

# CULTURAL HERITAGE IMPACT STATEMENT FOR 65 STEWART STREET, OTTAWA

REVISED JULY 28, 2021  
JUXTA PROJECT NO. 2105



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**APPENDICES**

**Appendix 1** – Heritage Permit Application (Robertson Martin Architects)

**Appendix 2** – Sandy Hill West Heritage Conservation District Plan

**Appendix 3** – Heritage documentation

**Appendix 4** – As-found drawings

## **1. INTRODUCTION**

**Note: This document has been revised to reflect the updated design documents provided by Robertson Martin Architects on July 28, 2021.**

Juxta Architects Inc. (the *Consultant*) was retained in February 2021 by Sam Elias (the Client) to provide a *Cultural Heritage Impact Assessment* (CHIS) for a proposed development at 65 Stewart Street, Ottawa (*the Site*).

*Section 4.6.1* of the City of Ottawa *Official Plan* has policies that outline when a CHIS is required, and which will evaluate the impact of a proposed development on cultural heritage resources when development is proposed that has the potential to:

- Adversely impact the cultural heritage value of properties designated under *Part IV* of the Ontario Heritage Act (OHA); and
- Adversely impact the cultural heritage value of districts designated under *Part V* of the OHA.

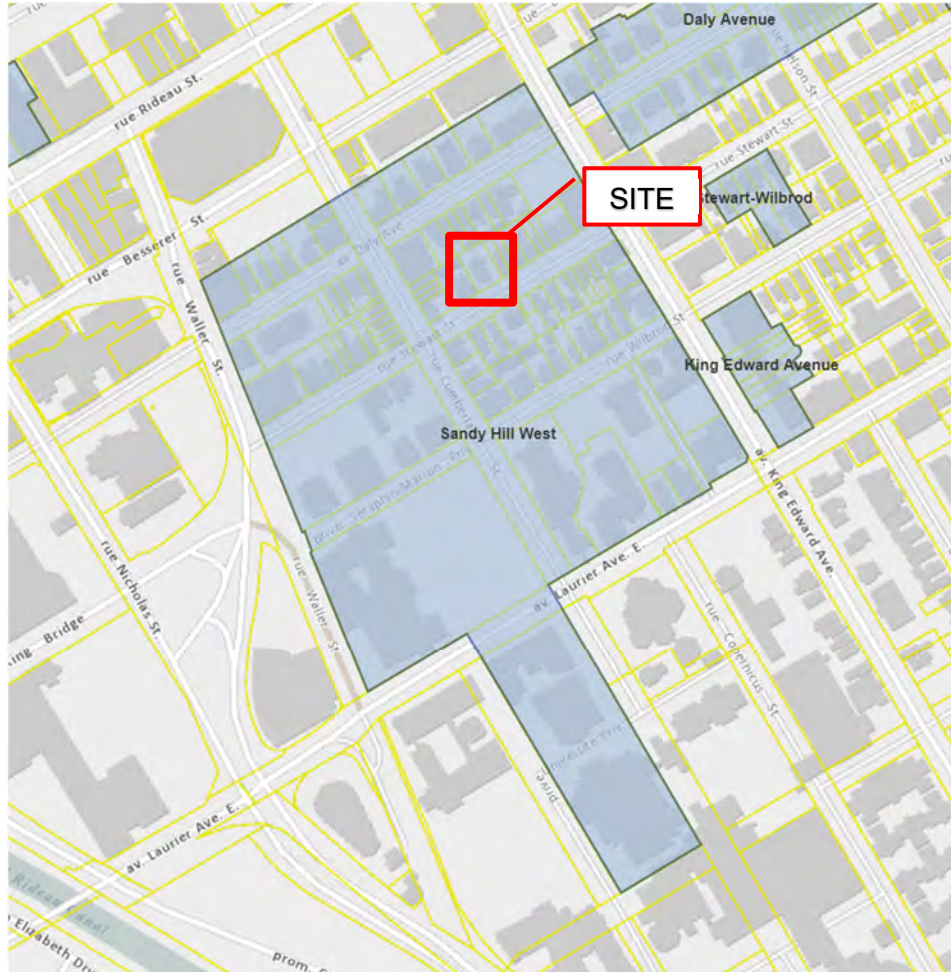
In addition:

- A CHIS may also be required for development applications adjacent to or within 35 meters of designated buildings and areas; and
- A CHIS is required when demolition is proposed.

This proposal is subject to a CHIS due to its individual Part IV OHA designation and location within the *Sandy Hill West Heritage Conservation District* (Part V, OHA). This report will examine the development proposal with regards to the *Heritage Character Statement* and *Design Guidelines* outlined in the conservation district plan and the *Heritage Survey and Evaluation Form*.

The Consultant has been provided with development proposal plans (refer to Appendix 1), and the content of this report is based on those plans.

