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Minor Variance Application

339 Eastview Street

City of Ottawa Committee of Adjustment
101 Centrepointe Drive, 4th Floor
Ottawa, ON K2G 5K7

December 9th, 2025

339 Eastview Street

P-Squared Concepts
Minor Variance Cover Letter
339 Eastview St.





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City of Ottawa Committee of Adjustment
101 CentrepoinTE Drive, 4th Floor
Ottawa, ON K2G 5K7

Attn: Mr. Michel Bellemare
Secretary Treasurer

Re: 339 Eastview St (Minor Variance application) - Ward 12
12093073 CANADA INC. (c/o Steven Cleiri)
242 Main St
Ottawa, ON K1S 1C7

Site Context

On behalf of our clients, we are submitting the enclosed Minor Variance application for the property located at 339 Eastview St. The subject property is currently developed with a two-storey building with a basement and a double-width driveway providing two rear parking spaces, all of which are proposed to be demolished. The proposal contemplates the construction of a **three-storey low-rise residential apartment building with a basement** containing a total of eight dwelling units (two units per storey), as well as an **accessory structure (shed)** on the south side of the property to serve as an enclosed solid waste storage area. No on-site vehicle parking or driveway is proposed as part of this development.

The subject property is a corner lot with frontage on both Eastview Street (27.42 m of frontage) and Levis Avenue (12.28 m of frontage). For zoning purposes, the lot line along Levis Avenue is recognized as the front lot line. The property is zoned **R4UA[1136]** – Residential Fourth Density, Subzone UA, Exception 1136, under Zoning By-law 2008-250, as amended. Site-specific Exception [1136] permits a dwelling unit in the basement. The property is located within the **Inner Urban Transect** and is designated **Neighbourhood** in the City of Ottawa Official Plan.



Fig.1.- Schedule B2- Inner Urban Transect, City of Ottawa Official Plan.

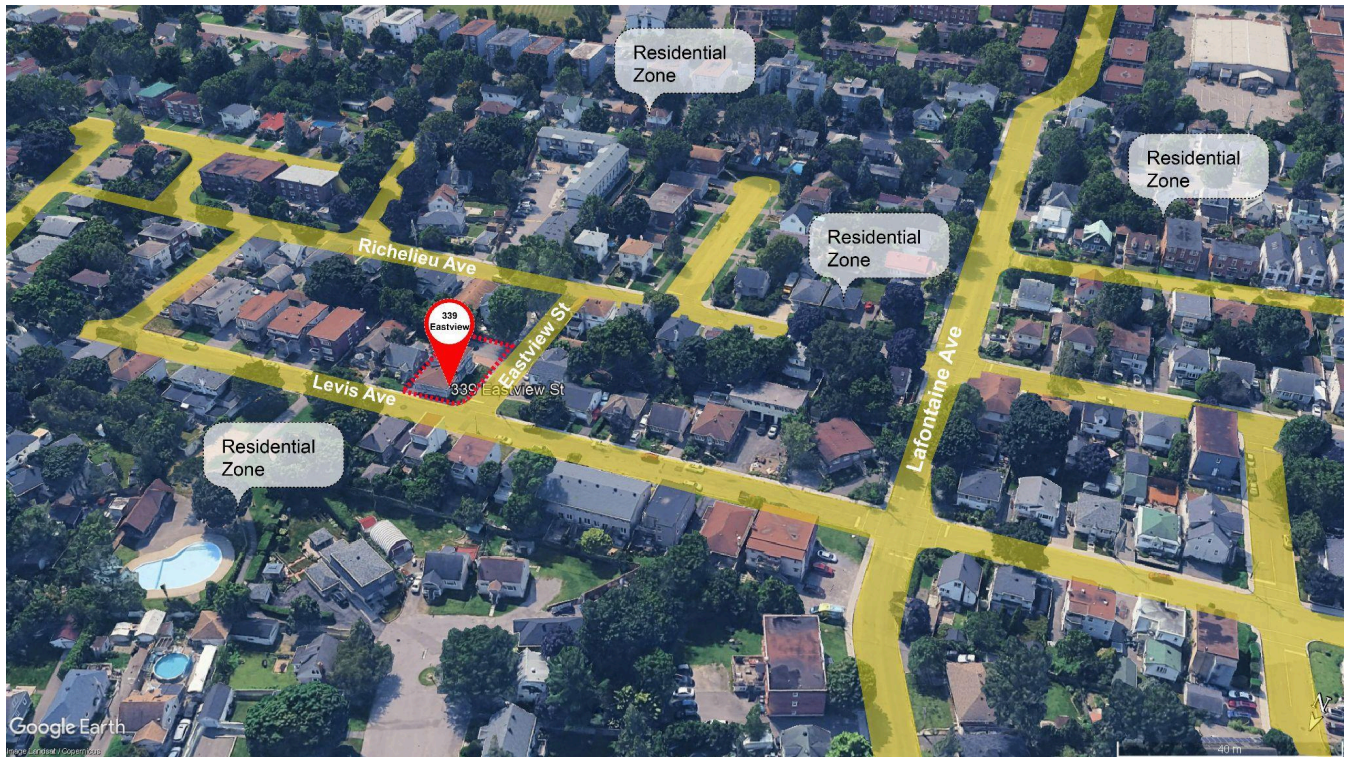


Fig.2. - Aerial imagery showing the surrounding context of the subject site, Google Earth.

