

Subject: Zoning By-law Amendment – 6301 Campeau Drive

File Number: ACS2022-PIE-PS-0052

Report to Planning Committee on 23 June 2022

and Council 6 July 2022

**Submitted on June 10, 2022 by Derrick Moodie, Director, Planning Services,
Planning, Real Estate and Economic Development**

Contact Person: Lisa Stern, Planner, Development Review West

613-580-2424, 21108, lisa.stern@ottawa.ca

Ward: Kanata North (4)

Objet : Modification du Règlement de zonage – 6301, promenade Campeau

Dossier : ACS2022-PIE-PS-0052

Rapport au Comité de l'urbanisme

le 23 juin 2022

et au Conseil le 6 juillet 2022

**Soumis le 10 juin 2022 par Derrick Moodie, Directeur, Services de la planification,
Direction générale de la planification, des biens immobiliers et du développement
économique**

**Personne ressource : Lisa Stern, urbaniste, Examen des demandes
d'aménagement oust**

613-580-2424, 21108, lisa.stern@ottawa.ca

Quartier : Kanata Nord (4)

REPORT RECOMMENDATIONS

- 1. That Planning Committee recommend Council approve an amendment to Zoning By-law 2008-250 for 6301 Campeau Drive for technical zoning updates required to facilitate the development of a mixed-use development on the site, as detailed in Document 2.**

2. That Planning Committee approve the Consultation Details Section of this report be included as part of the 'brief explanation' in the Summary of Written and Oral Public Submissions, to be prepared by the Office of the City Clerk and submitted to Council in the report titled, "Summary of Oral and Written Public Submissions for Items Subject to *the Planning Act* 'Explanation Requirements' at the City Council Meeting of July 6, 2022, subject to submissions received between the publication of this report and the time of Council's decision.

RECOMMANDATIONS DU RAPPORT

1. Que le Comité de l'urbanisme recommande au Conseil d'approuver une modification du Règlement de zonage 2008-250 visant le 6301, promenade Campeau, afin d'apporter des mises à jour de zonage techniques nécessaires pour permettre la réalisation d'un aménagement polyvalent sur cet emplacement, comme l'expose en détail le document 2.
2. Que le Comité de l'urbanisme donne son approbation à ce que la section du présent rapport consacrée aux détails de la consultation soit incluse en tant que « brève explication » dans le résumé des observations écrites et orales du public, qui sera rédigé par le Bureau du greffier municipal et soumis au Conseil dans le rapport intitulé « Résumé des observations orales et écrites du public sur les questions assujetties aux 'exigences d'explication' aux termes de la *Loi sur l'aménagement du territoire*, à la réunion du Conseil municipal prévue le 6 juillet 2022 », sous réserve des observations reçues entre le moment de la publication du présent rapport et la date à laquelle le Conseil rendra sa décision.

EXECUTIVE SUMMARY

Staff Recommendation

Planning staff recommend approval of a Zoning By-law Amendment for 6301 Campeau Drive to permit technical amendments to the zoning exception. The application was previously approved by Council in October 2021, and there are no changes to the development concept previously contemplated by Council.

Applicable Policy

The site is located within the Town Centre designation as shown on Schedule B of the City's current Official Plan. The proposal supports these policies by providing a mix of

land uses at transit-supportive densities in proximity to the Terry Fox Rapid Transit Station.

The site is within the boundaries of the Kanata Town Centre Area in Volume 2b of the Official Plan. The proposal supports these policies by providing a mix of commercial and residential use on the site.

The Urban Design Guidelines for Transit Oriented Development apply to this site. They , aim to promote walkability and transit use in the site and building design. The proposal's attributes relate well to the relevant Urban Design Guidelines.

RÉSUMÉ

Recommandation du personnel

Le personnel d'urbanisme recommande l'approbation d'une modification au Règlement de zonage visant le 6301, promenade Campeau, afin d'apporter des modifications d'ordre technique à l'exception de zonage. Cette demande a déjà été approuvée par le Conseil en octobre 2021, et aucun changement n'est prévu au concept d'aménagement préalablement examiné par le Conseil.