## **2. GENERAL INFORMATION**



**Figure 1:** The Sandy Hill West Conservation District

### **2.1. ZONING**

The City of Ottawa Comprehensive Zoning By-law (2008-250) designates the area of the subject property as **R4UD S70** (*Residential Fourth Density, Subzone UD*). The UD subzone allows for a variety of residential configurations and includes different setbacks for differing building types (townhouse, low-rise, etc.).

It should be noted that relief from the existing zoning provisions is being sought by the proponent:

- Rear yard set-back
- Rear yard landscaping
- Height of the addition
- Configuration of the addition

## 2.2. HERITAGE DESIGNATION

The house is designated under Part IV of the Ontario Heritage Act as a *Category 2* building (refer to Appendix 3) as well as under Part V of the Ontario Heritage Act as falling within the boundaries of the Sandy Hill West Conservation District.

According to the background documentation, the building has a heritage plaque dating to 1988, and its recommendation for individual designation predates the formation of the current Part V district.

In addition, there are other Part IV designated buildings in close proximity to the property:

- 473 Cumberland Street (St. Paul's Church)
- 106-110 Daly Avenue
- 112 Daly Avenue (Lyon House)
- 66-68 Stewart Street



**Figure 2:** Locations of the adjacent heritage buildings

The possible impacts on these adjacent sites is elaborated in Section 6 of the report.

### **2.3. DESCRIPTION OF BUILDING**

65 Stewart Street (alternatively know as *Jarvis House*) was originally constructed in 1886 and is a 2-1/2 storey wood-framed house, with a front gable and constructed in the Queen Anne vernacular style. It includes two rear additions: one two storey and one one-storey. It is clad with a painted wooden clapboard, and includes decorative bargeboards, shouldered window trim, projecting bay window, and a projecting asymmetrical porch facing the street. It is set back on the lot, which provides green space on the street and “front garden” feel to the site.



**Figure 3:** Street façade (1992, courtesy City of Ottawa)

### **2.4. BACKGROUND DOCUMENTS AND METHODOLOGY**

In addition to visits to the site and surrounding areas, the following documents were consulted during the preparation of this report:

- Drawings prepared by Robertson Martin Architects, **dated 2021-07-28 (revised)**.
- *Heritage Survey and Evaluation Form*; J. Smith & City of Ottawa; May, 1992.
- *Statement of Cultural Heritage Value*; City of Ottawa;
- The City of Ottawa *Official Plan* and zoning by-law;
- Historicplaces.ca – Sandy Hill West Conservation District;
- *Standards & Guidelines for the Conservation of Historic Places in Canada*, Parks Canada;
- *A Guide to Preparing Cultural Heritage Impact Statements*, prepared by the City of Ottawa, Draft, March 2012.
- *Sandy Hill West Heritage District Conservation Study*; Kayla Jonas Glavin, Robert Shipley, et al.; University of Waterloo; December 2012.

Juxta Architects Inc. also participated in a heritage pre-consultation meeting with members of the design team, the Client, and the City of Ottawa.

### **3. CURRENT CONDITIONS – DEVELOPMENT SITE**

**NOTE:** Refer to Appendix 4 for as-found drawings of the building.

#### **3.1. SITE ANALYSIS**

Located at 65 Stewart Street, within the Sandy Hill West conservation district, the existing building is a 2 ½ storey front-gabled residence with wood cladding, a front porch and decorative wood elements. The building has retained the appearance of a Victorian-era cottage.

The building's urban scale is different from that of the surrounding buildings. Notably, the buildings on either side (61 & 75 Stewart Street) are substantially boxier, taller and situated closer to the street.

This building is unique on the street in how it addresses the public domain; it is set back from the sidewalk, does not include the wide typical entrance stairs, and does not present a formal address to the street that recognizes the public domain in the same manner as its neighbours. Instead, the cottage-like appearance of the building reinforces a sense of privacy and showcases the front-yard garden condition as opposed to a formal street-front façade.

This difference in design intent should be maintained. Whereas a heritage house that is a “typical” example contributes to the urban fabric by way of the regularity of design and contribution to the overall urban typology, this building's history and design have allowed it to remain as a counterpoint to many of the other

adjacent buildings. Since this status as “something different” is an integral part of the building and neighbourhood’s history, it should be preserved as part of any proposed development.

The site also includes a mature blue spruce in the front yard (the same tree in sapling form can be observed in Figure 2), and an adolescent deciduous tree. Both trees are among the larger trees on the street and contribute positively to the street condition. In addition, the trees serve to reinforce the sense of privacy of the building, as noted above.

This building was specifically noted in the conservation district plan for these qualities.