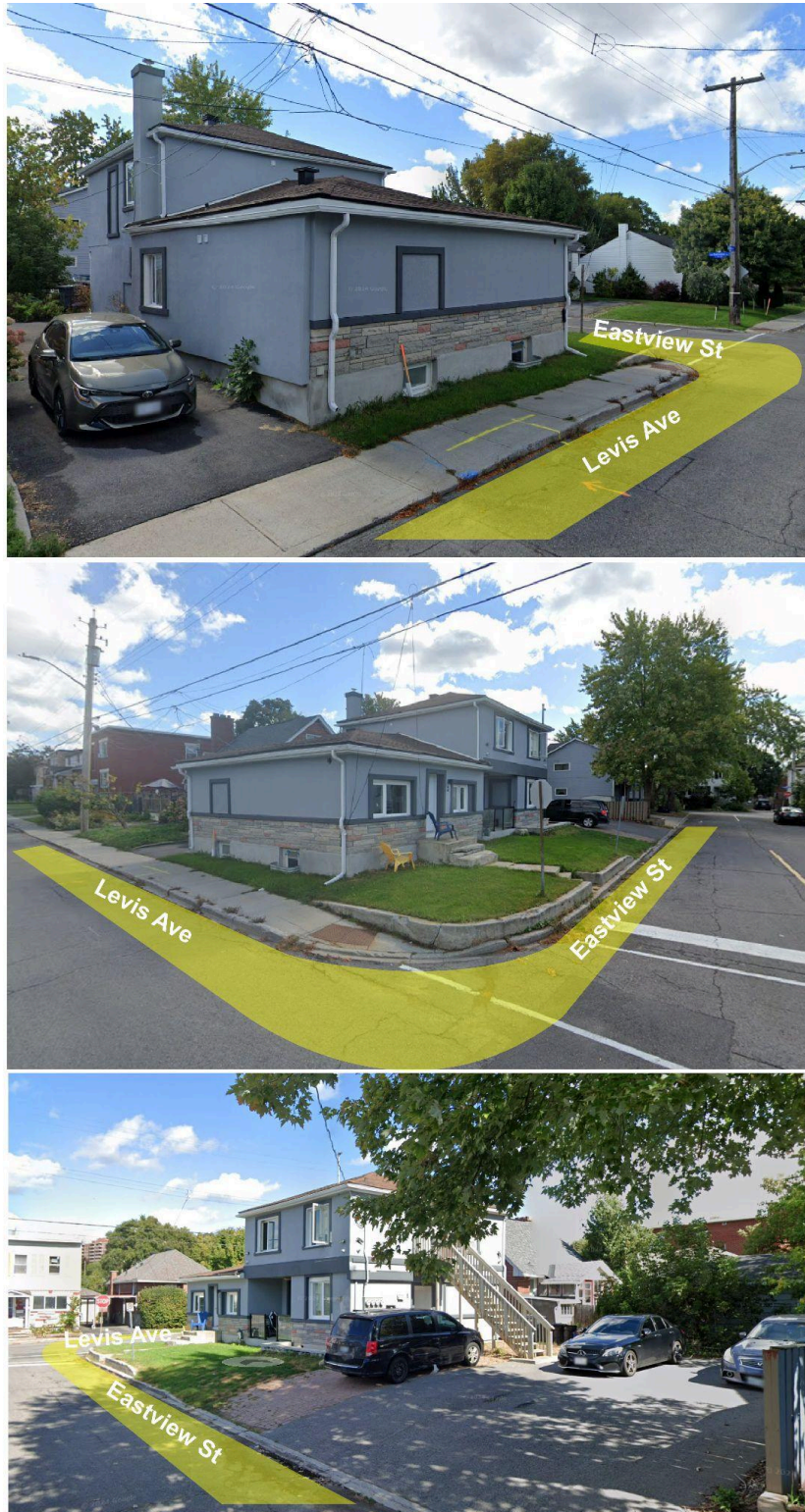


Fig.3. - Subject property showing existing dwelling to be demolished.

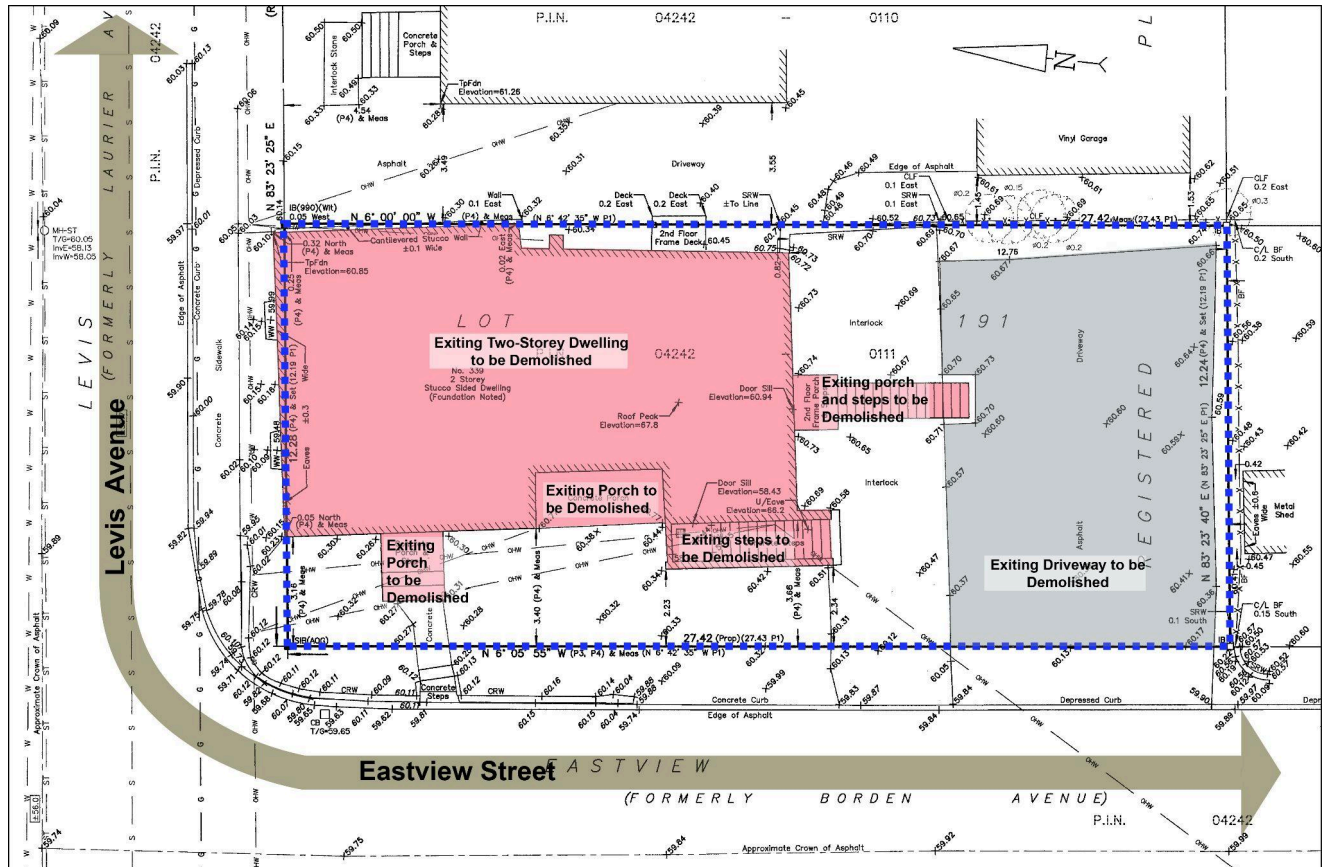


Fig.4. - Survey Plan showing *Property boundary, existing dwelling and driveway to be removed, completed by Farley, Smith & Denis Surveying Ltd, December 2024.*

Minor Variance Application

The proposal requires relief from the following Zoning By-Law sections:

- A) Minimum Lot Area. The proposed lot area is 335.99 m², whereas the minimum lot area for a low-rise apartment of maximum 8 units required by the Zoning By-Law is 360 m² (Table 162A, (V), Zoning By-Law 2008-250, as amended).
- B) Minimum Front Yard Setback. The proposed front yard setback abutting Levis Ave is 3.88 m, whereas the minimum front yard setback for a low-rise apartment of maximum 8 units required by the Zoning By-Law is 4.5 m (Table 162A, (VII), Zoning By-Law 2008-250, as amended).
- C) Minimum Corner Side Yard Setback. The proposed corner side yard setback is 2.02 m, whereas the minimum corner side yard setback for a low-rise apartment of maximum 8 units required by the Zoning By-Law is 4.5 m (Table 162A, (VIII), Zoning By-Law 2008-250, as amended).

D) Maximum Projected Stairways Setback to Corner Side Lot Line. The proposed stairway projection into the corner side yard has a setback of 0.13 m from the corner side lot line, whereas a minimum setback of 0.6 m to a lot line is required for stairways at or below the first-floor level in a corner side yard (Table 65,(5), (b), (i), (2), Zoning By-Law 2008-250, as amended).

The requested zoning relief is limited to the minimum lot area, front yard setback, corner side yard setback, and the setback of the projected stairways to the corner side lot line. All other applicable provisions including rear and interior side yard setbacks, soft landscaping in the front, corner, and rear yards, front facade balconies for each storey at and above the first storey, front and corner facade fenestration requirements, lot width, and building height are met by the proposal. A detailed performance standards review is provided in the table at the end of this document.

