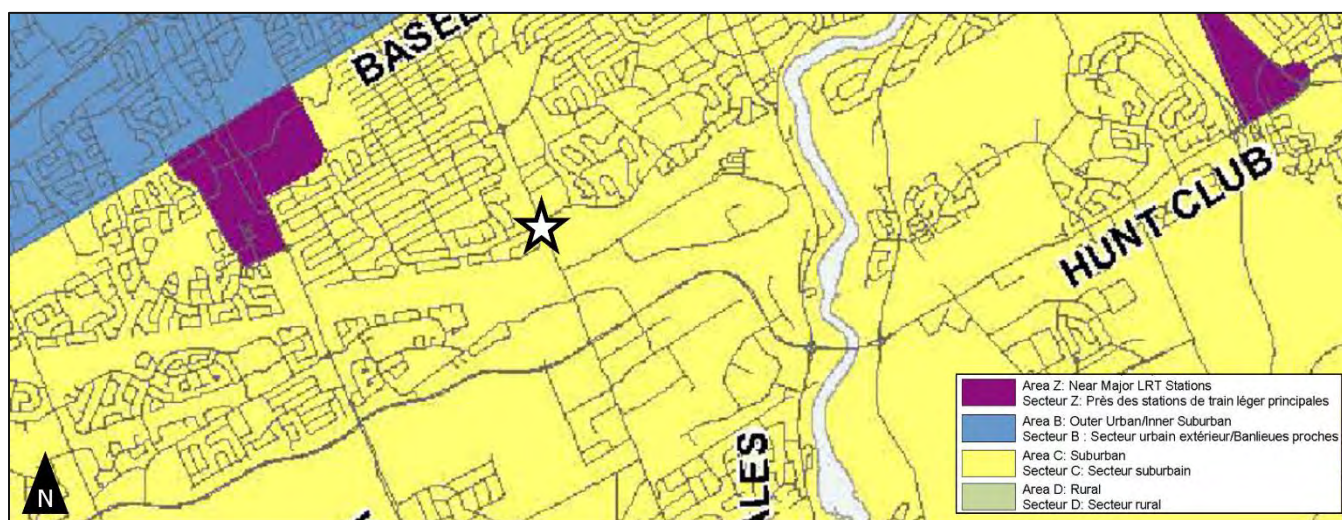
amusement centre	amusement park	animal care establishment
animal hospital	artist studio	automobile dealership
automobile rental establishment	automobile service station	bank
bank machine	bar	broadcasting studio
<b><u>car wash</u></b>	catering establishment	cinema
click and collect facility	community centre	community health and resource centre
<b><u>convenience store</u></b>	day care	diplomatic mission
drive-through facility	emergency service	funeral home
<b><u>gas bar</u></b>	hotel	instructional facility
library	medical facility	municipal service centre
museum	nightclub	office
park	parking garage	payday loan establishment
personal brewing facility	personal service business	place of assembly
place of worship	post office	production studio
recreational and athletic facility	research and development centre	residential care facility
restaurant	retail food store	retail store
school	service and repair shop	sports arena
storefront industry	technology industry	theatre
training centre	urban agriculture	

The development standards that apply to the AM10 zone and their compliance as part of the proposed development are provided in **Table 4-1** below.

**Table 4-1. Zoning Compliance Table**

Sec.	Zoning Provision	Required	Proposed Development	In Compliance (Yes/No)
185.3	(a) minimum lot area	No minimum	6,603.3m <sup>2</sup>	Yes
	(b) minimum lot width	No minimum	91.5m	Yes
	(g) maximum floor space index	No maximum	0.1	Yes
186.10	(c) minimum interior side abutting residential zone Beyond 20m back from street	7.5m	15.9m	Yes
	(d) minimum rear yard Building within 20m of lot line abutting public street	3.0m	16.1m	Yes
	(h) minimum ground floor façade consisting of transparent glazing/active customer entrance access (building walls facing public streets)			
	Merivale Road	50%	52%	Yes
	Viewmount Drive	50%	0%	No
	Glenmanor Drive	50%	0%	No
	(i) maximum front yard (gas bar uses)	3.0m	Existing Property Line: 42.2 m Future Widening: 36.2 m	No
110	(3) Minimum setback of refuse collection and refuse loading areas			
	(a) From lot line abutting public street	9.0m	17.4m	Yes
	(3c) Minimum screening of refuse collection Opaque screen or soft landscaping (in-ground container)	2.0m	2.0m (Soft landscaping)	Yes

Figure 4-6. Area for Minimum Parking Space Requirements



Part 4 of the Zoning By-law outlines the provisions for Parking, Queuing, and Loading. The Site is currently located within Area C (Suburban) of Schedule 1A (**Figure 4-6**), which identifies areas for minimum parking space requirements, which are identified in **Table 4-2**.

Table 4-2. Required Parking Spaces per Existing and Proposed Uses

Vehicle Parking (Section 101)	Proposed Gross Floor Area	Required Parking Ratio	Number of Spaces Required
Convenience Store	445m <sup>2</sup>	3.4 spaces/100m <sup>2</sup>	15
Car Wash/Gas Bar	209m <sup>2</sup>	None	0
TOTAL SPACES REQUIRED			15 parking spaces
TOTAL SPACES PROVIDED			17 parking spaces
Bicycle Parking (Section 111)	Proposed Gross Floor Area	Required Parking Ratio	Number of Spaces Required
Convenience Store	445m <sup>2</sup>	1 space/250m <sup>2</sup>	2
Car Wash/Gas Bar	209m <sup>2</sup>	1 space/1500m <sup>2</sup>	0
TOTAL SPACES REQUIRED			2 parking spaces
TOTAL SPACES PROVIDED			14 parking spaces

**Planning Response:** The proposed development provides convenience store uses permitted in the AM zone in a new building on the property. The development meets all zoning provisions, with the exception of maximum front yard setback for gas bar uses and minimum surface area composition for ground floor facades along Viewmount Drive and Glenmanor Drive however provides the required glazing and entrance to the Arterial Mainstreet (Merivale Road).

The proposed development requires relief from the required front yard setback and ground floor façade provisions. Minor variances are being requested to accommodate these zoning deficiencies and are further discussed in **Section 5.0**.

It is our opinion that the development proposal is in general conformance with the zoning requirements and policies of the Zoning By-law.



## 5.0 MINOR VARIANCE TESTS

As prescribed in Section 45 of the *Planning Act*, applications for minor variance seeking relief from terms of a Zoning By-law are required to demonstrate compliance with the four tests, which are:

- Is the application desirable for the appropriate development or use of the land, building, or structure?
- Is the application in keeping with the general intent and purpose of the Official Plan?
- Is the application in keeping with the general intent and purpose of the Zoning By-law?
- Is the application minor in nature?

A sketch depicting the proposed Minor Variance as required by *O.Reg 200/96*, is provided in **Appendix C**, and an analysis demonstrating compliance with the four tests of the proposed variances is provided below.