**Figures 4 & 5:** Oblique views of the building from both directions on the street.



**Figures 6 & 7:** LEFT: A front view of the façade now. RIGHT: a view of the street and neighbouring conditions (courtesy Google)



## DEVELOPMENT GUIDELINES

As per section VI.4 of the Sandy Hill West Heritage Conservation District Plan as well as the *Standards and Guidelines*, alterations to heritage buildings should seek to preserve the heritage qualities of the building and site.

The district plan also makes the following specific recommendations with regards to rehabilitation and adaptive re-use:

### VI.4.3.3 Rehabilitation and Adaptive Re-use

Rehabilitation is the modification of a heritage building to contemporary functional standards. It may involve some restoration, but also introduces new forms and materials to accommodate new requirements. Sensitive rehabilitation achieves these functional goals while protecting and even enhancing the qualities that give the property its unique heritage character.

Adaptive reuse involves a change in use or occupancy, often from an original residential or mixed use to a retail or other commercial use. Adaptive reuse will be limited in the Sandy Hill West area, because most of the buildings will retain their original residential or institutional uses.

#### Recommendations:

1. As with restoration, good rehabilitation is based on good historical documentation.
2. If new or modified uses are proposed, it should first be determined whether these can be contained within the heritage property without undue damage to the layout or finishes.
3. Structural upgrading, if required, should respect the original design intentions and reinforce these rather than working independently of them.
4. New mechanical and electrical services and other environmental control systems including insulation should be introduced in ways which do not upset the existing equilibrium or create new possibilities for deterioration.
5. Architectural layout and finishes should be respected, both inside and outside a property.
6. Distinctive features and good examples of design and craftsmanship should be retained. The contribution of all periods to the history of the property should be respected.
7. New work should be of its own time, but subservient to the heritage character of the existing property. It should take its form and direction from the history of the property itself.

With regards to this proposal, particular attention should be paid to points 5, 6, & 7. It is the scale of the any proposed development and the nature and quality of the interventions that must be sensitive to the heritage place.

In addition, as part of a district the development should retain the historic urban relationship between the building and the street (setbacks, greenspace, etc.).

## **4. STATEMENT OF SIGNIFICANCE**

### **4.1. ARCHITECTURAL VALUE**

The architectural value of the building is derived from its style and setting. It is a well-preserved example Queen-Anne vernacular, and the later additions (located at the rear and side of the building) do not detract from the building's character. The building is unique in character in the immediate environs.

Some of the notable exterior features include:

- The front-gabled design, which is unique in the immediate area,
- The horizontal wood cladding including the profile with shadow-line,
- Decorative elements, including:
  - Intricate front bargeboard,
  - Arched gable window,
  - Front porch cornice, columns and rails,
  - Bay window cornice and pilasters matching the porch,
  - Shouldered window trim with “keystone” detailing,
  - Paneled front doors, with stained glass inserts and transoms,
  - Original single-hung front windows (ground floor) with diamond patterned glazing and stained glass detailing, and
- The picturesque massing, particularly the relationship between the front gable and the octagonal porch bay.



**Figure 8:** Various exterior notable design features

#### **4.2. ENVIRONMENTAL VALUE**

This building contributes to the overall character of the street and the conservation district. As per the district plan:

*“The built form of Sandy Hill West is outstanding for its high proportion of early building stock. Most of this is from the 1880-1920 period, with contributions from the surrounding earlier 1840-80 and later 1920-50 periods...Sandy Hill West is a transition area. It is a small eight block area where diverse themes of the surrounding areas meet, mix, and emerge as distinct entities on opposing borders. As such, this area holds intact remnants from many divergent parts of our cultural heritage – elements that have often disappeared in their purer contexts.”*

As such, this building forms an important part of the pattern of development and tells a part of the story of the local history and is an important piece of urban fabric and cultural history.

In particular, the orientation of the building on the street, and the generous setback, which is unique in the area are both distinguishing features that should be preserved.

As per VI.4.2 of the district plan states: *“Because there are so few vacant lots within the ...district, the older buildings exist not only as individual fragments or architectural and historical interest, but also as components of significant streetscapes. It is important that this sense of historical and physical continuity be maintained and enhanced.”*

#### **4.3. HISTORICAL & CULTURAL VALUE**

The home was originally constructed by Samuel Jarvis, an early Ottawa photographer. This connection is noteworthy because Samuel Jarvis provided a sizeable portion of the documentary evidence from Ottawa of that period. Jarvis was also responsible for developing 88-94 Stewart, and 98 Stewart.

The house was sold to the Eastern Methodist Church in 1893 for use as the church manse, until the church closed its doors in the mid-1920's.

#### **4.4. CONSERVATION DISTRICT**

The Sandy Hill West Heritage Conservation District was designated in 1994, and is bounded by Daly Avenue, King Edward Avenue, Laurier Avenue East and Waller Street. It consists of approximately 90 residential and institutional buildings. The conservation district plan (Appendix 2) was written by Julian Smith & Associates, Margaret Carter, Cecilia Paine and Associates Inc. and Jane Ironside. The plan

includes a comprehensive study of the urban conditions and provides design guidelines for future alterations and development.