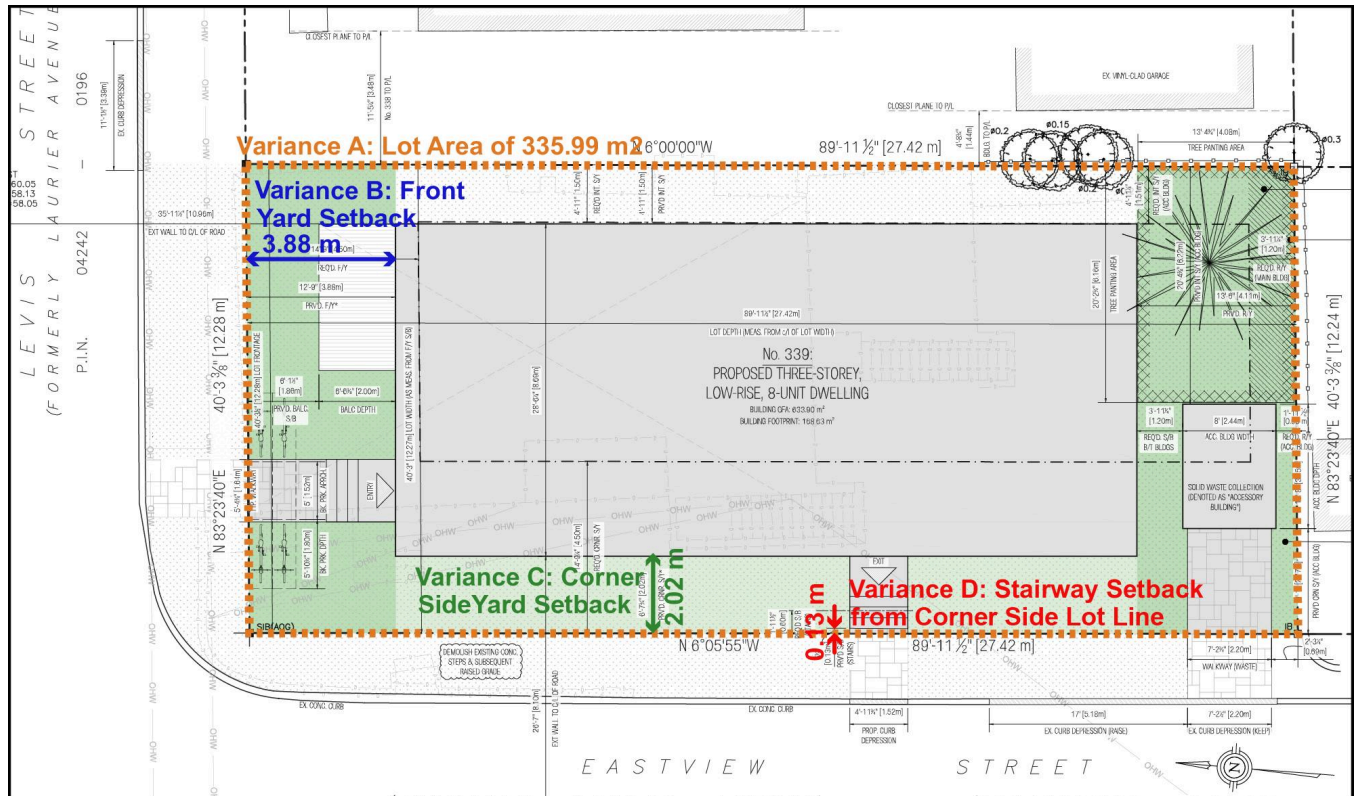


Fig. 5. - Site Plan showing the location of Minor Variances, completed by P-squared concepts, November 2025.

The four tests of a **Minor Variance** application from Section 45 (1) of the Planning Act are that the variances are minor in nature, are desirable for the appropriate development or use of the land and building, and maintain the general intent and purpose of the Zoning By-Law and Official Plan.

1. The variances are minor in nature.

The requested variances are minor in nature since they do not create significant departures from the Zoning By-Law or cause adverse impacts on abutting properties or the neighbourhood as a whole. The intent of this Minor Variance application is to allow for a small decrease of the lot area, front yard setback, corner side yard setback and corner side yard setback to the projected stairways, with compatibility maintained for the character and functionality of the surrounding area.

Variance A seeks to decrease the allowable minimum lot area from 360 m² to 335.99 m². This is a deficiency of 24.01 m² which is minor and does not negatively affect neighbouring properties. Despite the reduction in the lot area, the proposed lot size is in keeping with the existing urban fabric and patterns of the area. Several of the properties in the surrounding area have lot areas smaller than 360 m² (refer to Fig. 6. below). This precedent provides a good basis for the proposed lot area, with the variances requested not creating an inconsistency but rather contributing to the prevailing urban form.

It should be noted that the subject property is zoned N3B under the new Zoning By-law 2026-50- Final Draft. Under this by-law, there is no minimum lot area requirement for the N3B zone; instead, density is controlled through the maximum permitted number of dwelling units. For the N3 zone, the maximum is 2.2 dwelling units per 100 m² of lot area. On a lot area of 335.99 m², this equates to 7.39 units, which, in accordance with the pro-rating provision that requires rounding to the nearest whole even number (Section 801(3)), results in a permission for 8 units. Accordingly, once the new by-law comes into force, the proposed development would comply with the applicable density provisions and no minor variance for lot area would be required (Table 801A(i)(c) and Section 801(3), New Zoning By-law 2026-50, Final Draft) *(noting that Version 3 of the new Zoning By-law is currently only a Planning Department recommendation and has not yet been adopted by Council or come into force, and is referenced here solely to illustrate how the proposal aligns with the emerging zoning framework).*

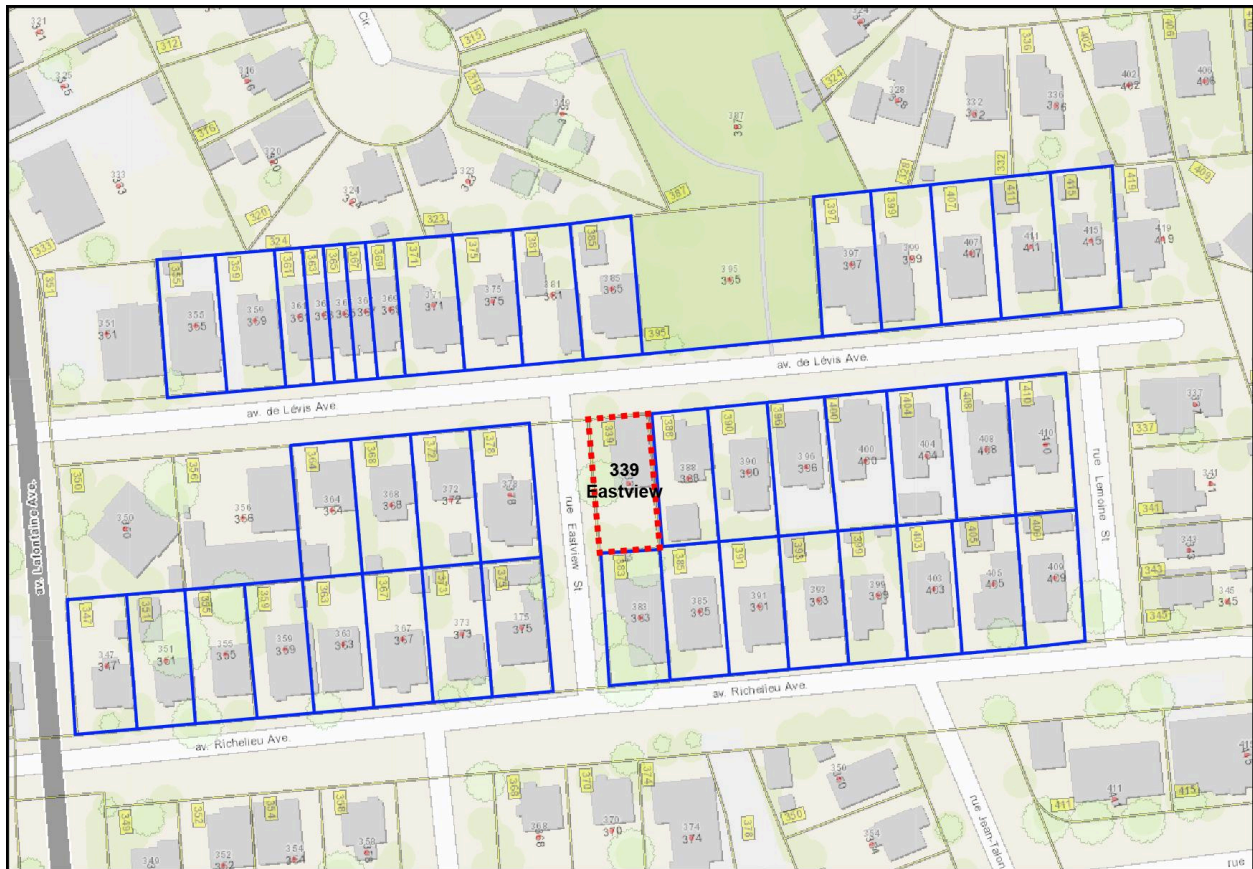


Fig. 6. - Diagram identifying nearby properties with lot areas less than 360 m².