Variance No. 1: Increased Maximum Front Yard Setbacks	
1. Is the application desirable for the appropriate development or use of the land, building, or structure?	<p>Recognizing that some existing buildings (Car Wash) are to remain as part of this proposal, the proposed variance would permit the location of the proposed convenience store building towards the rear of the property to allow the relocation of the underground storage tanks and movement of fuel delivery trucks through the site and away from the residential neighbourhood to the rear of the property (along Glenmanor Drive). The intention of providing a maximum front yard setback is to enhance the public realm and create an engaging streetscape. This intention is further implemented through the widened landscape areas provided along Merivale Road and Viewmount Drive and provides safe and direct sidewalk connections from the proposed building to the municipal sidewalks.</p> <p>On this basis, it is our opinion that the variance is desirable for the development and use of land.</p>
2. Is the application in keeping with the Official Plan?	<p>The Official Plan provides goals, objectives, and policies that guide growth and physical land use within the City. Policies relating to the <i>Outer Urban Transect</i> outline requirements for maintaining compatibility with surrounding neighbourhood areas and providing for active transportation and transit while acknowledging the car-dependent tendencies of these areas. Additionally, Design Priority Areas set expectations for place making and built form excellence that provide high-quality pedestrian environment. The proposed variance maintains this intention by providing landscaping that buffers on-site activity from the public realm to reduce potential impacts and conflicts and provides defined pedestrian pathways to the property that enhances the pedestrian environment and connectivity to nearby public transit and the adjacent municipal sidewalk network and surrounding residential community.</p> <p>Additionally, the proposed variance permits relocation of the underground storage tanks, required to accommodate the required right-of-way dedication along Merivale Road and Viewmount Drive, to an area easily accessible by fuel delivery trucks.</p> <p>On this basis, it is our opinion that the variance is in keeping with the general purpose and intent of the Official Plan.</p>
3. Is the application in keeping with the Zoning By-law?	<p>The intent of the Zoning By-law is to act as an implementation tool to guide growth and development within the City of Ottawa in alignment with the Official Plan. The AM10 zone is intended to provide a range of uses while imposing development standards that support intensification and compatibility with surrounding uses. The proposal includes constructing a new building containing convenience store uses located towards the west portion of the property away from the Merivale Road frontage. The intent of the maximum front yard setback as provided in the Zoning By-law is to enhance the public realm and streetscape. This intention is maintained in the proposed redevelopment through the inclusion of widened landscape areas and well-defined pedestrian pathways that improve connectivity to the public realm and improve the visual appeal of the property frontage. The location of the main building further buffers the gas bar uses from the residential property to the west.</p> <p>On this basis, it is our opinion that the variance is in keeping with the purpose and intent of the Zoning By-law.</p>
4. Is the application minor in nature?	<p>The proposed variance seeks to permit an exception from maximum front yard setback requirements. The increased maximum setback is not anticipated to negatively impact the functionality of the site but rather improve it, buffer nearby residential properties from the gas bar uses, and improve the adjacent public realm. Expanded landscape areas along the property frontage and internal to the development as well as providing improved pedestrian pathways through the site and from public sidewalks act to enhance the streetscapes.</p> <p>On this basis, it is our opinion that the variance can be considered minor in nature.</p>

<b>Variance No. 2: Ground Floor Façade Relief</b>	
1. Is the application desirable for the appropriate development or use of the land, building, or structure?	<p>The property has frontage along three public streets, Merivale Road, Viewmount Drive, and Glenmanor Drive (a residential use facing frontage), with the lot boundary along Merivale Road representing the property's front lot line as the Arterial Mainstreet. The east side of the building facing Merivale Road, which contains the main entrances to the convenience store use, meets the requirements for ground floor façade composition and provide views into the retail spaces. Areas to the north and west of the building will contain operational and employee areas where interior wall surface areas are anticipated to be used for equipment placement and storage. Therefore, providing transparent glazing materials or active customer entrance points along these building sides would reduce the useful and necessary spaces required to operate the use on the site.</p> <p>Further, as the site has three frontages on public streets, the location of one main entrance that is connected to each of the streets through enhanced pedestrian connections provides wayfinding and direct customers to the main entrance.</p> <p>On this basis, it is our opinion that the variance is desirable for the development and use of land.</p>
2. Is the application in keeping with the Official Plan?	<p>The Official Plan provides goals, objectives, and policies that guide growth and physical land use within the City. The property is located along a the Mainstreet Corridor of Merivale Road, which is identified as third tier Design Priority Areas. These areas seek to provide a high-quality pedestrian environment, which includes providing transparent materials to allow for pedestrians to be able to see inside a building. Given that the required ground floor façade composition requirements are met along Merivale Road, the building contributes to the enhancement of the pedestrian environment along the adjacent Mainstreet Corridor. Additionally, the enhancing of landscape areas throughout the property and provision of well-defined pedestrian walkways between the building and the public realm also contribute to the enhancement of the pedestrian environment in keeping with the policies and directions of the Official Plan.</p> <p>On this basis, it is our opinion that the variance is in keeping with the general purpose and intent of the Official Plan.</p>
3. Is the application in keeping with the Zoning By-law?	<p>The intent of the Zoning By-law is to act as an implementation tool to guide growth and development within the City of Ottawa in alignment with the Official Plan and further aims to avoid incompatible developments. The minimum ground floor façade composition requirement is intended to ensure that the building contributes to an engaging pedestrian environment by providing visual interest. The proposed variance is not anticipated to negatively impact the pedestrian environment in the public realm surrounding the property given that the façade composition required is met along Merivale Road, where the majority of pedestrian traffic is likely to be encountered. Additionally, that the pedestrian environment along Glenmanor Drive and Viewmount Drive will benefit from the enhanced landscaped areas and pedestrian pathways provided in the development. Further, the placement of the building serves to buffer the residential uses opposite the site on Glenmanor Road from the commercial and service activities associated with the gas bar use of the site.</p> <p>On this basis, it is our opinion that the variance is in keeping with the purpose and intent of the Zoning By-law.</p>
4. Is the application minor in nature?	<p>The proposed variances seek to provide relief from minimum ground floor façade requirements along Glenmanor Drive and Viewmount Drive, where 50% of the ground floor façade is required to consist of transparent glazing or active customer entrance access for building walls facing public streets. Relief from this provision is not anticipated to negatively impact the contribution of the site to the public realm given that the front of the building facing the Arterial Mainstreet maintains the façade requirement and provides views into the building from the adjacent public realm. Further, the additional landscaping provided along each public street also contributes to the objective of improving the public realm.</p> <p>On this basis, it is our opinion that the variance can be considered minor in nature.</p>

## 6.0 CONCLUSION

This Planning Rationale has been prepared on behalf of Harnois Énergies in support of minor variance and Site Plan Control Approval applications for the property at 1660 Merivale Road in the City of Ottawa. The proposed minor variances include relief from increased maximum front yard setbacks and ground floor façade composition requirements. These variances are requested to support the concurrent application for Site Plan Control for the proposed redevelopment of the existing gas station on the site. Approval of the Site Plan and Minor Variance applications is recommended on the following basis:

### IT IS CONSISTENT WITH THE PROVINCIAL POLICY STATEMENT

The proposed redevelopment is consistent with the broad goals and objectives of the PPS through promoting the efficient use of existing commercial land within a settlement area with access to full municipal services to provide commercial uses in close proximity to residential areas. Supporting studies completed highlight measures taken by the proposed redevelopment to limit impacts to the surrounding properties, land uses, and the natural environment.

### IT DEMONSTRATES COMPLIANCE WITH THE FOUR TESTS

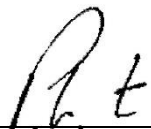
The variances required for the proposed development demonstrate compliance with the four tests set out in the *Planning Act*, in that they have been shown to be minor, desirable for the development, building, and use of land, and are in keeping with policies and objectives of the City of Ottawa Official Plan and Zoning By-law.

On this basis, it is our professional planning opinion that approval of the proposed development will result in good land use planning.