Politique applicable

L'emplacement se trouve dans la désignation du Centre-ville de l'annexe B du Plan officiel actuel de la Ville. La proposition est conforme à ces politiques, car elle consiste en une variété d'utilisations du sol à des densités favorables au transport en commun, à proximité de la station du transport en commun rapide Terry-Fox.

L'emplacement est situé dans les limites du centre-ville de Kanata dans le Volume 2b du Plan officiel. La proposition est conforme à ces politiques, car elle offre sur place une variété d'utilisations commerciales et résidentielles.

Les lignes directrices en matière de conception urbaine pour les aménagements axés sur le transport en commun s'appliquent à cet emplacement. Elles ont pour objet de favoriser la marche et l'utilisation du transport en commun au moyen des conceptions d'emplacement et de bâtiment. Les caractéristiques de la proposition correspondent bien aux lignes directrices pertinentes en matière de conception urbaine.

BACKGROUND

Learn more about [link to Development Application process - Zoning Amendment](#)

For all the supporting documents related to this application visit the [link to Development Application Search Tool](#).

Site location

6301 Campeau Drive

Owner

Bayview Hospitality Holdings Ltd.

Applicant

Momentum Planning & Communications (Dennis Jacobs)

Architect

Fabiani Architect Ltd.

Description of site and surroundings

The subject site is located at 6301 Campeau Drive in the Kanata Town Centre as shown in Document 1. 6301 Campeau Drive is located on the south side of Campeau Drive, west of Cordillera Street, approximately 600 metres walking distance from the Terry Fox Rapid Transit Station. The site has an approximate area of 17,410 square metres and is characterized by rugged Canadian Shield terrain and existing tree and shrub cover.

To the north of the site is Campeau Drive, beyond which is a low-density residential neighbourhood.

To the east is Cordillera Street, beyond which is a six-storey residential building and hotel.

To the south is Bill Teron Park, which is undeveloped and well treed, and a portion of undeveloped right-of-way associated with Canadian Shield Avenue.

To the west is a 20-metre-wide City right of way and a vacant site known as 6475 Campeau Drive, also owned by the applicant.

Brief history of proposal

In October 2021, City Council approved an Official Plan and Zoning Bylaw amendment for 6301 and 6475 Campeau Drive (report [ACS2021-PIE-PS-0106](#)), to facilitate the development of a mixed-use development consisting of three 10-storey buildings, stacked dwelling units and commercial space fronting Cordillera Street and Canadian Shield Avenue. Subsequently, an appeal was filed with the Ontario Land Tribunal (OLT) by Clublink Corporation for issues relating to storm water management (OLT File Number: OLT-21-001721). Clublink Corporation has withdrawn the appeal for 6301 Campeau Drive only and By-law 2021-328 approved by Council is in full force and effect. The applicant wishes to proceed with development on the site.

As determined through the review of the concurrent Site Plan Control application (D07-12-20-0171), the timing for the construction by the City of the Canadian Shield Avenue extension along the southern property line of the site by the City of Ottawa is uncertain. As the road will not be constructed prior to the issuance of a building permit on the site, several zoning provisions are affected. A technical Zoning By-law amendment is required to address these inconsistencies; however, there are no changes to the development concept previously contemplated by Council.

Summary of requested Zoning By-law amendment proposal

The subject lands are zoned Mixed Use Centre Subzone 5 urban exception 2747 (MC5[2747]). The proposed rezoning is a technical amendment to clarify the following due to the timing of the construction of the extension of Canadian Shield Avenue:

- The setback of the portion of the building abutting the undeveloped portion of Canadian Shield Avenue.
- A parking rate of 0.5 spaces per unit in a mixed use building and 3.4 spaces/100m² for commercial space is required.
- A rear yard setback of 3 m instead of 4.6m is required as the underground parking garage extends above grade and becomes a retaining wall and planter.

The proposed zoning updates would not change the development illustrated in report [ACS2021-PIE-PS-0106](#).