Variance B, seeks to reduce the required front yard setback from 4.5 m to 3.88 m, representing a modest deficiency of 0.62 m. At present, the existing building extends beyond the front lot line and encroaches 0.32 m into City property, effectively resulting in a 0 m front yard setback. Demolishing the existing building and replacing it with a new building that is entirely within the property boundary, while still providing a 3.88 m setback to the front lot line, brings the condition significantly closer to conformity and removes any encroachment into the public realm.

Furthermore, several nearby properties fronting Levis Avenue have front yard setbacks ranging from approximately 2 m to 4.4 m (refer to Figure 7). This establishes a clear local pattern demonstrating that the proposed front yard setback is consistent with existing conditions in the area. The requested reduction will not negatively impact neighbouring properties and remains compatible with the established character of the neighbourhood.

In addition, while a variance to the numerical front yard setback is required, the proposed development substantially improves the front yard landscaped area. The by-law requires 19.18 m² of front yard soft landscaping (40.00% of the front yard area),

whereas the proposed development provides 33.12 m², or approximately 69.08% of the front yard area, as landscaping. By contrast, the existing building, with its 0 m front yard setback, provides no landscaped area at the front. The proposed increase in soft landscaping will enhance the streetscape, improve the public realm condition, and contribute positively to the neighbourhood character.

It should also be noted that the subject property at 338 Eastview is zoned N3B under the New Zoning By-law 2026-50 (Final Draft), where the required minimum front yard setback is 3 m. Accordingly, the proposed 3.88 m front yard setback would fully comply with the new zoning framework and exceed the minimum front yard requirement, such that no minor variance for front yard setback would be required once the new by-law comes into force (Table 801B(iii)(b), By-law 2026-50). (*noting that Version 3 of the new Zoning By-law is currently only a Planning Department recommendation and has not yet been adopted by Council or come into force, and is referenced here solely to illustrate how the proposal aligns with the emerging zoning framework*)



Fig. 7. - Diagram identifying nearby properties on Lévis Ave with front yard setback between 2 m - 4.4 m.

Variance C, proposes to reduce the required corner side yard setback from 4.5 m to 2.02 m. This reduction remains compatible with the established context of nearby corner



lots: 378 Lévis Avenue provides a corner side setback of approximately 0.8 m, 375 Richelieu Avenue has a setback of 0 m, and 383 Richelieu Avenue provides 2.3 m. These examples demonstrate that reduced corner side setbacks are characteristic of the area and do not disrupt the rhythm, function, or safety of the streetscape. In this context, the proposed 2.02 m setback is consistent with existing conditions and is further softened by new soft landscaping along the corner side lot line, which will enhance the streetscape and maintain a comfortable, pedestrian-friendly edge.

In the existing condition, a portion of the corner yard is occupied by an asphalt driveway, and the entire rear yard is paved and used for surface parking. Although the current building provides a corner side yard setback of 3.16 m, much of that space functions as vehicular area rather than landscaped setback. The proposed development eliminates parking and driveway areas from the corner yard and instead provides landscaped areas. This represents a substantial improvement to the public realm and reduces potential vehicle–pedestrian conflicts at the corner.

It is also important to note that, while the numerical corner side yard setback is reduced from 4.5 m to 2.02 m, the proposed development provides 36.24 m² of soft landscaping in the corner yard (41.53% of the corner yard area), which slightly exceeds the required 34.90 m² (40.00% of the corner yard area). Except for the stairways, the remainder of the corner yard will be softly landscaped. In addition, although the building is set back 2.02 m from the corner side lot line, there is an additional approximately 1.85 m of City-owned, softly landscaped space between the lot line and the street (refer figure 7). Together, this creates a total separation of about 3.87 m from the building to the street edge, maintaining appropriate sightlines and safety at the intersection and protecting the quality of the public realm along Eastview street.

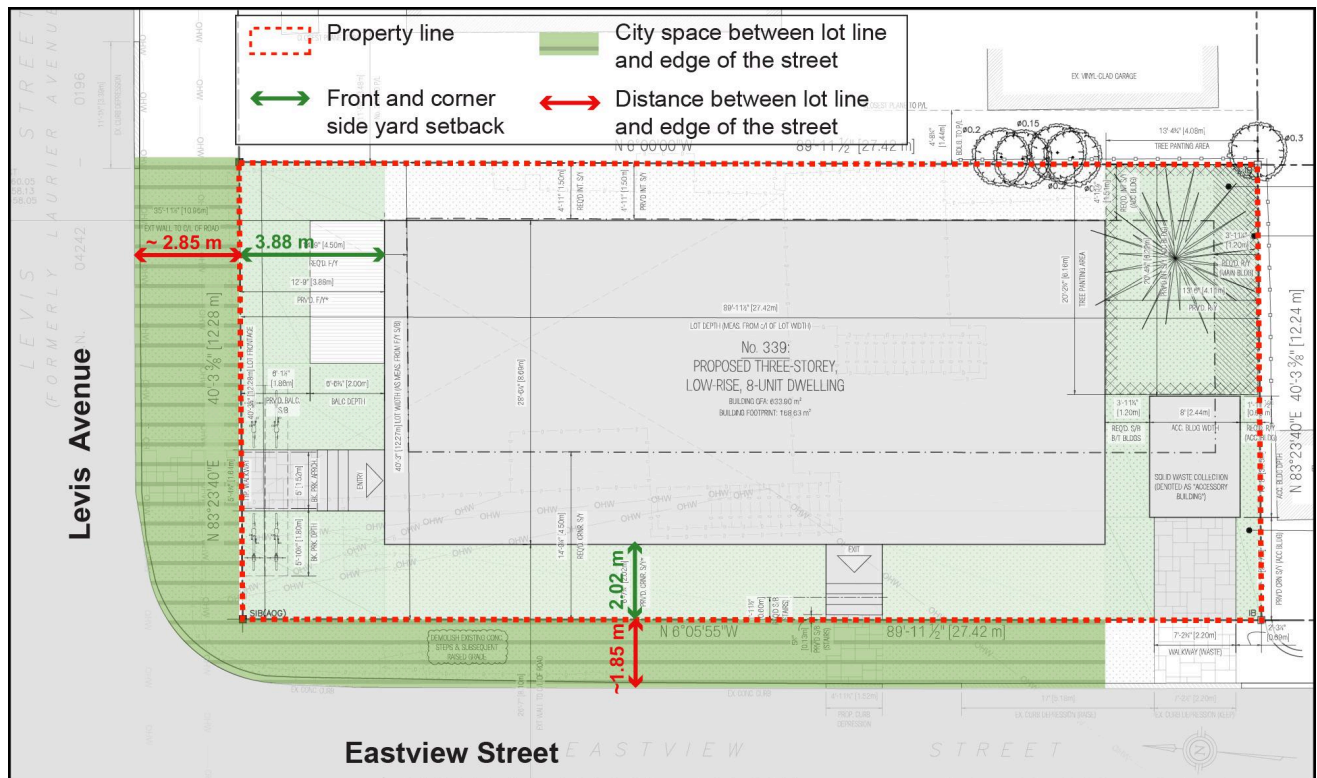


Fig.7. Corner side yard and front yard setback and City space illustrating the total ~3.87 m (corner side) and ~6.73 m (front) separation between the building facade and the street edge.