Respectfully Submitted,



Ivan Ho, MPlan  
Planner  
Parsons Ottawa



















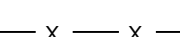




Pamela Whyte, MCIP, RPP  
Manager of Planning  
Parsons Ottawa





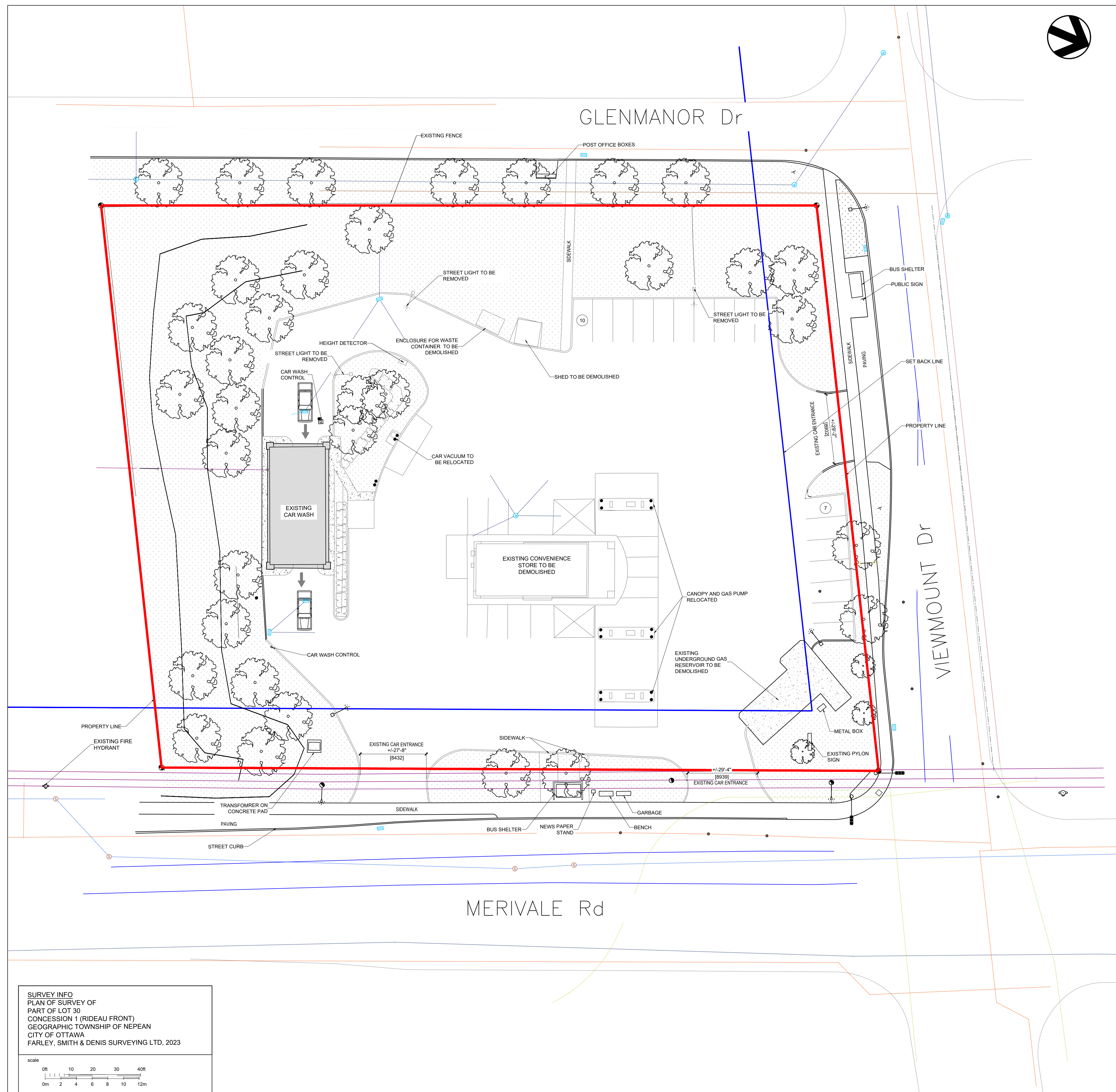
**Appendix A: Proposed Site Plan**

### LEGEND

- |                                                                                   |                           |
|-----------------------------------------------------------------------------------|---------------------------|
|  | GAS LINE                  |
|  | ELECTRICAL AERIAL CABLE   |
|  | UNDERGROUND BELL LINE     |
|  | AQUEDUCT PIPELINE         |
|  | RAIN CONDUCT              |
|  | SANITARY CONDUCT          |
|  | GUY-WIRE                  |
|  | UTILITY POLE              |
|  | GUYED POLE                |
|  | FIRE HYDRANT              |
|  | CIRCULAR GRID             |
|  | STORM SEWER               |
|  | PUBLIC SIGN               |
|  | TRAFFIC LIGHT             |
|  | WATER VALVE COVER         |
|  | STREET LAMP               |
|  | BUMPER POST               |
|  | FENCE                     |
|  | EXISTING TREE             |
|  | EXISTING TREE TO CONSERVE |
|  | NEW TREE TO PLANT         |

NOTES:

PROPERTY BOUNDARY INFORMATION AND PIN DERIVED  
FROM PLAN OF SURVEY DATED APRIL 24, 2023,  
BY FARLEY SMITH & DENIS SURVEYING LTD"



## IMPORTANT

NE PAS MESURER LES PLANS, LIRE LES COTES SEULEMENT. L'ENTREPRENEUR DEVRA VÉRIFIER TOUTS LES DESSINS, DÉTAILS, SPÉCIFICATIONS, DIMENSIONS ET MESURES AINSI QUE LES CONDITIONS DE LOCAUX OU DU CHANTIER, ET SIGNALER TOUTES ERREURS, OMISSIONS ET / OU ANOMALIES AVANT DE COMMENCER LES TRAVAUX. TOUS LES CHANGEMENTS OU SUBSTITUTIONS DE DÉTAILS ET DE MATÉRIAUX SPÉCIFIÉS SUR CE PLAN DEVRONT ÊTRE APPROUVÉS PAR L'ARCHITECTE OU LE DESIGNER.

DO NOT SCALE DRAWINGS, READ DIMENSIONS FIGURES ONLY. CONTRACTOR SHALL VERIFY ALL DRAWINGS, DETAILS, SPECIFICATIONS, DIMENSIONS AND MEASUREMENTS AS WELL AS SITE OR PREMISES CONDITIONS AND NOTIFY THE DESIGNER OF ALL ERRORS, OMISSIONS AND / OR ANOMALIES BEFORE BEGINNING THE WORK. ANY CHANGES OR SUBSTITUTIONS TO DETAILS OR SPECIFIED MATERIALS SHOW ON THIS DRAWING MUST BE APPROVED BY THE ARCHITECT OR THE DESIGNER.

## RÉVISIONS / REVISIONS

[illegible]

ÉQUIPE DE PROJET / TEAM PROJECT :

ARCHITECTURE &amp; DESIGN

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+ design

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Ontario K1J 7T2  
**www.parsons.com**

DOSSIER No. / FILE No.

CHARGÉ DE PROJET :

SIGNÉ PAR

M. RENAUD

MANON RENAUD

CLIENT

HARNOIS NEPEAN

PROJECT / PROJET

CONVENIENCE STORE /RESTO

DISCIPLINE / DEPARTEMENT

ARCHITECTURE

TITRE DU DESSIN / TITL

## EXISTING SITE PLAN

ÉCHELLE / SCALE

SINÉ PAR / DRAWN BY

 $1'' = 20'$ 

N.HANNA

DATE \_\_\_\_\_

252

No SITE






















A-050

1000

Year	1990	1995	2000	2005
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### LEGEND

- |                                                                                   |                           |
|-----------------------------------------------------------------------------------|---------------------------|
|  | GAS LINE                  |
|  | ELECTRICAL AERIAL CABLE   |
|  | UNDERGROUND BELL LINE     |
|  | AQUEDUCT PIPELINE         |
|  | RAIN CONDUCT              |
|  | SANITARY CONDUCT          |
|  | GUY-WIRE                  |
|  | UTILITY POLE              |
|  | GUYED POLE                |
|  | FIRE HYDRANT              |
|  | CIRCULAR GRID             |
|  | STORM SEWER               |
|  | PUBLIC SIGN               |
|  | TRAFFIC LIGHT             |
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|  | STREET LAMP               |
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|  | NEW TREE TO PLANT         |