DISCUSSION

Public consultation

Notification and public consultation were undertaken in accordance with the Public Notification and Public Consultation Policy approved by City Council for Official Plan and Zoning By-law amendments. A public information session was held virtually on February 10, 2021. The meeting was attended by the ward Councillor, city staff, the applicant and approximately 29 residents. Comments were received from 27 residents. Most comments were related to concerns about shadow impacts on low-rise residential dwellings north of Campeau, height, parking reduction, tree removals, and pedestrian connectivity across Campeau Drive. Comments were addressed through the review and approval of Zoning Bylaw 2021-328 ([ACS2021-PIE-PS-0106](#)). Eight residents submitted delegations to Planning Committee mainly related to the loss of trees on the site, parking and transportation impacts. The committee considered all submissions in making its decision and carried the report recommendations to approve the Bylaw.

Official Plan designation(s)

Current Official Plan

The site is located within the Town Centre designation as shown on Schedule B of the City's Official Plan which permits a broad variety of land uses at transit-supportive densities given its proximity to one or more stations along the rapid transit network and adjacent major roads.

Other relevant policies against which this proposal was reviewed (Sec. 2.2.2), urban design and built form (Sec. 2.5.1 and 2.5.6), and compatibility (Sec. 4.11).

New Official Plan

The proposed new Official Plan (OP), adopted by Council in November 2021, has been developed to reflect the opportunities and challenges that face the City as it continues to evolve. The proposed new Official Plan introduces a new Transect framework to further direct growth to the appropriate locations of the city. Until the new Official Plan is approved by the Province, the policies in the new Official Plan are informative and not determinative. The Council adopted Official Plan designates the subject site Town Centre (Hub) within the Suburban Transect and subject to the Evolving overlay.

Other applicable policies and guidelines

The site is within the boundaries of the Kanata Town Centre Area in Volume 2b of the Official Plan. The Kanata Town Centre Area Specific policies provide direction to create an active, vibrant core area containing a diverse mix of urban uses arranged in a form and a scale, which is both human and appropriate to its function.

Urban Design Review Panel

The property is within a Design Priority Area and the Zoning By-law amendment application was subject to the Urban Design Review Panel (UDRP) process. The applicant presented their proposal to the UDRP at an informal review meeting on July 2, 2020, and a subsequent formal review meeting on February 5, 2021, the latter which was open to the public. Details with respect to the UDRP review are contained within report [ACS2021-PIE-PS-0106](#). There are no changes to the proposed building as a result of this report.

Planning rationale

This application has been reviewed under the consolidated Official Plan (2003) and amendments in effect from Official Plan Amendment 150 (OPA 150).

The site is designated as a Town Centre (3.6.2). Town Centres occupy strategic locations on the Rapid-Transit network and act as central nodes of activity within their surrounding communities and the city as a whole. Town Centres are intended to achieve 10,000 jobs, and high-density housing including high-rise buildings provided that appropriate transitions are provided to the surrounding area. In this regard, the proposed development is consistent with these policies by providing mixed use development.

Section 2.2.2 – Managing Growth Within the Urban Area

Section 2.2.2 of the Official Plan supports intensification throughout the urban area where there are opportunities to accommodate more jobs and housing and increase transit use. Intensification is directed to Target Areas for intensification which have the potential to develop at moderate to high densities in a compact form. The subject lands are within Kanata Town Centre, which is identified as a Target Area for intensification with a minimum density target of 120 people and jobs per hectare. The proposed development will contribute towards these density targets.

The Official Plan states that all intensification will occur in accordance with the provisions of Section 2.5.1 and 4.11, dealing with matters of urban design and compatibility.

Section 2.5.1 – Designing Ottawa

Tools and design objectives for new development are provided in Section 2.5.1 to guide compatibility and a high quality of design. The application supports the Design Objectives of the Plan listed below as follows:

1. To enhance the sense of community by creating and maintaining places with their own distinct identity.

It is the vision of the Kanata Town Centre to be an active, vibrant core area containing a diverse mix of urban uses arranged in a form and a scale, which is both human and appropriate to its function. The building is located close to the street frontage while maintaining appropriate setbacks for street trees and landscaping and provides commercial uses facing Cordillera Street.

2. To define quality public and private spaces through development.

The proposed development provides a high-quality and attractive public realm. Landscaping will be provided along all public roadways.

3. To ensure that new development respects the character of existing areas.

The proposal maintains the expectations for a transit oriented urban form as considered in the Kanata Town Centre Special Policies. The site design has maintained a large well-treed rocky outcropping on the site.

4. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

The proposal to build a mix of unit types and sizes will contribute to the choice and availability of housing options within proximity to transit.

5. To understand and respect natural processes and features in development design.

A significant portion of the site will remain naturalized.

6. To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

The site is well served by local public transit and the site is within a 600m radius of the Terry Fox Rapid Transit Station. It is also within easy walking distance of both day-to-day services and extensive shopping and entertainment along Kanata Avenue.

Section 4.11 – Urban Design and Compatibility

New development is reviewed and evaluated using the policies of Section 4.11, which address urban design and compatibility. The purpose of reviewing these design aspects is to ensure that new development is sensitive and compatible to the existing context while providing appropriate transitions between densities and land uses. Section 4.11 references compatibility of new buildings with their surroundings through setbacks, heights, transitions, colours and materials, orientation of entrances, and location of loading facilities. The development provides an urban form as contemplated by the Kanata Town Centre Special Policies and the proposed buildings are compatible in scale with surrounding development.

Kanata Town Centre Area

Within Volume 2B – Site Specific Policies of the City’s Official Plan, the site is located within the Kanata Town Centre and is designated as Central Business District on Schedule B-1.

5.7.5.1 Central Business District

It is the intent of this designation that these lands become an urban place that provides for employment uses, complemented by higher density residential uses. Uses fronting Campeau Drive should be residential in nature. The proposal supports the vision of the Central Business District by providing commercial uses facing Cordillera Street and Canadian Shield Avenue and providing residential uses facing Campeau Drive.

New Official Plan

The subject site is within a Town Centre (Hub) designation within the Suburban Transect. The Kanata Town Centre policies have been carried forward in the Council adopted Official Plan as an Urban Secondary Plan in Volume 2A. Hubs are areas centred on planned or existing rapid transit stations and/or frequent street transit stops. The planned function of Hubs is to concentrate a diversity of functions, a higher density of development, a greater degree of mixed uses and a higher level of public transit connectivity than the areas abutting and surrounding the Hub. The Evolving overlay is applied to areas in close proximity to Hubs and Corridors to signal a gradual evolution

over time that will see a change in character to support intensification, including guidance for a change in character from suburban to urban to allow new built forms and more diverse functions of land. The proposal meets the intent of the new Official Plan.

Zoning By-law

The applicant has requested relief as follows for the proposed development:

- Clarify setback of the portion of the building abutting the undeveloped portion of Canadian Shield Avenue.

Due to the timing of the construction of the Canadian Shield Avenue extension the lot line abutting Canadian Shield Avenue is not considered a side yard abutting a street and must be specifically denoted in the zoning exception for the site.

- Specify a parking rate of 0.5 spaces per unit in a mixed use building and 3.4 spaces/100m² for commercial space.

As the Canadian Shield Avenue extension will not be constructed at the time of building permit issuance, walking distance to the Terry Fox rapid transit station will temporarily exceed 800 m requiring an update to the zoning. 73 excess parking spaces intended for shared use with the future phase of development at 6475 Campeau Drive are available for use on the site. Additionally, visitor parking is provided to discourage visitors from parking on local streets. No changes to the parking rates provided on the site have been proposed.

The Official Plan encourages intensification and development where there are opportunities to support alternative modes of travel from the car. The site is located within close proximity of a planned transit station. In support of the application, a Transportation Impact Assessment (TIA) was submitted and reviewed by staff, who found the report to be satisfactory.

- Rear yard setback of 3 metres vs. 4.6 metres.

At the time of rezoning, it was unclear that a portion of the underground parking garage extended into a retaining wall and planter. This structure would not meet the definition of 'projection' in the Zoning By-law and principal building setbacks apply. An exception is required to amend the rear yard setback from 4.6 metres to 3 metres. There are no changes to the plans submitted in support of Council approved By-law 2021-328 as a result of this technical amendment.

Provincial Policy Statement

Staff have reviewed this proposal and have determined that it is consistent with the 2020 Provincial Policy Statement.

RURAL IMPLICATIONS

There are no rural implications with this report.