Variance D, proposes to reduce the required setback for the projected stairway from 0.6 m to 0.13 m from the corner side lot line. Although the stairway is only 0.13 m from the lot line, it remains approximately 1.98 m from the edge of the street, as there is an intervening 1.85 m wide City-owned, softly landscaped space between the property lot line and the curb (refer figure 7). In practice, this maintains adequate separation from the travelled roadway, preserves sightlines and pedestrian safety, and avoids any adverse impacts on adjacent properties or the public realm. Further, the proposed development provides 36.24 m² of soft landscaping in the corner yard (41.53% of the corner yard area), which slightly exceeds the required 40% of the corner yard area. Except for the stairways, the remainder of the corner yard will be softly landscaped. Therefore, this deficiency of 0.47 m is considered minor in nature.

In summary, the variances included in this application will permit a development consistent with the neighbourhood character. The variances are minor as they adhere to zoning purposes, and do not have any significant impact on adjacent properties, while maintaining compatibility with the surrounding suburban character.

2. Desirable and appropriate for the development or use of land and building

The proposed development is the construction of a new three storey building with a basement, bike parking in the front yard and garbage shed in the rear yard, on a site located within area A on Schedule 342. As such, this proposal is subject to the design guidelines listed in the Urban Design Guidelines for Low-rise Infill Housing (2023). The following section features excerpts from this guideline document and *responses*.

Siting and setbacks

- 1.6 Front yards: In cases where there is a uniform setback along a street, match this setback to fit into the street pattern and create a continuous, legible edge to the public street. In cases where there is no uniform setback, locate the infill building at roughly the same distance from the property line as the buildings on the adjacent lots.

Response: On Lévis Avenue, existing front yard setbacks vary between 2 m and 4.4 m. The proposed 3.88 m setback is well within this range, ensuring the building contributes to the continuous, legible streetscape. On the corner side, the proposed 2.02 m setback reflects patterns observed on nearby corner lots, maintaining rhythm and compatibility.

Tree Retention & Planting

- 3.16 Existing trees: The retention and protection of existing trees is key to preserving the character of existing neighbourhoods. Incorporating existing trees into the design of the site will ensure adequate protection over time. The Tree Protection By-law information on the City's website, "Planning Around Trees", provides guidance for tree retention on infill sites. Where trees cannot be retained on a site, ensure sufficient space is made available in the site design to plant replacement trees. The impact of construction on adjacently owned trees must also be considered early in the design process.

Response: Only one existing Silver Maple tree, located on the adjacent property at 383 Richelieu Ave, has been identified in the TIR. This tree will be retained and protected to help preserve the existing character of the neighbourhood. Additionally, new areas for tree planting have been identified within the site as part of the proposed development.

Neighbourhood Character

- 1.1 Ground floor: Contribute to an inviting, safe, and accessible streetscape by emphasising the ground floor and street façade of infill buildings. Locate principal entries, windows, porches, and key internal uses at or close to street level.

Response: The proposed building contributes to an inviting, safe, and accessible streetscape by orienting principal entrances and ground-floor windows toward the street and framing them with new landscaping, creating an active and well-defined public edge on Eastview St and Levis ave. The front facade provides 27.89% fenestration, exceeding the required 25%, and the corner side facade provides 15.15% fenestration, meeting and slightly exceeding the 15% requirement for corner side facades

fenestration (Section 161(15)(g)(i), Zoning By-law 2008-250; see Figure 8). The removal of the existing double width driveway and associated hardscaping reduces vehicle dominance and frees up space for soft landscaping and pedestrian use, thereby reinforcing the neighbourhood character and improving the quality and safety of the streetscape.

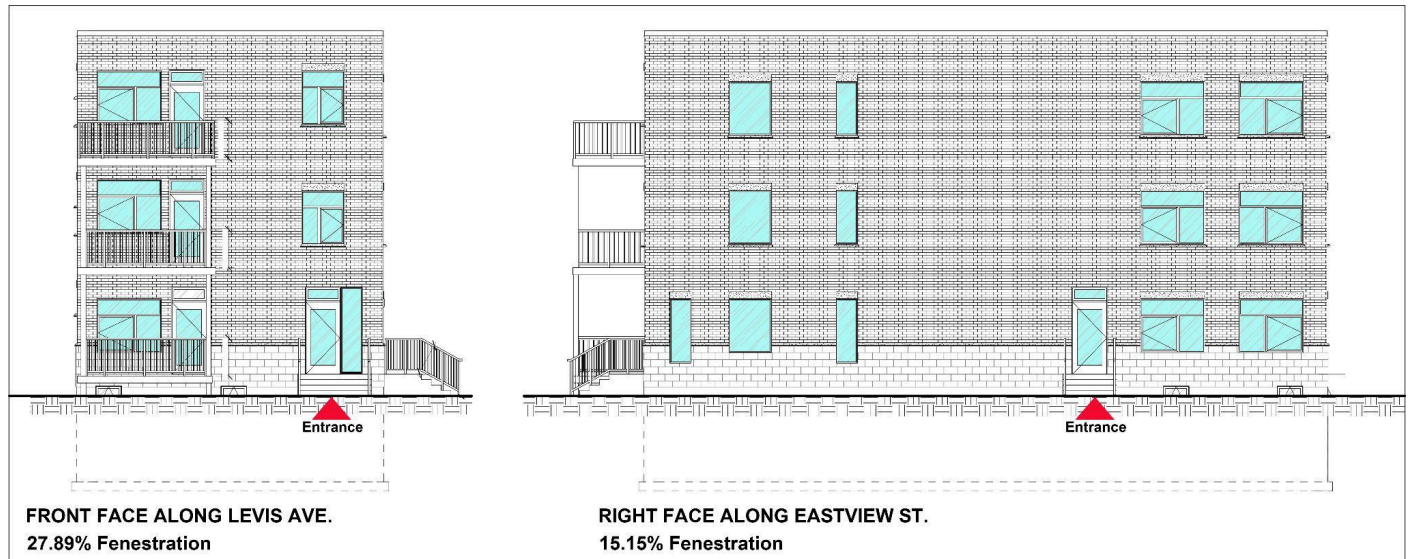


Fig.8. proposed front and corner side facades showing fenestration and entrances.

Built Form

- 2.2 Building massing and scale: The massing and scale of infill buildings should consider the surrounding neighbourhood in terms of building setbacks, building width and length, and the relationship between buildings and the public realm. New infill housing should utilise architectural treatment and articulation to replicate the predominant scale and built form of the surrounding neighbourhood.

Response: The proposed massing and scale are compatible with the surrounding low-rise neighbourhood and maintain an appropriate relationship with the public realm. The building remains within the permitted R4UA height of 11m (providing 9.95 m and three storeys) and reflects the prevailing low-rise form on the street. While minor variances are required to reduce the front and corner side yard setbacks (from 4.5 m to 3.88 m and 2.02 m respectively), the presence of additional City-owned space between the lot lines and the curb results in an effective separation of approximately 6.73 m from the front facade to the edge of the street (Levis Ave.) and 3.88 m from the corner facade to the flanking street (Eastview St.). This maintains generous spacing, supports a pleasant streetscape, and preserves the established rhythm of buildings and open space. The use of facade articulation and materials allows it to blend in well with the surrounding homes, and the modest lot area reduction (24.01 m² below the standard) does not compromise functionality or livability on the site.