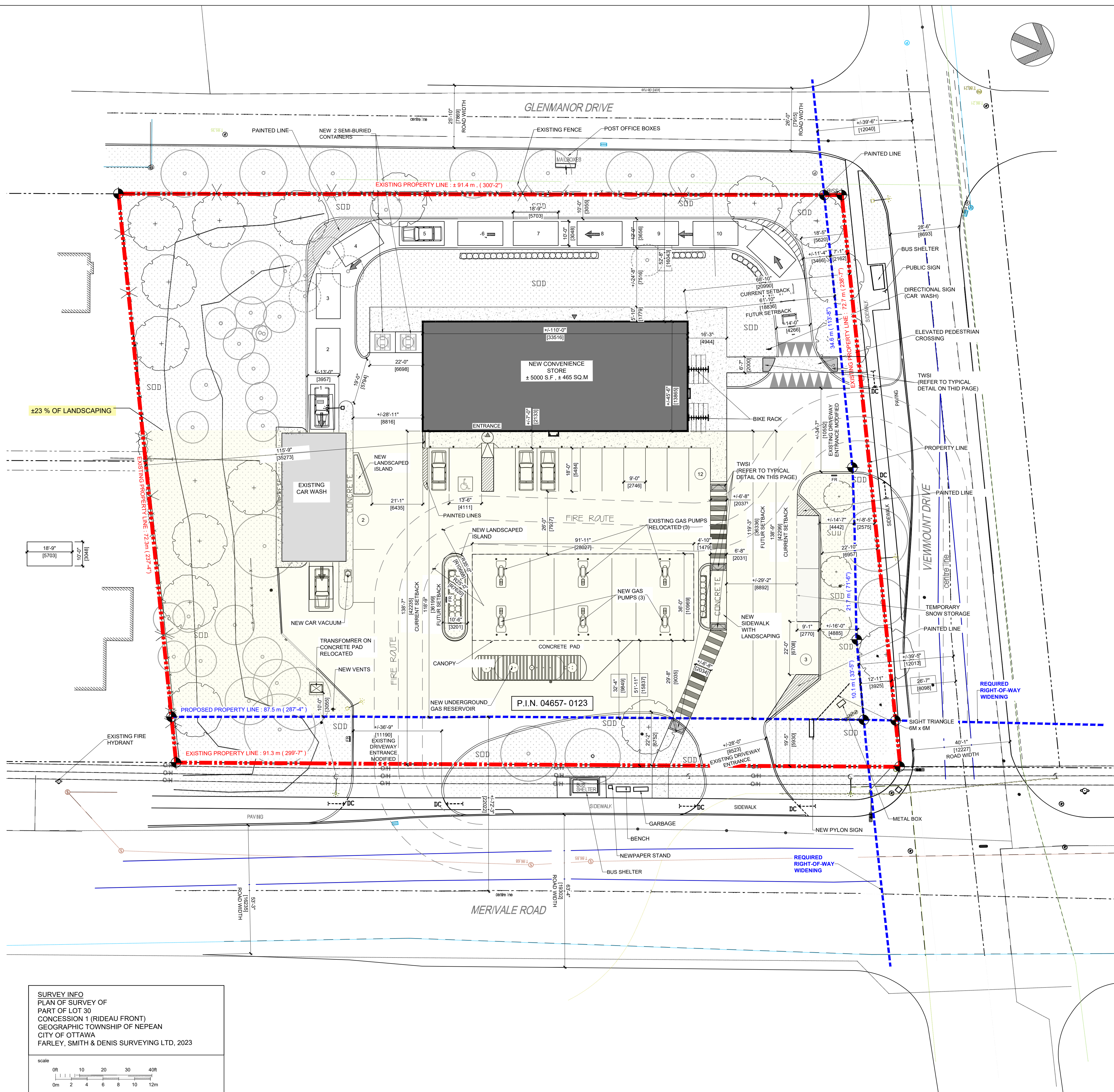
NOTES:

PROPERTY BOUNDARY INFORMATION AND PIN DERIVED  
FROM PLAN OF SURVEY DATED APRIL 24, 2023,  
BY FARLEY SMITH DENIS & SURVEYING LTD"

## ZONING COMPLIANCE TABLE

Sec.	Zoning Provision	Required	Proposed Development	In Compliance (Yes/No)
185.3	(a) minimum lot area	No minimum	6,603.3m <sup>2</sup>	Yes
	(b) minimum lot width	No minimum	91.5m	Yes
	(g) maximum floor space index	No maximum	0.1	Yes
186.10	(c) minimum interior side abutting residential zone Beyond 20m back from street	7.5m	15.9m	Yes
	(d) minimum rear yard Building within 20m of lot line abutting public street	3.0m	16.1m	Yes
	(h) minimum ground floor façade consisting of transparent glazing/ active customer entrance access (building walls facing public streets) Merivale Road Viewmount Drive Glenmanor Drive	50% 50% 50%	52% 0% 0%	Yes No No
	(f) maximum front yard (gas bar uses)	3.0m	36.2m	No
110	(3) Minimum setback of refuse collection and refuse loading areas (a) From lot line abutting public street	9.0m	17.4m	Yes
	(3c) Minimum screening of refuse collection Opaque screen or soft landscaping (in-ground container)	2.0m	2.0m (Soft Landscaping)	Yes

Vehicle Parking (Section 101)	Proposed Gross Floor Area	Required Parking Ratio	Number of Spaces Required
Convenience Store	442m <sup>2</sup>	3.4 spaces/100m <sup>2</sup>	15
Car Wash/ Gas Bar	209m <sup>2</sup>	None	0
		TOTAL SPACES REQUIRED	15 parking spaces
		TOTAL SPACES PROVIDED	17 parking spaces
Bicycle Parking (Section 111)	Proposed Gross Floor Area	Required Parking Ratio	Number of Spaces Required
Convenience Store	442m <sup>2</sup>	1 space/250m <sup>2</sup>	2
Car Wash/ Gas Bar	209m <sup>2</sup>	1 space/1500m <sup>2</sup>	0
		TOTAL SPACES REQUIRED	2 parking spaces
		TOTAL SPACES PROVIDED	14 parking spaces

[illegible]

## IMPORTANT

NE PAS MESURER LES PLANS, LIRE LES COTES SEULEMENT. L'ENTREPRENEUR DEVRA  
VÉRIFIER TOUTS LES DESSINS, DÉTAILS, SPÉCIFICATIONS, DIMENSIONS ET MESURES ANCIENS  
LES CONDITIONS DE LOCAUX OU DU CHANTIER, ET SIGNALER TOUTES ERREURS, OMISSIONS ET  
/ OU ANOMALIES AVANT DE COMMENCER LES TRAVAUX. TOUTS LES CHANGEMENTS OU  
SUBSTITUTIONS DE DÉTAILS ET DE MATÉRIAUX SPÉCIFIÉS SUR CE PLAN DEVRONT ÊTRE  
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## RÉVISIONS / REVISIONS

[illegible]

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+ design

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DOSSIER No. / FILE No.

[illegible]

CHARGÉ DE PROJET :

**SIGNÉ PAR**

M. RENAI

MANON RENAUD

CUENT

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HARNOIS NEPEAN

PROJECT / PROJECT

CONVENIENCE STORE /RESTO

1660 MERIVALE  
NEPEAN, ONTARIO

ARCHITECTURE

TITRE DU DESSIN / TITLE

\_\_\_\_\_

ÉCHELLE / SCALE  
1" = 20'-0"

SINÉ PAR / DRAWN BY  
N HANNA

DATE 27-01-2025

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No SITE	
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A-100

DOSSIER	2304-1010
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**Appendix B: Pre-Consultation Meeting Notes**

April 23, 2024

Ivan Ho  
Parsons Inc.  
Via email: [ivan.ho@parsons.com](mailto:ivan.ho@parsons.com)

**Subject: Phase 3 Pre-Consultation: Review Feedback  
Proposed Site Plan Control Application – 1660 Merivale Road**

Please find below information regarding next steps as well as consolidated comments from the review of the studies and plans submitted in support of the above-noted pre-consultation.