The Canadian Register at [www.historicplaces.ca](http://www.historicplaces.ca) describes the heritage character as follows:

*The heritage value of the district of Sandy Hill West lies in its associations with the growth and development of the City of Ottawa in the late nineteenth century, its connection with prominent institutions (including the University of Ottawa) and its architectural variety.*

*Sandy Hill West is a relatively intact residential neighbourhood with the majority of buildings dating from the 1860s to the 1920s. The area of Sandy Hill West developed on a portion of land granted to Quebec City notary, Louis Besserer. From 1840-1880, the area saw little growth until the choice for Ottawa as the capital of Canada in 1857 by Queen Victoria. Besserer conveyed six lots to the Roman Catholic diocese, which became the site of Bytown College, Canada's first bilingual educational institution. The Oblate Fathers and a number of other religious bodies built churches and schools in the area. A number of influential, affluent residents were attracted to the area, and residential development accelerated in the late 1860s and early 1870s.*

*The majority of development occurred from 1880 – 1920 when the area was redeveloped as part of the rapid change and intensification in the core area of Ottawa as the city's population quadrupled. Within Sandy Hill West, lots were redeveloped to provide smaller single family and multiple unit residential properties. The area was middle class, with working level civil servants, railway employees and merchants. Some of the churches were also rebuilt on a larger scale in order to serve citizens across the city. Sandy Hill West is an important indicator and remnant of the incredible growth that Ottawa experienced in the late nineteenth century.*

...

*Sandy Hill West represents an unusually rich cross-section of Ottawa architecture over the last one hundred and fifty years. The survival rate has also been quite high; over 80% of the buildings date from before 1920.*

...

*A variety of architectural styles and expressions are represented in Sandy Hill West. Despite the diversity of building stock, they are unified by the dignified decorative and ornamental elements, added to create a modest prestige in the*

*district. The architecture is also representative of new building techniques being employed in Ottawa for the first time, such as the widespread use of stone and brick veneer, the emergence of the front gable and flat roofed building, as well as new preferences regarding siting and orientation. Overall, the architecture of Sandy Hill West represents a great diversity of styles and expressions, which contribute to the heritage character of the district.*

## **5. DESCRIPTION OF DEVELOPMENT PROPOSAL**

**NOTE:** Refer to **Appendix 1** for detailed drawings of the development proposal.

### General Description

The development proposal consists of a rear and side-yard addition that increases the footprint of the building. The existing building footprint is proposed to be expanded from 113m<sup>2</sup> to 266m<sup>2</sup> (increase of 153m<sup>2</sup> or 135%). The proposed addition is a wood-framed, three storey structure with a flat roof and sloped roofs facing the street (south) and east facades.

The development proposal increases the unit count from 2 units to 12 units. The unit types vary, with bachelor, one-bedroom, and two-bedroom units included in the design.

### Site Plan

The footprint of the addition extends to the north (rear) and east of the existing building and creates a new orientation to the street on the site, including a new entranceway. The main mass of the new addition occurs on the north, in the building's rear yard. The eastern part of the addition is set back approximately 3.3m (11') from the face of the existing building. The new addition's set back from both the street and face of the existing building serves to maintain the primacy of the original façade and relationship to the street.

In the front, a new concrete pathway is proposed as a secondary entrance. This pathway is connected to a side-facing door that enters to a common area for the building. The drawings indicate that the existing front-yard trees shall remain in place. The drawings do not indicate any new front yard fencing or other gardens, planting or landscaping features.

The west side of the building maintains the existing right-of-way, with a chamfered corner for vehicle turning and safety.

A new service entrance on the east side of the building opens into the lot set-back and provides access to the service room. As a result of the proposed addition, the green space on the east side of the site has been reduced to accommodate the addition.

Three spots for exterior bicycle parking are proposed in proximity to the south-east corner of the building.

## Architecture

The drawings indicate that the street-facing heritage facades of the original building will be preserved, including the leaded glass, wood windows, original door and transom, exterior mouldings, bargeboards, finials, and other decorative elements. The existing wood siding and white colour will be maintained on the existing and heritage portions of the building, whereas the new addition will be clad in a grey plank material.

The proposed cladding at the rear of the addition and at the rear portions of the east and west facades are a cementitious board (Hardi Plank) in a grey colour. These areas are not visible from the streetscape. The cladding on the street-facing portion of the new addition (south) and the east facing façade is proposed to be in a painted wood board which matches the profile of the original siding, and painted a distinguishing grey colour.