2.9 Street facing

- Locate at least one primary entrance facing the street. This can be to a shared entrance or a grade related unit; and

1.4 Street facing facades

- Orient the principal façade of building(s) to face the street. Where they are in keeping with the character of the neighbourhood, include front yard projections, such as porches, bay windows, and balconies, to enhance the façade of the infill and contribute to the sociability of the street, while balancing with energy impacts.

Response: The building is oriented to actively address both streets, with two principal entrances one facing Eastview Street and one facing Levis Avenue. The front facade along Levis Avenue also provides one balcony per storey, in accordance with Section 161(15)(j) of Zoning By-law 2008-250. Together, these street-facing entrances and balconies help animate the facades, support casual overlooking of the street, and strengthen the relationship between the building and the public realm, contributing to walkability, sociability, and neighbourhood safety.



Fig.9. - Elevations from all four sides completed by Clear Line Designs, 2025.

3. The variances maintain the intent of the Official Plan

The subject site is located within the Inner Urban transect designated as a Neighbourhood. It is approximately 400 m walking distance from Montreal Rd and about 650 m walking distance from McArthur Ave- both of which are Mainstreet Corridors, and approximately 650 m walking distance to Olmstead st which is Minor Corridor. Proposed changes are to decrease the lot area, front and corner side yard setback and projected stairway setback to corner lot line. The following section features applicable excerpts from the Official Plan (2022) and *responses*.

2.2.1 Intensification and Diversifying Housing options

- Policy 1 - Direct residential growth within the built-up urban area to support an evolution towards 15-minute neighbourhoods

2.2.4 Healthy and Inclusive Communities

- Policy 1 - Encourage development of healthy, walkable, 15-minute neighbourhoods that feature a range of housing options, supporting services and amenities.

Response: The proposed development represents residential growth as it proposes eight (8) dwelling units in an area where previously there was a two storey building with three dwelling units. The site is located in the urban area, is serviced municipally, and is in close proximity to a variety of daily and weekly amenities including:

- *Kiwanis Park – 84 m (1-minute walk)*
- *Janeville Park – 280 m (4-minute walk)*
- *Montreal Road Medical Centre – 500 m (7-minute walk)*
- *St-Laurent Complex– 650 m (9-minute walk)*
- *Vanier Community Service Centre – 650 m (9-minute walk)*
- *Don Gamble Center St-Laurent Park – 650 m (9-minute walk)*
- *LCBO –750 m (11-minute walk)*
- *McArthur Medical Center – 900 m (13-minute walk)*
- *Police department – 900 m (13-minute walk)*
- *Vanier Animal Hospital – 900 m (12-minute walk)*
- *Shoppers Drug Mart – 1.1 (15-minute walk)*
- *Green Fresh Supermarket Vanier – 1.1 (15-minute walk)*
- *Additional amenities within walking distance include schools, church, parks, banks, convenience stores, clinics, gyms, and restaurants.*

Furthermore, the property is located within a 30-minute walk and 15-minute bike ride to other daily and weekly needs like Staples (24-min walk / 8-min bike ride), Winners (25-min walk / 8-min bike ride), Giant Tiger (28-min walk / 6-min bike ride), Adonis (28-min walk / 7-min bike ride), Food Basics (29-min walk / 9-min bike ride), and some other parks, restaurants, gyms, banks, and various bus stops.

The site is located within the Inner Urban transect and the proposed development makes a direct contribution towards housing growth in the built-up urban area. This kind

of intensification introduces housing diversity while maintaining compatibility with the character of the surrounding neighbourhood, and increases walkability and accessibility to amenities in short proximity, ultimately contributing to the 15-minute neighbourhoods concept. As per Section 161(16)(a) of Zoning By-law 2008-250, motor vehicle parking is not permitted on lots smaller than 450 m². Accordingly, the proposed development does not include any on-site parking or driveway, and the existing driveway and parking spaces will be removed. Residents of the development are expected to rely on alternative modes of transportation; such as transit, walking, cycling, and ride-sharing, given that the property provides only bicycle parking and no motor vehicle parking.

4.2.1 Enable greater flexibility and an adequate supply and diversity of housing options throughout the city

- 1) A diverse range of flexible and context-sensitive housing options in all areas of the city shall be provided through the Zoning By-law, by: a) Primarily regulating the density, built form, height, massing and design of residential development, rather than regulating through restrictions on building typology; b) Promoting diversity in unit sizes, densities and tenure options within neighbourhoods including diversity in bedroom count availability;

Response: The proposed three-storey dwelling with 8 dwelling units enhances housing diversity by introducing a context-sensitive, low-rise residential development that integrates seamlessly into the existing neighbourhood fabric. The design respects the surrounding built form while contributing to gentle intensification through the creation of eight (8) dwelling units. Importantly, the application does not seek variances related to building height, ensuring that the development maintains the intended low-rise character of the area. The building variances requested relate to lot area, corner and front yard setbacks, and projected stairways setback to corner lot line which remain consistent with established patterns in the neighbourhood and do not disrupt the existing streetscape. Additionally, the inclusion of eight (8) residential units with varied layouts, supports a mix of unit sizes, offering greater flexibility in housing options for a range of residents.

4.6.5 Ensure effective site planning that supports the objectives of Corridors, Hubs, Neighbourhoods and the character of our villages and rural landscapes

- 1) Development throughout the City shall demonstrate that the intent of applicable Council-approved plans and design guidelines are met.
- 3) Development shall minimise conflict between vehicles and pedestrians and improve the attractiveness of the public realm by internalising all servicing, loading areas, mechanical equipment and utilities into the design of the building, and by accommodating space on the site for trees, where possible. Shared service areas, and accesses should be used to limit interruptions along sidewalks. Where underground parking is not viable, surface parking must be visually screened from the public realm.

Response: This letter includes applicable sections from the new Urban Design Guidelines for Low-rise Infill Housing - refer to page 14-16.

The proposed development features a bicycle parking in the front yard with convenient access to the entrance and does not propose a vehicle parking space as opposed to the double width driveway existing on the site, and as opposed to long single driveways or double width driveways sometimes leading to a garage or rear yard parking as is typical in this neighbourhood. The proposed development maximizes the street frontage on Eastview St to give priority to building entrance, greenspace, walkways, and minimize hardscaping and asphalt surface and conflict between pedestrians and vehicles. Considering the close proximity of the site to transit stations, detailed below, and that the future tenants will use bikes this will also provide adequate parking and will not result in parking on the street.

- *Rapid Transit:*
 - *OC Transpo Route 12: Blair <-> Tunney's Pasture; provides direct connections to major transit hubs and is accessible via Montréal / Granville Station, located 400 m away (approximately a 6-minute walk).*
 - *OC Transpo Route 14: St-Laurent <-> Tunney's Pasture; provides direct connections to major transit hubs and is accessible via McArthur / Lafontaine Station, located 600 m away (approximately an 8-minute walk).*
 - *OC Transpo Route 7: St-Laurent <-> Carleton; provides direct connections to major transit hubs and is accessible via St-Laurent / Côté Station, located 850 m away (approximately a 12-minute walk).*

4.6.6 Enable the sensitive integration of new development of Low-rise, Mid-rise and High-rise buildings to ensure Ottawa meets its intensification targets while considering liveability for all

- 6) Low-rise buildings shall be designed to respond to context, and transect area policies, and shall include areas for soft landscaping, main entrances at-grade, front porches or balconies, where appropriate. Buildings shall integrate architecturally to complement the surrounding context.

Response: The proposed building is considered low-rise, being only three storeys complying with maximum building height, and will be well integrated into the neighbourhood context. The building is designed to ensure the proposal contains more than required front, corner and rear yard soft landscaping area. The buildings contain principal entrances fronting on Levis Ave, and Eastview st consistent with Neighborhood building entrances (refer to figure 8). The proposed flat roof, building height, and material articulation are all elements that contribute to the existing neighbourhood fabric.