**Next Steps**

1. A review of the materials submitted for the above-noted pre-consultation has been undertaken and staff have identified deficiencies needing to be resolved. Please proceed to complete a Pre-consultation Application Form for another Phase 3 review and submit together with the necessary revised studies and/or plans to [planningcirculations@ottawa.ca](mailto:planningcirculations@ottawa.ca).
2. In your subsequent Phase 3 pre-consultation submission, please ensure that all deficiencies detailed herein are addressed. A detailed cover letter stating how each deficiency has been addressed must be included with the submission materials. Please coordinate the numbering of your responses within the cover letter with the number(s) herein.
3. Please note, responses to any additional comments provided below may be included within the subsequent pre-consultation submission, however, may also be addressed as part of the future formal application submission.

**Planning**

**List of Studies and Plans Reviewed:**

- ☐ **Planning Rationale**, prepared by Parsons Inc. dated March 2024.
- ☐ **Existing Site Plan**, A-102, prepared by MRA Architecture + Design, dated 13-03-2024, revision 1 dated 13-03-2024.
- ☐ **Proposed Site Plan**, A-102, prepared by MRA Architecture + Design, dated 13-03-2024, revision 1 dated 13-03-2024.
- ☐ **Proposed Elevations**, A-102, prepared by MRA Architecture + Design, dated 13-03-2024, revision 1 dated 1-03-2024.



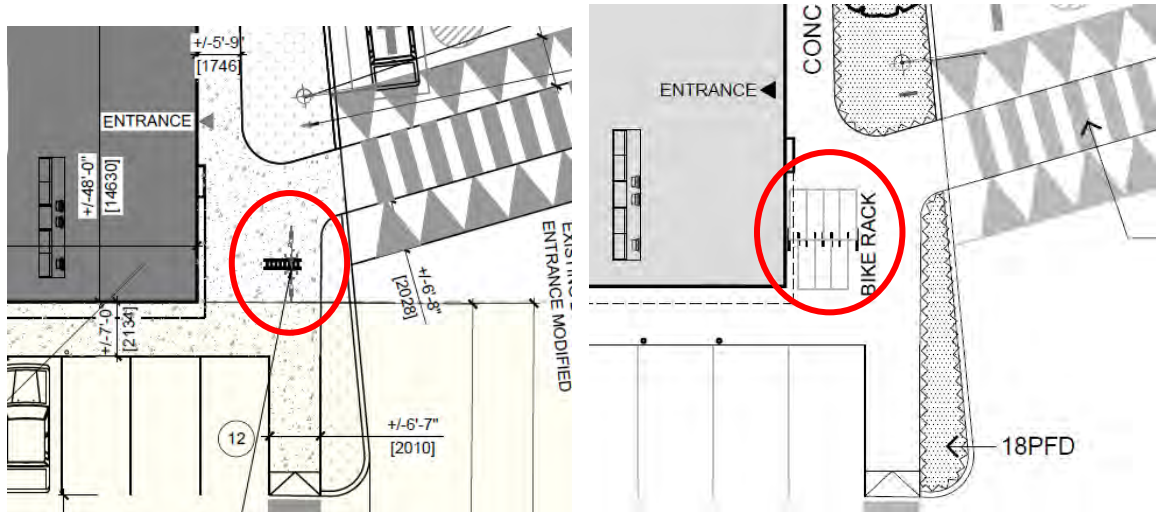
- **Landscape Plan**, L1.01, prepared by Levestek Consultants Inc., revision 1 dated MAR. 11/24.
- **Canopy Cover Plan**, L1.03, prepared by Levestek Consultants Inc., revision 1 dated MAR. 11/24.
- **Plan of Survey**, prepared by Farley, Smith & Denis Surveying Ltd., dated 04-24-23.
- **Zoning Confirmation Report**, dated March 27, 2024.

Deficiencies:

1. It is noted that all the architectural drawings (i.e., Existing Site Plan, Proposed Site Plan, proposed Site Plan Fuel Truck Trajectory, Site Plan Truck Trajectory, Proposed Elevations) have the same drawing number "A-102". Please give each drawing its own unique drawing number as it will be important for the purposes identifying the plan in the future approval report and site plan agreement.

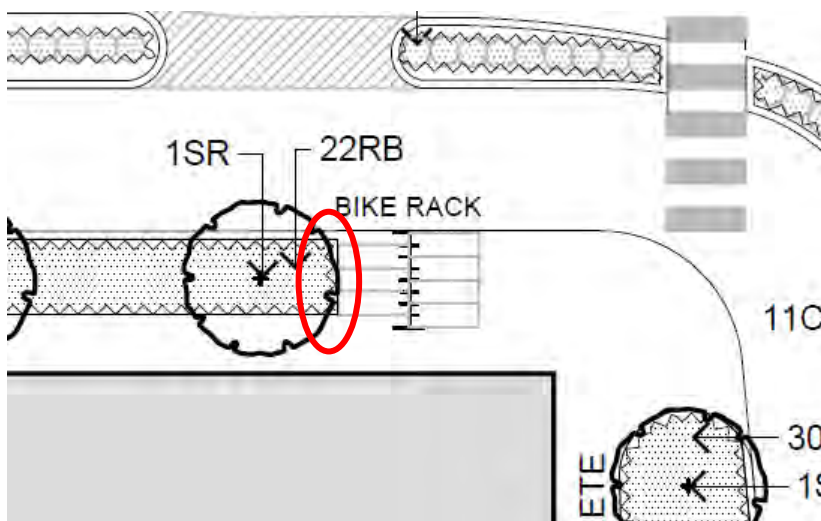
Proposed Site Plan

2. Please include the following information on the plan, per the [Site Plan ToR](#).
  - a. Bar scale and written ratio scale in the Legend.
  - b. Bearings and lengths of all property lines.
  - c. Identify the width of all roads abutting the site.
  - d. Property Identification Number(s) (PINs) or legal description of property.
  - e. A statement of where the property boundary information was derived from.
  - f. Location of existing/proposed fire hydrants, proposed fire route and fire route sign locations
  - g. Snow storage areas.
  - h. Waste management and recycling enclosure design details.
  - i. The location and nature of any easement affecting the subject land.
3. Ensure that the identified locations of bicycle parking align with the locations shown on the landscape plan. It appears that there is discrepancy between the plans for the bike rack located next to elevated pedestrian crossing – see below. Please note that the location shown on the landscape plan is preferred in order to maintain clear pedestrian paths of travel.



### Landscape Plan

4. Provide landscaping around the proposed waste containers to achieve the screening requirement identified in the Section 110(3)(d) of the Zoning By-law. Please note that the proposed waste containers are considered to be accessed via a parking lot and the provisions of Section 110 apply.
5. Show the dimensions of the bicycle parking spaces on the plan. Please refer to Table 111B in the Zoning By-law for minimum width and length requirements.
6. Please ensure that every bicycle parking space is accessed from an aisle with a minimum width of 1.5m, per Section 111(9) of the Zoning By-law. Staff have concerns that this requirement is not being achieved for a portion of the spaces on the bike rack abutting the drive-through. Consider shifting the location of the bike rack 1.5m north (towards Viewmount).