The existing trees on the south side of the site have been proposed to be retained. These existing trees will serve to minimize the appearance of the new addition. However, given the scale of the proposed intervention, the consultant is skeptical whether the trees will be able to be successfully conserved. For this reason, the design of the new addition should not rely on the presence of trees to mask its appearance but should be designed in such a manner as to provide a sensitive heritage addition in any case.

The addition wraps the north and east sides of the existing heritage building, and includes a front-gabled street address at the east, and a flat roof in the rear. The top of the flat roof parapet is approximately 750mm above the current highest point of the existing roof peak.

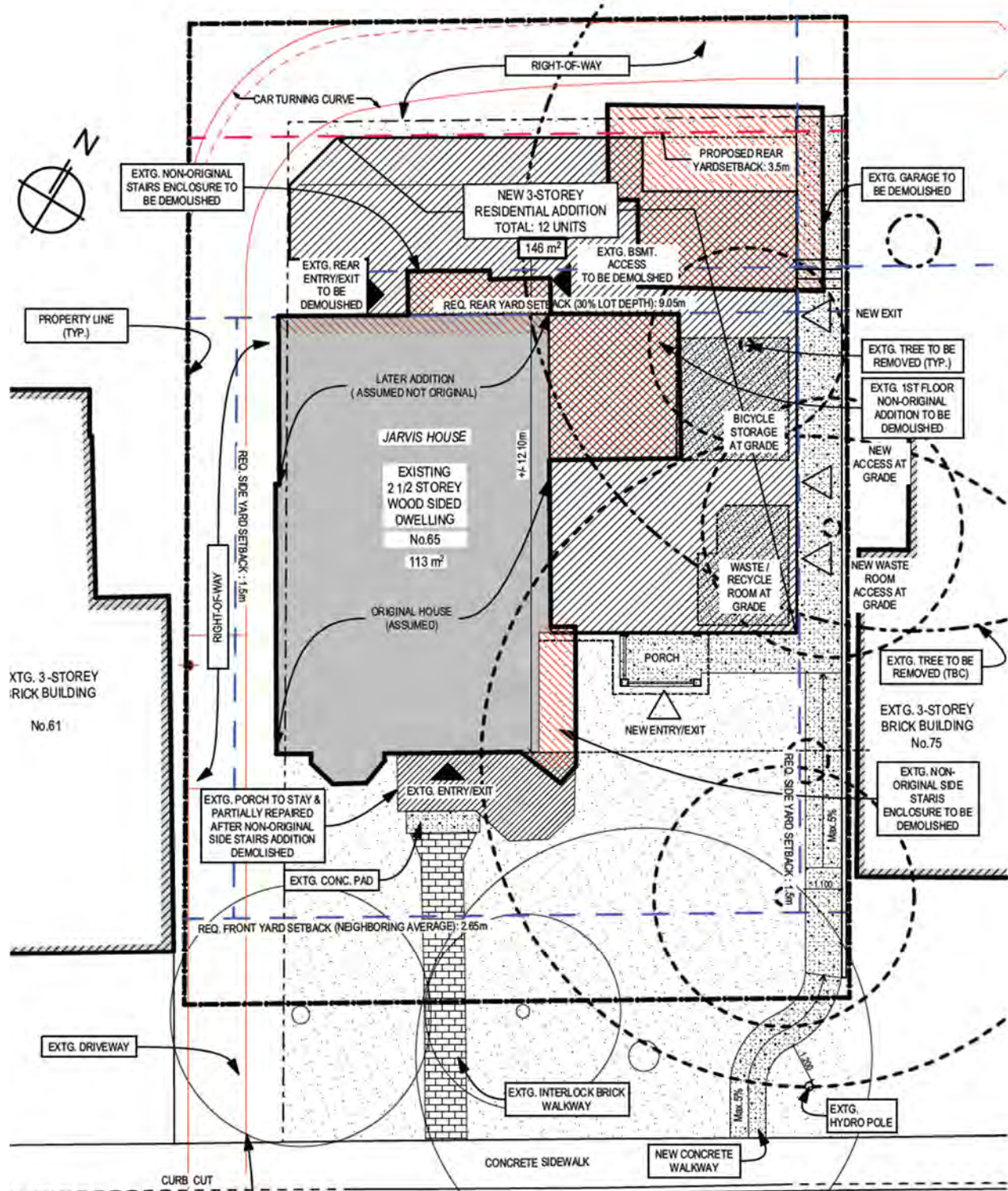
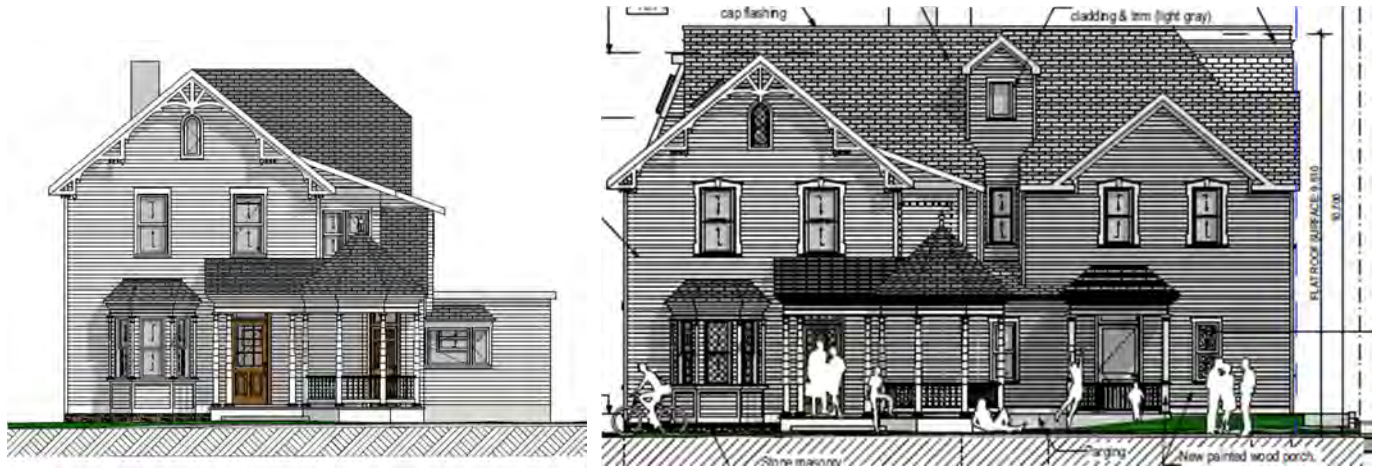