5.2.1 Enhance or establish an urban pattern of built form, site design and mix of uses

- 1) The Inner Urban Transect’s built form and site design includes both urban and suburban characteristics as described in Table 6. Its intended pattern is urban.

URBAN	SUBURBAN
Shallow front yard setbacks and in some contexts zero front yards with an emphasis on built-form relationship with the public realm	Moderate front yard setbacks focused on soft landscaping and separation from the right-of-way
Principal entrances at grade with direct relationship to public realm	Principal entrances oriented to the public realm but set back from the street
Range of lot sizes that will include smaller lots, and higher lot coverage and floor area ratios	Larger lots, and lower lot coverage and floor area ratios
Minimum of two functional storeys	Variety of building forms including single storey
Buildings attached or with minimal functional side yard setbacks	Generous spacing between buildings
Small areas of formal landscape that should include space for soft landscape, trees and hard surfacing	Informal and natural landscape that often includes grassed areas

Fig. 10. - General Characteristics of Urban Built Form and Suburban Built Form and Site Design, Table 6, Official Plan

Response: The proposed development aligns with the intended urban pattern of the Inner Urban Transect by incorporating a low-rise three-storey built form incorporating two entrances along Eastview St and Levis Ave, with slightly smaller than permitted lot area of 335.99 m² and with functional front, corner and rear yard setbacks, as described in Table 6 of the Official Plan. The requested lot area, front and corner side yard setbacks variances, are consistent with the prevailing lot fabric in the neighbourhood, where reduced setbacks are common. This ensures that the development integrates seamlessly with adjacent properties, contributing to the continuous urban frontage that defines the character of the Inner Urban Transect.

5.2.2 Prioritize walking, cycling and transit within, and to and from, the Inner Urban Transect

- 3) Motor vehicle parking in the Inner Urban Transect shall be managed as follows:

- a) Motor vehicle parking may only be required for large-scale developments, and only to the extent needed to offset sudden large increases in parking demand.

Response: As the policy states, motor vehicle parking is not a requirement for this development. By excluding on-site vehicle parking and instead providing bicycle parking in the front yard, the proposal supports sustainable transportation choices such as walking, cycling, and transit. Given the site's proximity to frequent and reliable public transit, as referenced in Policy 4.6.5, future residents will benefit from convenient mobility options while reducing reliance on private vehicles.

5.2.4 Provide direction to the Neighbourhoods located within the Inner Urban Transect

- 1) Neighbourhoods located in the Inner Urban area and within a short walking distance of Hubs and Corridors shall accommodate residential growth to meet the Growth Management Framework. The Zoning By-law shall implement the density thresholds in a manner which adheres to the built form requirements as described in Subsection 5.6.1, as applicable and that:
 - a) Allows and supports a wide variety of housing types with a focus on missing-middle housing, which may include new housing types that are currently not contemplated in this Plan.
 - c) Provides for a low-rise built form, by requiring in Zoning a minimum built height of 2 storeys, generally permitting 3 storeys, and where appropriate, will allow a built height of up to 4 storeys to permit higher-density low-rise residential development.

Response: The proposed development supports residential growth within the Inner Urban Transect by introducing eight (8) dwelling units in a low-rise, three-storey building. The proposal adheres to the intended height framework, providing a three-storey building that fits within the permitted range for low-rise development. This residential growth occurs in a neighbourhood close to various essential everyday needs and services, and this would encourage using active transportation modes for its residents, while still providing bike parking spaces in the front yard of the property.

4. The variances maintain the intent of the Zoning By-Law.

The purpose of the R4 - Residential Fourth Density Zone is to:

- (1) allow a wide mix of residential building forms ranging from detached to **low rise apartment dwellings**, in some cases limited to four units, and in no case more than four storeys, in areas designated as General Urban Area in the Official Plan;
- (2) allow a number of other residential uses to provide additional housing choices within the fourth density residential areas;
- (3) permit ancillary uses to the principal residential use to allow residents to work at home;
- (4) regulate development in a manner that is **compatible with existing land use**

patterns so that the mixed building form, **residential character of a neighbourhood is maintained or enhanced**: and

- (5) permit different development standards, identified in the Z subzone, primarily for areas designated as Developing Communities, which promote efficient land use and compact form while showcasing newer design approaches.

The list of permitted uses in the R4 zone includes **apartment dwelling, low rise**. Also, as per section 55, (1), (a) an **accessory use** is permitted in any zone if it is on the same lot as the principal use to which it is accessory. As proposed, the application presents permitted uses (a new three-storey residential building with a basement and an accessory enclosed garbage structure at the south end of the property) that is compatible with the existing land use patterns and maintains the residential character of the neighbourhood.

The intent of the minimum lot area requirement is to ensure a site is large enough to accommodate a low-rise apartment form, required setbacks, soft landscaping, and on-site functions (e.g., waste storage and bicycle racks) without overbuilding relative to the neighbourhood. At 335.99 m², the lot is 24.01m² below the 360 m² standard lot area. Eight units are a permitted low-rise apartment use within the R4 zone. The proposal exceeds the soft landscaping requirements in the front, rear and corner yards and also provides space for additional tree planting in the interior rear yard area. No on-site vehicle parking is proposed, which reduces land area demands. The objective of orderly, compatible development is therefore maintained. Further, under the new N3B zoning in Zoning By-law 2026-50 (Final Draft), there is no minimum lot area; instead, the by-law regulates maximum density, and this lot can permissibly accommodate eight units. In this context, the reduced lot area still upholds the intent of orderly, functional, and compatible low-rise development. *(noting that Version 3 of the new Zoning By-law is currently only a Planning Department recommendation and has not yet been adopted by Council or come into force, and is referenced here solely to illustrate how the proposal aligns with the emerging zoning framework)*

The intent of the minimum front yard setback is to create a clear, consistent street edge and a comfortable transition between the public sidewalk and private entrances, with room for landscaping and a positive public realm. The proposed 3.88 m front yard on Lévis Avenue replaces an existing condition where the building encroaches into City property and effectively provides a 0 m setback. By pulling the new building fully back onto the lot, the proposal removes this encroachment and establishes a proper landscaped front yard. The front yard soft landscaping is also significantly improved: the by-law requires 19.18 m² (40% of the front yard area), while the proposed development provides 33.12 m² (approximately 69.08%), almost entirely soft landscaping. This greatly enhances the streetscape and pedestrian experience. Further, under the new N3B zoning in Zoning By-law 2026-50 (Final Draft), the minimum front yard setback is 3 m, so the proposed 3.88 m would meet and exceed the future standard. Overall, the requested variance maintains the intent of the front yard setback provisions. *(noting that*

Version 3 of the new Zoning By-law is currently only a Planning Department recommendation and has not yet been adopted by Council or come into force, and is referenced here solely to illustrate how the proposal aligns with the emerging zoning framework)

The intent of the minimum corner side yard setback requirement is to provide appropriate separation to the side street, protect sightlines and pedestrian comfort at the corner, and allow space for soft landscaping and utilities while reinforcing streetscape rhythm. The requested reduction from 4.5 m to 2.02 m still maintains a generous perceived setback when the City space is considered: there is an additional approximately 1.85 m of City-owned, softly landscaped space between the lot line and the curb, resulting in a total distance of about 3.87 m from the building face to the street edge.