### Zoning Confirmation Report

7. It is noted that there are several parallel parking spaces proposed on the site plan. Please also identify the by-law requirement and proposal for parallel spaces in the “Size Space Section 105 and 106” row in the Zoning Review chart.
8. Identify the applicable screening requirement in the “Refuse Collection Section 110” row – see Section 110(c)-(d) in the Zoning By-law.
9. Add a row and identify the Drive-through Operations Requirements (Section 112) under “Other applicable relevant Provision(s)”.
10. Add additional rows to address the applicable requirements in the AM10 zone.

Comments:

11. [Phase 2 Pre-con, Comment 21] It understood that the applicant does not intend to remove the existing underground fuel storage tanks from the protected right-of-way identified in Schedule C16 of the Official Plan. Please note that the City’s right-of-way should typically always be unencumbered (i.e., no infrastructure, etc.). An alternative road widening approval must be granted from Transportation Planning in order to proceed – identify the proposed road widening on the site plan in the next submission for consideration.

[Applicant response] Noted. Transportation planning has been engaged for alternative road widening approval. Please refer to email from Ivan Ho sent to Max Walker on March 14, 2024. Attached to this comment-tracker for reference.

City (April 2024) – Comment outstanding pending resolution with Transportation Planning. Please continue to engage with the Transportation Planning on the matter and cc planning staff on all correspondence.

12. Improvements to pedestrian connectivity are appreciated.
13. Provide more information on how snow storage will be handled on site. Will it be stored on site or removed?
14. Please confirm the height of the proposed gas bar canopy. Please note that maximum height limits apply.
15. It is noted that minor variances are required to permit the proposed development. Please contact [panel2\\_planners@ottawa.ca](mailto:panel2_planners@ottawa.ca) to speak with a Committee of Adjustment Planner regarding the requested zoning relief.
16. Staff have some concerns with potential light spillage onto abutting residential properties from vehicles in the drive-through. Please note that a Site Lighting Certification letter will be required as a condition of Site Plan approval. The purpose of this letter is to confirm that the site has been designed, among other things, to result in minimal light spillage onto adjacent properties. As a guideline, 0.5 fc is normally the maximum allowable spillage.





Feel free to contact Colette Gorni, Planner (DR West), for follow-up questions.

## **Urban Design**

### **List of Studies and Plans Reviewed:**

- ☐ **Urban Design Brief**, prepared by Parsons Inc., dated March 2024.

Deficiencies:

- 17. No deficiencies – Urban Design Brief is complete.

Comments:

- 18. Urban Design is generally pleased with the changes made to the site and landscape plans for 1660 Merivale Road, however, we still have concerns with the third access near the intersection of Merivale Road and Viewmount Drive. Our preference is that the third access be removed, and hardscape be replaced with softscape.

Feel free to contact Nader Kadri, Urban Design Planner, for follow-up questions.

## **Engineering**

- ☐ **Erosion/ Sediment Control & Removals Plan**, Drawing No. C-101, prepared by Parsons, dated January 2024.
- ☐ **Site Servicing Plan**, Drawing No. C-102, prepared by Parsons, dated January 2024.
- ☐ **Grading Plan**, Drawing No. C-103, prepared by Parsons, dated January 2024.
- ☐ **Detail Page**, Drawing No. C104, prepared by Parsons, dated January 2024.
- ☐ **Pre Development Drainage Areas**, Drawing No. C105, prepared by Parsons, dated January 2024.
- ☐ **Post Development Drainage Areas**, Drawing No. C106, prepared by Parsons, dated January 2024.
- ☐ **Geotechnical Study**, prepared by SCP Geotek, dated May 2023.
- ☐ **Phase I - Environmental Assessment**, prepared by Barenco Inc., dated November 2011.
- ☐ **Phase II - Baseline Environmental Assessment**, prepared by Barenco Inc., dated December 2011.

- **Site Servicing & Stormwater Management Report - Gas Station & Convenience Store - 1660 Merivale Road**, prepared by Parsons, dated March 2024.
- Phase 2 Pre-Consultation Comments & Response – PC2024-0105, dated March 2024.

Deficiencies:

Phase 2 Pre-Consultation Comments & Response

19. Comment #29: Please confirm and show on servicing plan how each building will be serviced with water (existing and proposed) and where the main watermain feed will be located. The current servicing plan only shows a water service line of 50mm from the existing car wash to the proposed restaurant.
20. Comment #30: The response states that the “existing boundary conditions provide adequate pressures to the proposed building.” However, the report mentions that the Viewmount Drive connection would not meet the peak hourly demands for the site. As stated in comment 23 above, further clarification is required as to which existing water services will remain, which water services are being proposed.
21. Comment #33: This comment is related to the sanitary downstream system. Please demonstrate adequate system for the additional flow.
22. Comments #31, #32, #34, #35 are addressed.

Geotechnical Study (Abi to complete)

23. Please provide an updated report to be sealed and dated by a Geotechnical Professional Engineer licensed in Ontario
24. As per the City of Ottawa Geotechnical Guidelines, basic chemical analysis of soil or groundwater should also be completed to assess the potential for corrosion of buried ferrous elements and sulphate attack on buried concrete. Please discuss this in the report
25. The assessment for seismic design and liquefaction must be based on the Ontario Building Code as per the City of Ottawa Geotechnical Guidelines. Please update the report accordingly
26. Please provide an updated report to be sealed and dated by a Geotechnical Professional Engineer licensed in Ontario.
27. As per the City of Ottawa Geotechnical Guidelines, basic chemical analysis of soil or groundwater should also be completed to assess the potential for

corrosion of buried ferrous elements and sulphate attack on buried concrete. Please discuss this in the report.

28. The assessment for seismic design and liquefaction must be based on the Ontario Building Code as per the City of Ottawa Geotechnical Guidelines. Please update the report accordingly.

#### Site Servicing & Stormwater Management Report

29. As per OSDG Section 8.3.11.3, please provide the following information on the design drawings: rooftop storage volume, depth of flow depth, location of roof drains and scuppers, number of roof drains, flow per roof drain, total flow from roof.
30. Section 8.0 Water Servicing page 9 – states that the Viewmount Dr. connection would not meet the peak hourly demands for the proposed site. See comments #23 and #24 above.

#### Erosion/ Sediment Control & Removals Plan

31. Please show on plan which of the existing water servicing will be removed.

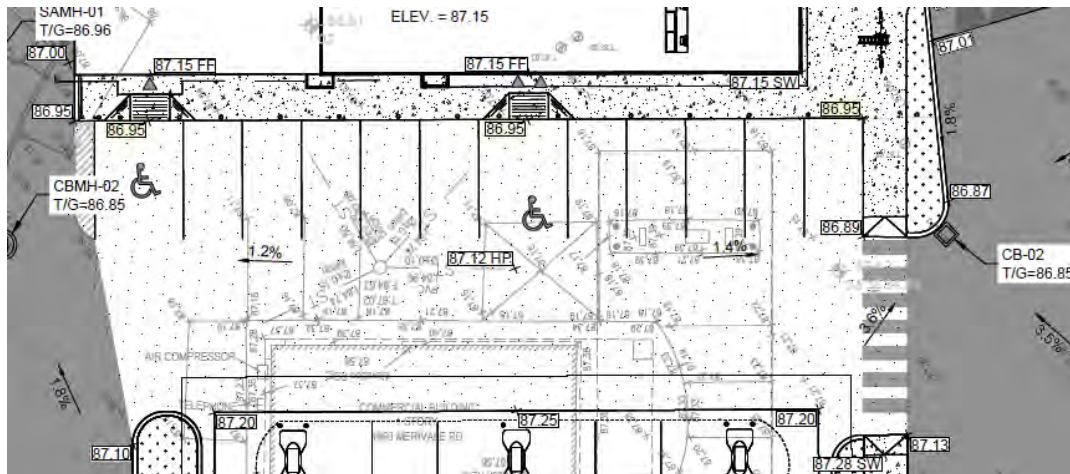
#### Servicing Plan

32. Please provide a sanitary monitoring manhole close to the property line at Merivale Road proposed sanitary connection.
33. Ensure that all buildings foundation drains are connected to the storm sewer downstream of the ICD.
34. The storm lead between TRD.CB-02 to CBMH-03 is approximately 0.15m below finished asphalt grade. Please lower the storm lead so that the invert is at least 1.0m below finished surface. Ensure proper insulation is provided.
35. Please lower the storm lead between TRB.CB-01 to storm sewer and storm lead between CB-01 and CBMH-01 to be at least 1.0m below finished surface. Ensure proper insulation is provided.
36. Please show on plan which of existing water services will remain, which water services are being proposed as requested in comment #23 and #24.
37. Please add a note that all site entrances are to be installed as per City Standard SC7.1