COMMENTS BY THE WARD COUNCILLOR

Councillor Curry provided the following comments:

“Our main concerns (shared by the community) are the traffic issues on Campeau Drive that require reconstruction of Campeau, we think. Kanata Avenue as well will be impacted, and it was supposed to be redone as well. The loss of trees is always a concern to the community despite the fact that the land is owned privately, and tree permits were followed. One final concern by the community is the reduced parking that, even though it is near to the LRT, the reduced parking allotment in current buildings means that there is considerable street parking creating further issues.”

LEGAL IMPLICATIONS

There are no legal implications associated with implementing the recommendations contained within this report.

RISK MANAGEMENT IMPLICATIONS

There are no risk management implications.

ASSET MANAGEMENT IMPLICATIONS

It has been confirmed that there is sufficient capacity in the existing water, wastewater and stormwater infrastructure systems to accommodate the proposed development. The on-site infrastructure will remain under private ownership and therefore will not add to the City's inventory of assets to be managed.

FINANCIAL IMPLICATIONS

There are no direct financial implications.

ACCESSIBILITY IMPACTS

The new building will be required to meet the accessibility criteria contained within the Ontario Building Code. The Accessibility for *Ontarians with Disabilities Act* requirements for site design will also apply and will be reviewed through the Site Plan Control application.

ENVIRONMENTAL IMPLICATIONS

The applicant submitted an Environmental Impact Study and Tree Conservation Report as a part of the review and approval of By-law 2021-328. The Environmental Impact Assessment and Tree Conservation Report identified butternut trees within the application area. As butternut trees are identified in the *Endangered Species Act*, approval is required from the Ontario Ministry of Environment for their removal.

The Planning Forester is satisfied that requirements for tree removal have been met and a Tree Cut Permit was issued in March 2022 to avoid the timeframe between April 15 and August 15 where tree removals are not permitted to protect nesting birds.

TERM OF COUNCIL PRIORITIES

This project addresses the following Term of Council Priorities:

- Economic Growth and Diversification
- Thriving Communities
- Integrated Transportation

SUPPORTING DOCUMENTATION

Document 1 Location Map

Document 2 Details of Recommended Zoning

Document 3 Proposal Details

CONCLUSION

The proposal complies with the Official Plan and represents good planning. The Zoning By-law Amendment is recommended for approval.