In the existing condition, part of the corner yard and the entire rear yard are paved and used for driveway and surface parking, even though the existing building is set back 3.16 m from the corner side lot line. The proposal removes these vehicular areas in the corner yard and rear yard and replaces them with soft landscaping. The corner yard will provide 36.24 m² of soft landscaping (41.53% of the corner yard area), exceeding the required 34.90 m² (40%). This reduces vehicle–pedestrian conflict, improves the public realm, and creates a greener, more comfortable corner condition. Nearby corner lots also provide reduced corner side setbacks (ranging from approximately 0 m to 2.3 m), so the proposed 2.02 m is consistent with the established pattern. Although the new N3B zoning as per the Zoning By-law 2026-50 (Final Draft), sets a 3 m exterior side yard, the proposed condition is closer to that standard than the current 4.5 m requirement and still fulfils the underlying intent of maintaining a safe, landscaped, pedestrian-friendly edge along the flanking street. *(noting that Version 3 of the new Zoning By-law is currently only a Planning Department recommendation and has not yet been adopted by Council or come into force, and is referenced here solely to illustrate how the proposal aligns with the emerging zoning framework)*

The intent of the minimum setback for projected stairways to a lot line is to keep structures at a safe distance from the lot line and travelled way, protect sightlines, and avoid encroachment that could impede pedestrian movement or compromise the public realm. The variance reduces the required stairway setback from 0.6 m to 0.13 m from the corner side lot line; however, the actual distance from the stairway to the edge of the street is approximately 1.98 m because there is about 1.85 m of City space between the lot line and the curb. The stairway is a small element within an otherwise fully landscaped corner yard, and it does not obstruct pedestrian movement or visibility at the intersection. Given the maintained separation from the street, the predominance of soft landscaping, and the absence of driveways or parking in this area, the intent of the stairway setback to protect safety, sightlines, and the public realm is effectively achieved.



Taken together, the four variances allow for a compact, low-rise apartment building that fits the existing and emerging zoning framework, enhances landscaping, removes existing encroachments and hardscaping, and improves the relationship between the building and the public realm. As such, the variances maintain the general intent and purpose of Zoning By-law 2008-250 and the forthcoming Zoning By-law 2026-50.

Performance Standard Review

Provision	Section	Required	Proposed	Compliance
Permitted uses	S.161, (1) S.55, (1), (a)	Detached dwelling, and accessory structure	Detached dwelling, and accessory structure	Y
Min. lot area	T.162A, (V)	360 m² (Low-rise Apartment, max of 8 units)	335.99 m²	N
Min. lot width	T.162A, (IV)	12 m (Low-rise Apartment, max of 8 units)	12.27 m	Y
Max. building height	T.162A, (VI)	11 m (Low-rise Apartment, max of 8 units)	9.95 m	Y
Min. front yard setback	T.162A, (VII)	4.5 m (Low-rise Apartment, max of 8 units)	3.88 m	N
Min. corner side yard setback	T.162A, (VIII)	4.5 m (Low-rise Apartment, max of 8 units)	2.02	N
Min. rear yard setback	S.144, (5), (b)	Rear yards on corner lots <ul style="list-style-type: none"> 1.2 m: units face different streets. 	4.11 m	Y



Min.int. yard area (for Corner lots with units facing different streets)	S.144, (6)	<ul style="list-style-type: none"> Interior yard width is 30% × affected lot width (12.27 m) = 3.68 m and the depth required is equal to rear yard setback =1.20 m 	4.42 m ² (3.68 m width and 1.20 m depth)	Y
Min. int. side yard setback	T. 162A, (X)	1.5 m (Low-rise Apartment, max of 8 units)	1.5 m	Y
Min. corner side yard soft landscaped area	T. 139(1)	40% of the corner side yard area: 34.90 m ² (40.00% of 87.26 m ²)	36.24 m ² (41.53%)	Y
Min. front yard soft landscaped area	T.161	40% of the front yard area: 19.18 m ² (40.00% of 47.94 m ²)	33.12 m ² (69.08%)	Y
Min. rear yard soft landscaped area	S.161, (15), (b), (i)	At least 35 m ²	36.22 m ² (71.17%)	Y
Min. aggregated rectangular area at the rear yard for the purposes of tree planting	S.161, (15), (b), (iv)	25 m ² and whose longer dimension is not more than twice its shorter dimension	25.17 m ² allocated of which is: 4.08 m width, and; 6.16 m depth	Y
Soft landscaping requirements of any yard other than the rear yard	S.161, (15), (c)	Any part of any yard other than the rear yard not occupied by accessory buildings and structures, permitted projections, bicycle parking and aisles, hardscaped paths of travel for waste and recycling management,	Softly landscaped as per the requirement	Y



		pedestrian walkways, permitted driveways and parking must be softly landscaped.		
Min. number of principal entrances	S.161, (15), (f), (i)	One; Located on front or corner facade	Two, both on front and corner facade	Y
Min. fenestration required on the front facade	S.161, (15), (g)	25% windows	27.89%	Y
Min. fenestration required on the corner side facade	S.161, (15), (g), (i)	15% windows	15.15%	Y
Min. number of balconies or porches provided on the front facade	S.161, (15), (j), (i)	one balcony or porch for each storey at or above the first storey is provided	one balcony is provided for each storey	Y
Min. horizontal area of balconies or porches	S.161, (15), (j), (iii)	2 m ²	7.66 m ²	Y
Motor vehicle parking requirements for lots < 450 m ²	S.161, (16), (a)	No motor vehicle parking is permitted	No motor vehicle parking is provided	Y
Bicycle parking space rates	T.111A, (b), (i)	0.5 per dwelling unit (8 units= 4 space)	4 spaces	Y
Bicycle parking space location	S.111, (6)	in any yard	Front yard	Y
Min. bicycle parking space dimensions	T.111B, (a & b)	Horizontal: 0.6 x 1.8 m	0.6 x 1.8 m	Y
Max. size and extent of open	T.65, (5), (b), (i), (2)	No closer than 0.6m to a lot line	0.13 m from the corner lot line	N



stairways projection into a corner side yard				
Max. size and extent of open stairways projection into a front yard	T.65, (5), (b), (i), (2)	No closer than 0.6m to a lot line	2.04 m	Y
Max. size and extent of covered or uncovered balcony projection into a front yard	T.65, (c) T.65, (6), (a), (ii)	2 m, but no closer than 1 m from any lot line Where not higher than 0.6 m above adjacent Grade: the greater of 2m or 50% of the required front yard= 2.25 m	2 m, and 1.86 m from front lot line	Y
Min. required setback of an accessory structure from a corner side lot line abutting a street	T.55, (2), (b), (II)	1.2 m	2.77 m	Y
Min. required setback of an accessory structure from a rear lot line not abutting a street	T.55, (3), (e), (ii), (II)	In a rear yard: 0.6 m	0.6 m	Y
Min. required distance of an accessory structure	T.55, (4), (II)	1.2 m	1.2 m	Y



from any other building located on the same lot				
Max. permitted height of an accessory structure	T.55, (5), (II)	3.6 m, with the height of the exterior walls not to exceed 3.2 m	3.2 m	Y
Max. number of accessory buildings permitted on a lot	T.55, (7), (II)	2	1	Y

We request the Committee of Adjustment authorise the requested variances in accordance with the plans filed as they relate to the variances being requested.

At this time we are submitting the following in support of the application:

- Completed application form for the Minor Variance application;
- Property owner's authorization for submission of the applications;
- Survey plan of the entire property;
- Site Plan showing the entirety of the proposed site;
- Elevations from all four sides;
- Tree Information Report;
- Application fees.

When the notification signs are ready for this application please email the undersigned and we will arrange for their installation on the property.

Should you have any questions or require anything further, please do not hesitate to contact the undersigned at (613) 695 0192 or via email at planning@p2concepts.ca.

P-Squared Concepts Inc.

Amrinder Kaur, Planner
 BPlan, MPlan

P-Squared Concepts Inc.

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 B.Sc, M.A, MPlan