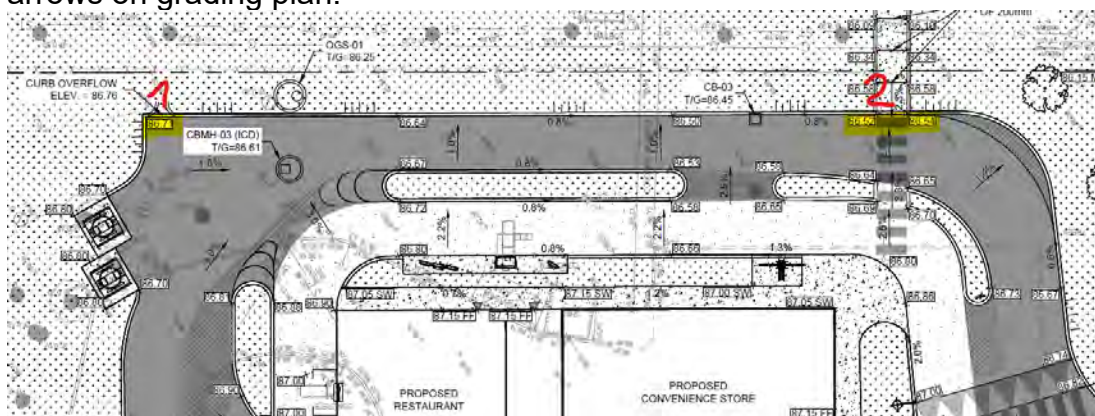
#### Grading Plan

38. Ensure that a slope of minimum 0.5% is provided along curb lines. Grading needs to direct water away from the building entrance.





39. Please provide the emergency overland flow route location on the grading plan. The curb overflow elevation (1) shown on plan is higher than the highlighted location (2). Please review and revise as needed. Also please show major flow arrows on grading plan.



40. Please show 100-year ponding areas on grading plan. Ensure building entrances are at least 0.15m higher than spill elevation.

### Detail Page

41. Please add City Standard SC7.1 – Rev Date – March 2021 to the Detail Page.

### Comments:

42. Please update City Standard SC5 and S11.1 to the latest revision.  
43. Review and revise note below as some wording appears to be missing.

### NOTES: SEWER

1. ALL CATCH BASIN, TRENCH DRAIN, MAINTENANCE HOLE COVERS AND OTHER STORM SEWER CONNECTION SHALL HAVE WITH NITRILE RUBBER GASKET.

44. Please fill out the attached Flow Control Declaration form and provide before Site Plan Approval.



Flow\_Control\_Declaration (2).pdf

### Phase 2 Pre-Consultation Comments & Response

45. [Phase 2 Pre-con, Comment 36] An MECP ECA will be required as part of the proposed development. This would be a direct submission to the MECP.

[Applicant response] once City approves design, MECP ECA will be submitted directly to MECP.

City (April 2024) – The City acknowledges that MECP ECA will be submitted directly to MECP once design is approved.

46. [Phase 2 Pre-con, Comment 37] There is currently a restriction to cut within new pavement on Merivale Road as it has been resurfaced in 2021. The restriction is in effect until end of 2024. During the restriction, road cut may be granted on a case-by-case basis only with additional charges. There is also a pavement degradation fee applicable on all road cut permits. The newer the pavement structure, the higher the rate.

[Applicant response] To be discussed with client & city. Sanitary sewer connection to Merivale Road could maybe be avoided.

City (April 2024) – The proposed site servicing shows the sanitary connection at Merivale Road. It seems that the sanitary connection to Merivale Road is the only one available to this site.

Feel free to contact Anton Chettrar, Infrastructure Project Manager, for follow-up questions.

### **Noise**

#### List of Studies and Plans Reviewed:

- ☐ **Stationary Noise Assessment**, prepared by Gradient Wind, dated March 2024.

Comments:

47. No comments

Feel free to contact Mike Giampa, Transportation Project Manager, for follow-up questions.

## **Transportation**

- ☐ **1660 Merivale Road TIA Strategy report**, prepared by Parsons, dated March 2024.

Comments:

- 48. At the revised private approaches, any sidewalks that are damaged should be replaced as per specification SC7.1.
- 49. Show the depressed curbs at all internal pedestrian crossings and ensure that TWSIs are installed.
- 50. Corner triangles should be conveyed to the City.

Feel free to contact Mike Giampa, Transportation Project Manager, for follow-up questions.

## **Environment**

- 51. No comments.

Feel free to contact Sami Rehman, Environmental Planner, for follow-up questions.

## **Forestry**

- ☐ **Landscape Plan**, L1.01, prepared by Levestek Consultants Inc., revision 1 dated MAR. 11/24.
- ☐ **Tree Conservation Plan / Report**, L1.02, prepared by Levestek Consultants Inc., revision 1 dated MAR. 11/24.
- ☐ **Canopy Cover Plan**, L1.03, prepared by Levestek Consultants Inc., revision 1 dated MAR. 11/24.

Comments:

## **TCR**

- 52. It is appreciated that the design primarily stays within the existing paved area and does not expand into the treed area to the south.
- 53. Plan design requires removal of 15 trees on private property. Is it possible to relocate the garbage containers to reduce the impacts in that location?

## **Landscape Plan**

- 54. The landscape plan and canopy cover plan are very well done, providing a significant amount of canopy cover for the site – thank you.



55. To reduce the impact of car lights shining into the residences facing Glenmanor, please consider planting conifers at select in the west and southwest planting areas, for screening in all seasons.

Feel free to contact Nancy Young, Planning Forester, for follow-up questions.

### **Parkland**

Comments:

56. No further comments – per previous comments, Parks and Facilities Planning requires Cash-in-lieu of Parkland dedication for the development site. The total amount will be based on 2% of the gross land area of the development site.

Feel free to contact Louise Cervený, Parks Planner, for follow-up questions.

### **Surveys & Mapping**

- ☐ **Proposed Site Plan, A-102**, prepared by MRA Architecture + Design, dated 13-03-2024.

Deficiencies:

57. Site plan requires a note stating the source of legal/topographic information.

### **Environmental Remediation Unit (ERU)**

- ☐ **Phase I Environmental Assessment 1660 Merivale Road, Ottawa, Ontario**, prepared by Barenco Inc., dated November 9, 2011.
- ☐ **Baseline Environmental Assessment 1660 Merivale Road, Ottawa, Ontario**, prepared by Barenco Inc., dated December 5, 2011.

Deficiencies:

58. The Phase One and Phase Two ESA reports submitted with the application are both dated 2011. The Official Plan requires that Phase One and Phase Two reports submitted in support of Site Plan Applications comply with the applicable regulations. O. Reg 153/04 states that Phase One and Phase Two reports must be based on information that is no more than 18 months old. Therefore, the submitted reports do not comply with the regulation and do not represent current site conditions.

59. Phase One and Phase Two ESAs that comply with the regulation and represent current site conditions must be submitted.