Figure 9: The proposed site plan, with areas to be demolished shown in red (revised July 28, 2021).



**Figure 10:** The south elevations facing Stewart Street with the existing (left) and proposed (right) (revised July 28, 2021).

## 6. IMPACT OF DEVELOPMENT & MITIGATION STRATEGIES

The following matrix is provided to list the various impacts and considerations for the proposed design:

Impacts & Considerations	Comments & Mitigations Strategies
<b>Heritage District Considerations</b>	
<p><b>Given the high degree of authentic building stock in the district, does the development proposal conserve and maintain the existing building?</b></p>	<p>The development proposal is in partial conformity with this recommendation; the building’s outer shell and massing will remain, whereas the interiors will be modified.</p> <p>With regards to the building’s orientation and street address, the Consultant assesses that the most prominent and “high value” portions of the building will be conserved (the Stewart Street elevation). The building interiors are not listed as character-defining elements in the background documentation.</p> <p>The existing building’s Queen Anne Vernacular style will be conserved, and an understanding of the original building’s massing and design intent will remain clearly visible from the street.</p>



	<p>Overall, the Consultant assesses that the interior reconfiguration of the building is an acceptable change which includes minor impacts to the district and building heritage values.</p> <p>With regards to the building’s overall massing, representatives of the community have expressed concern that the overall scale of the addition will overwhelm the existing building. While the addition is larger than the existing building, it is the Consultant’s opinion that the proposed setbacks provide enough breathing room for the existing building to be understood as the original design element on the site. Given that the building’s contribution to the urban environment and heritage value is related to the street façade, the massing at the rear is not of particular concern.</p> <p>Currently, the rear yard is a fully paved area and does not contribute to the picturesque streetscape and is largely unnoticed by the public.</p>
<p><b>Is the proposed development in keeping with the historic character of the district and the immediate streetscape? As per the district plan, does it “compliment the heritage qualities of the area, and create a comfortable, safe and pleasant street environment”?</b></p>	<p>This is an important consideration. The consultant notes that two factors appear to be contributing to this potential impact:</p> <ol style="list-style-type: none"> <li>1: The continuity of the setbacks, and prominence of the proposed addition relative to the existing building, and</li> <li>2: The appearance of street-facing facades.</li> </ol> <p>With regards to #1, the Consultant’s opinion is that the new addition’s set-back provides ample “breathing room” around the existing building and street. The impression of the front yard “private garden” strategy will be maintained and could be reinforced with planting and exterior improvements. Despite the size of the intervention, the development proposal does a good job of maintaining the street-front character and allows the existing building to maintain its primary focus on the site.</p> <p>With regards to # 2, the Consultant feels that the appearance of the street-facing portion of the new addition could be detailed to be more sympathetic with the heritage context. It is important to allow the new construction to be “of its own time”, but it should</p>