DISPOSITION

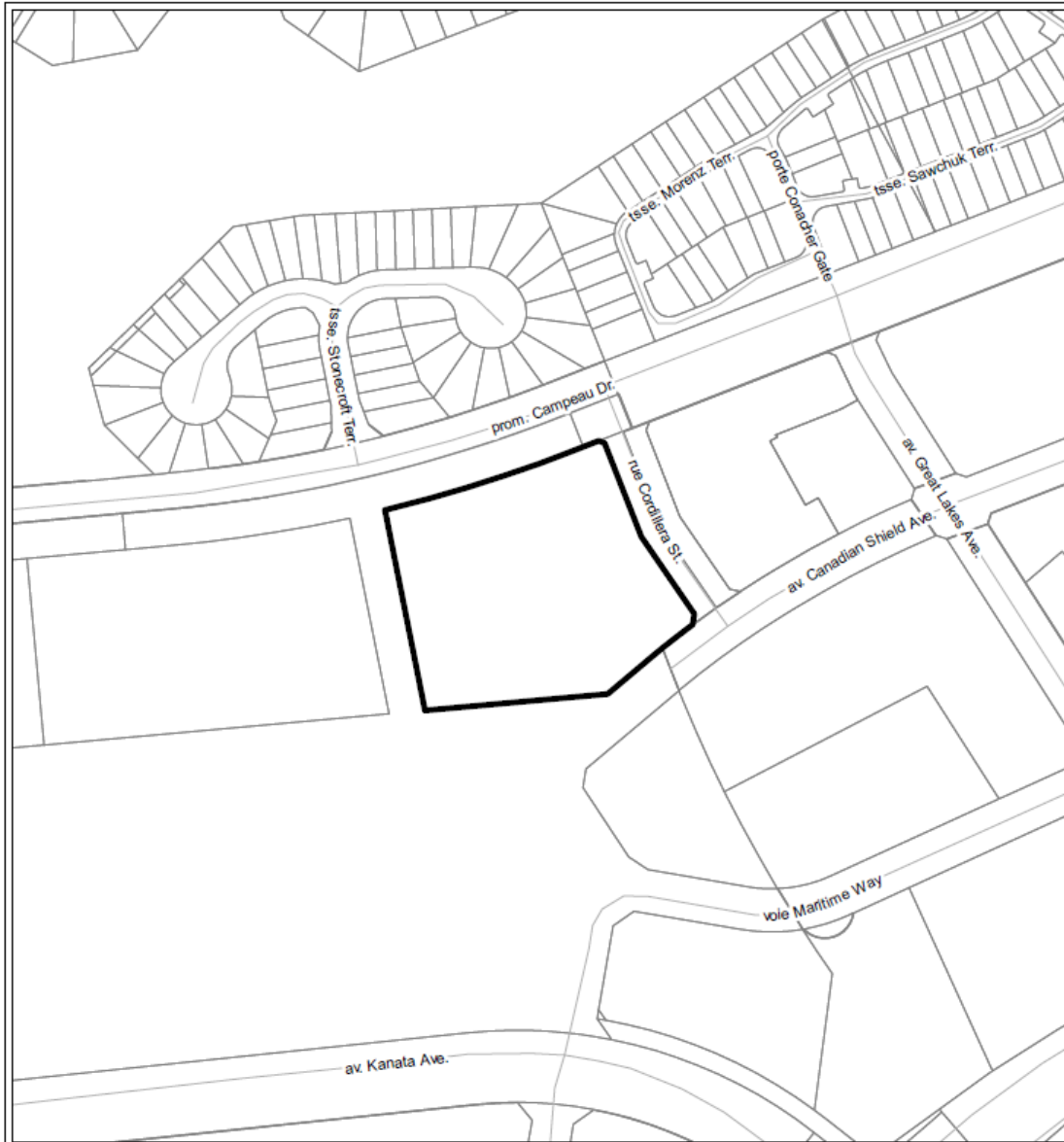
Office of the City Clerk, Council and Committee Services to notify the owner; applicant; Krista O'Brien, Program Manager, Tax Billing & Control, Finance Services Department (Mail Code: 26-76) of City Council's decision.




Zoning and Interpretations Unit, Policy Planning Branch, Economic Development and Long Range Planning Services to prepare the implementing by-law and forward to Legal Services.

Legal Services, Innovative Client Services Department to forward the implementing by-law to City Council.

Planning Operations Branch, Planning Services to undertake the statutory notification.

Document 1 – Location Map



		LOCATION MAP / PLAN DE LOCALISATION ZONING KEY PLAN / SCHÉMA DE ZONAGE SITE PLAN / PLAN D'EMPLACEMENT	
D02-02-20-0124 D07-12-20-0171	22-0388-L	 6301 prom. Campeau Dr.	
©Parcel data is owned by Terraset Enterprises Inc. and its suppliers. All rights reserved. May not be produced without permission. THIS IS NOT A PLAN OF SURVEY		 <small>NOT TO SCALE</small>	
©Les données de parcelles appartiennent à Terraset Enterprises Inc. et à ses fournisseurs. Tous droits réservés. Ne peut être reproduit sans autorisation. CE CI N'EST PAS UN PLAN D'ARPENTAGE			
REVISION / RÉVISION - 2022 / 04 / 27			

Document 2 – Details of Recommended Zoning

The proposed change to the City of Ottawa Zoning By-law No. 2008-250 for 6301 Campeau Drive:

1. Amend Section 239, Urban Exceptions, by amending exception 2747 with provisions similar in effect as follows:
 - a. In Column V, delete the following provisions:
 - i. Minimum rear yard setback: 4.6 m
 - b. In Column V, include provisions similar in effect to the following:
 - i. Minimum rear yard setback: 3 m
 - ii. Minimum setback from the southeastern most lot line: 0 m
 - iii. Despite Section 101, the minimum parking space rate for a dwelling unit in a mixed-use building shall be 0.5 spaces per dwelling unit.
 - iv. Despite Section 101, the minimum parking space rate non-residential uses is 3.4 spaces/100m²