Comments:

60. I did look through the reports and noted that the text of the Phase Two indicates that all soil and groundwater samples complied with the appropriate site standards, however the figures included in the report show several groundwater exceedances of the standards. The location of the samples that exceed are close to the property boundary at Viewmount and Merivale indicating likely migration to the City rights of way, which is consistent with more recent data provided to the City by the previous site owner. As noted in the pre-consultation meeting with the proponent, and Off-Site Management Agreement will be required as a condition of Site Plan to address the contamination to City property.

Feel free to contact Richard Barker, Specialist (Environmental Remediation), for follow-up questions.

### **Traffic Management**

Comments:

- 61. No impacts or reductions to Merivale Road or Viewmount Drive – should there need to be any right-of-way (ROW) impacts, traffic management would need to review a traffic control plan and the timeline/duration for what the constructability scope would look like – [tmconstruction@ottawa.ca](mailto:tmconstruction@ottawa.ca) .
- 62. Will there be any encroachments needed on City ROW – sidewalk closures? Lane closures? Will need traffic control plans showing the limits and location of the request.
- 63. If there are any impacts to the transit shelters or stops – please notify traffic management [tmconstruction@ottawa.ca](mailto:tmconstruction@ottawa.ca), and [anise.samimi@ottawa.ca](mailto:anise.samimi@ottawa.ca) a minimum of 15 days in advance to review and approve.
- 64. Will there need to be site servicing – this needs to also have a dimensioned site plan and TCP to show the impact.
- 65. Where will the contractor be storing material, on site?
- 66. Will there be any crane swing impacts on adjacent properties including the City ROW (if any).

### **National Capital Commission**

- 67. No comments.

### **Zoning Plans Examiner**

List of Studies and Plans Reviewed:

- ☐ **Zoning Confirmation Report**, dated March 27, 2024.

Comments:

68. The proposed development exceeds the maximum 3-metre building setback for non-residential buildings identified in Section 186(10)(b)(i).
69. The proposed development does not meet the minimum parking requirements for the site – 13 spaces are provided, whereas the Zoning By-law requires 22 spaces.
- a. A total of 24 spaces are required per Section 101.
- Convenience store:  $3.4/100\text{sqm GFA} = \mathbf{10 \text{ spaces required}}$
  - Restaurant:  $10/100\text{sm GFA}$   
 $10 \times 167\text{sm} = 1067/100 = 16.7 = 17 \text{ required}$   
 $6b(i)17 - 20\% (3) = \mathbf{14 \text{ spaces required}}$
- b. With the Shared Parking provisions (Section 104) applied to the Section 101 amounts identified above, the number of spaces required is **22 spaces** (“Weekday Evening” is the the largest cumulative total for all the uses in any time period).
70. Please note that a minimum of 50% of the surface area of the ground floor façade, measured from the average grade up to a height of 4.5 metres, facing a public street must be comprised of transparent glazing and active customer or resident entrance access doors, per Section 186(10)(h.. Confirm that this provision is being met for each ground floor façade facing a public street (i.e., Merivale, Viewmount, Glenmanor). It appears that relief may be required for the Viewmount and Glenmanor facades.

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Should there be any questions on the above, please do not hesitate to contact myself or the contact identified for the above areas / disciplines.

Yours Truly,  
Colette Gorni

c.c.

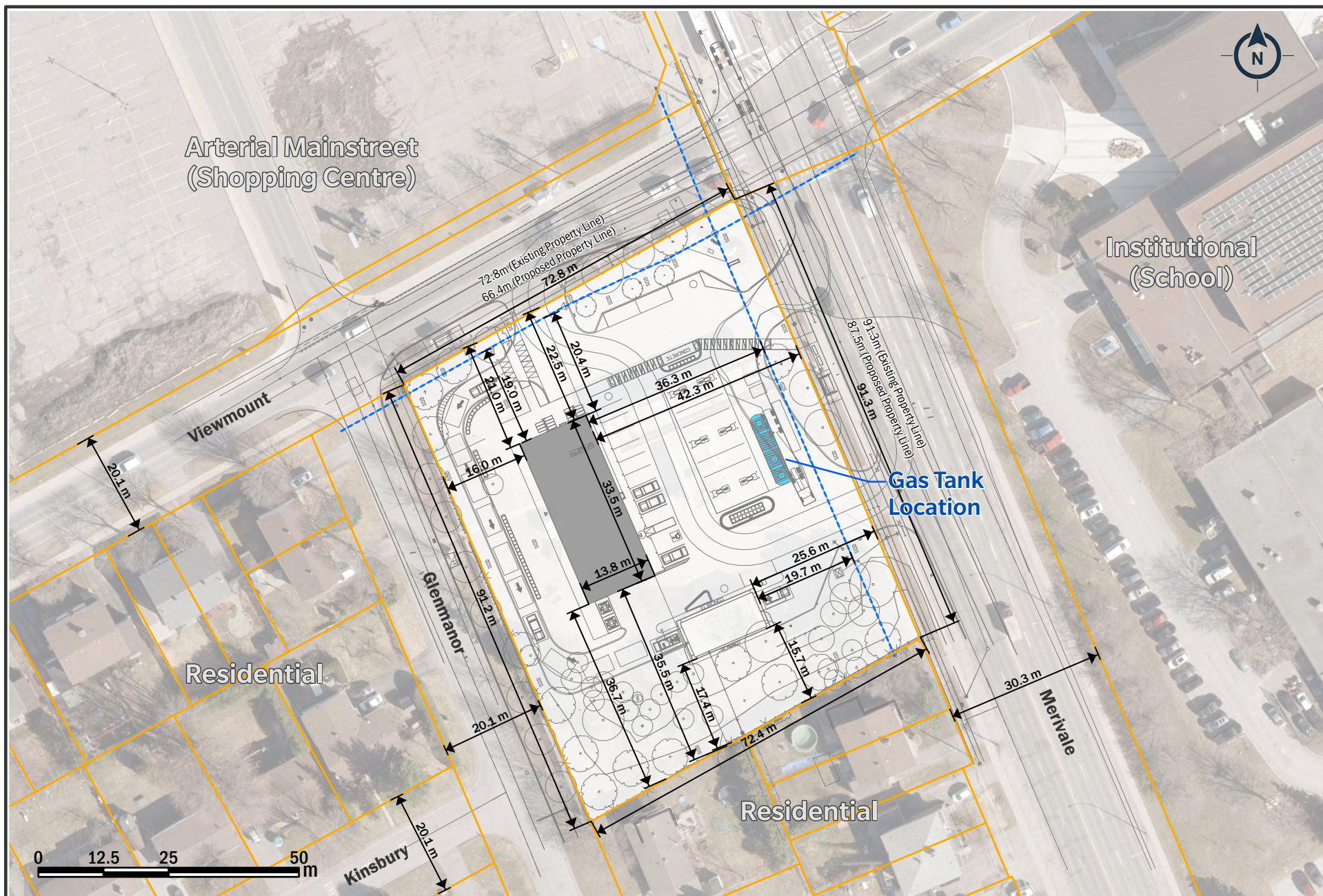
Anton Chetnar, Infrastructure Project Manager  
Abi Dieme, Infrastructure Project Manager  
Mike Giampa, Transportation Project Manager  
Nader Kadri, Urban Design Planner  
Matthew Hayley, Environmental Planner  
Nancy Young, Planning Forester  
Aly Mann, Zoning Plans Examiner  
Saeid Sedaghat, Surveys & Mapping





Richard Barker, Environmental Remediation Unit  
Adrianna Lawlor, Traffic Management  
Ted Horton, National Capital Commission

**Appendix C: Minor Variance Sketch**



1660 Merivale Road (Harnois Energies)

**Legend**

- Proposed Building
- Concrete Area (Gas Tank)