	<p>also be designed in such a way as to support the character of the district. This could include: the use of compatible wood windows (but not replicas of the existing), re-use of the secondary door and transom that is proposed to be removed, period compatible wood trim and detailing that is distinct from the heritage façade (but still compatible). <b>Note: As per the revised design documents dated July 28, 2021 the designer had addressed several of these concerns. In addition, the City provided their own list of similar design recommendations which have been addressed by the designer. Positive changes include the continuous use of wood siding on the east façade, the removal of the metal parapet, updated designs for the bargeboard and windows.</b></p> <p>In general, the existing heritage character of buildings should be reinforced using materials of a similar quality as the original construction. It is the detailing and design that should support a distinction from the original, not a reduction in quality or the exclusive use of contemporary materials.</p> <p>As shown in the design drawings, the grey Hardi Board, and flat metal fascia and window trim appear more “contemporary suburban” in nature and could be improved to provide a more sympathetic heritage design. We recommend that the colour, materials, and detailing on the street facing façade of the addition be revisited. The continued use of wood, and the targeted use of compatible detailing is encouraged. <b>Note: the updated design documents dated July 28, 2021 have addressed some of these concerns, particularly the positive inclusion of wood detailing and siding on the east façade.</b></p>
<p><b>Is the historical pattern of land use (residential) changed or disrupted?</b></p>	<p>Although the development proposal represents an increase in density for this specific lot, the residential use has been maintained.</p> <p>The Consultant does not assess any substantive impact with regards to the historical patterns of land use.</p>
<p><b>Building-Specific Considerations</b></p>	
<p><b>Is the new addition “compatible with, distinct from, and subordinate to” the</b></p>	<p>There is no precise consensus in the heritage community as to the interpretation of this important standard. It is often</p>

<p><b>existing building? (Standard 11 from the <i>Standards and Guidelines for the Conservation of Historic Places in Canada</i>)</b></p>	<p>interpreted to mean that the existing building should maintain its status as the primary focal point on the site and that the massing, style, and materials should strike a balance between complimentary design and avoiding historical mimicry. In this case, Standard 11 will be considered primarily from the point of view of the street character. How does this development affect the daily users and passersby?</p> <p>It is the consultant’s opinion that the site is essentially a “single-view” street orientation. The east, west, and north facades do not contribute meaningfully to the urban character of the building. This is similar for the neighbouring buildings as well, where various neighbourhood buildings have been enlarged in the rear yard, which is largely unnoticed from the street. In our opinion, interventions at the rear of the building can be acceptable so long as the street-side setbacks and front-yard character are maintained.</p> <p>While the new addition is larger in scale and slightly higher than the existing building, the view from the street does not appear to overwhelm the site or existing building.</p> <p>It is the Consultant’s opinion that the existing building will not be dominated by the new addition by virtue of the single-view nature of the site. The existing building will continue to be the focal point.</p> <p>However, the Consultant would like to underscore, that while it is our opinion that the street character will only be minimally impacted, the building’s massing will be substantively changed. It comes down to, in our opinion a question of <i>tolerance</i> of the immediate surroundings. In this case, the much more massive buildings that surround the existing building will not be overwhelmed by the addition, and may serve to mask the scale of the addition from the street view.</p>
<p><b>The district strategy states: “<i>New work should be of its own time, but subservient to the heritage character of the existing property. It should take its form and direction from the history of the property itself</i>”. Does the</b></p>	<p>Again, this potential impact will be largely considered from the perspective of the Stewart Street elevation. It should also be noted that contemporary codes require that the proposed side-yard and rear yard elevations be constructed of non-combustible cladding.</p>

<p><b>development proposal follow this recommendation?</b></p>	<p>The Consultant’s concern here is that the parts of the new addition that face the street have been designed with an overly “suburban” appearance – and could be improved. The use of the grey Hardi-board, the flat metal facias, and window design could be made more complimentary to the heritage building.</p> <p>The level of detail at the haptic scale should be continued through the new addition. For example, the existing building’s character is largely derived from the composition of finely detailed elements:</p> <ul style="list-style-type: none"> <li>• Stained glass patterns</li> <li>• Shouldered window trim</li> <li>• Decorative bargeboard</li> <li>• Unique column and pilaster design</li> <li>• Etc.</li> </ul> <p>The new addition should support the haptic scale of the building with its own materiality and detailing.</p> <p>As an example, this could include:</p> <ul style="list-style-type: none"> <li>• The continued use of wood siding on the Stewart Street elevation, including a profile with a compatible shadow-line profile,</li> <li>• Using wood fascia, including a two-step design to add more detail to the roofline.</li> <li>• Designing the windows and trim with more detail to support the existing appearance of the building,</li> <li>• Consider taking clues for the new addition from some of the existing detailed elements such as pilasters, cornice, and railing.</li> </ul> <p><b><i>Note: The updated design documents dated July 28, 2021 have addressed many of these concerns, particularly the use of wood detailing on the east façade.</i></b></p>
<p><b>The district plan specifically notes this property for the quality of its private green space. Has this important quality been maintained?</b></p>	<p>As mentioned previously, the development proposal includes a set-back from the face of the existing building, which allows for the front yard “garden” space to be maintained. A new secondary pathway has been included in the design, which will also provide access to the new service entrance on the east side.</p>

	<p>The Consultant does not view this as a problematic element of the proposed design.</p> <p><b>Note: The updated design documents dated July 28, 2021 have eliminated some of the problematic site elements, such as the exterior bicycle parking.</b></p>
<p><b>How does the development proposal and the request for zoning relief impact the adjacent heritage properties (identified in fig. 2)?</b></p>	<p>The City of Ottawa has identified four additional Part IV buildings that could be impacted by this development proposal:</p> <ul style="list-style-type: none"> <li>• 473 Cumberland Street (St. Paul’s Church)</li> <li>• 106-110 Daly Avenue</li> <li>• 112 Daly Avenue (Lyon House)</li> <li>• 66-68 Stewart Street</li> </ul> <p><u>473 Cumberland – St. Paul’s Church</u>                  St. Paul’s Church is around the corner (kiddie-corner in the rear yard, and offset by two properties) at the intersection of Cumberland and Daly. It has no direct views of the site, and the Consultant feels that impacts (if any) to this building will be negligible.</p> <p><u>106-110 Daly</u>                  This building is the rear-yard neighbour of the development site. It consists of three row houses constructed in a brick Italianate style – with one-storey additions at the rear. One of the units appears to make use of the existing right-of-way to access their rear yard for parking purposes.</p> <p>While it is indisputable that the proposed development will have an impact of the appearance of the rear-yard conditions, it is not clear what the heritage impacts might be. This building, like others in the neighbourhood/district is most valued for its street facing façade. The limited heritage documentation provided did not indicate any character-defining elements at the rear.</p> <p>The Consultant assess that the architectural integrity of this building will not be compromised by the proposed development – and that the proposed variance relief regarding the rear-yard setback is a zoning consideration, and not a heritage issue.</p> <p><u>112 Daly – Lyon House</u></p>

This property is kiddie-corner to the site in the rear-yard, and does not appear to share either the lot line. It is the neighbour of 106-110 Daly as noted above. The building is a particularly well-preserved picturesque Victorian house, with a compatible modification to the front veranda to provide separated access for the upper unit.

The proposed development will not impact this building, except for partial views outwards from the building's rear windows and yard.

66-68 Stewart Street

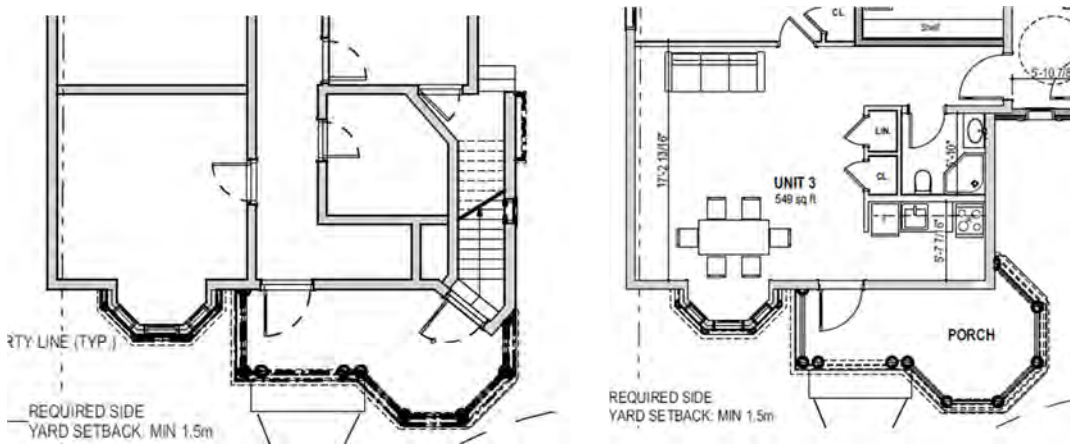
This 3-storey building is located across the street from the site. It has been designated for its architectural and historical value. The symmetrical design and ornamentation contribute to the overall street character in the immediate surroundings.

Similarly to others, this building's main contribution to the district is derived from its street façade.

The Consultant does not anticipate that the rear and side-yard addition of the development proposal will negatively impact this building's heritage value.

In addition to the above, the Consultant notes one area of the proposal that could be better defined in terms of the heritage impact. While not specifically called out in the provided drawings, it appears that the front porch will be partially demolished and rebuilt.

The existing conditions include a secondary door and staircase at the porch. The demolition of this staircase will also precipitate the partial demolition of the octagonal porch, which is an important element of the Stewart Street façade. It is critical that the execution of this work preserves the existing porch, including the detailing on the cornice, columns and rails.

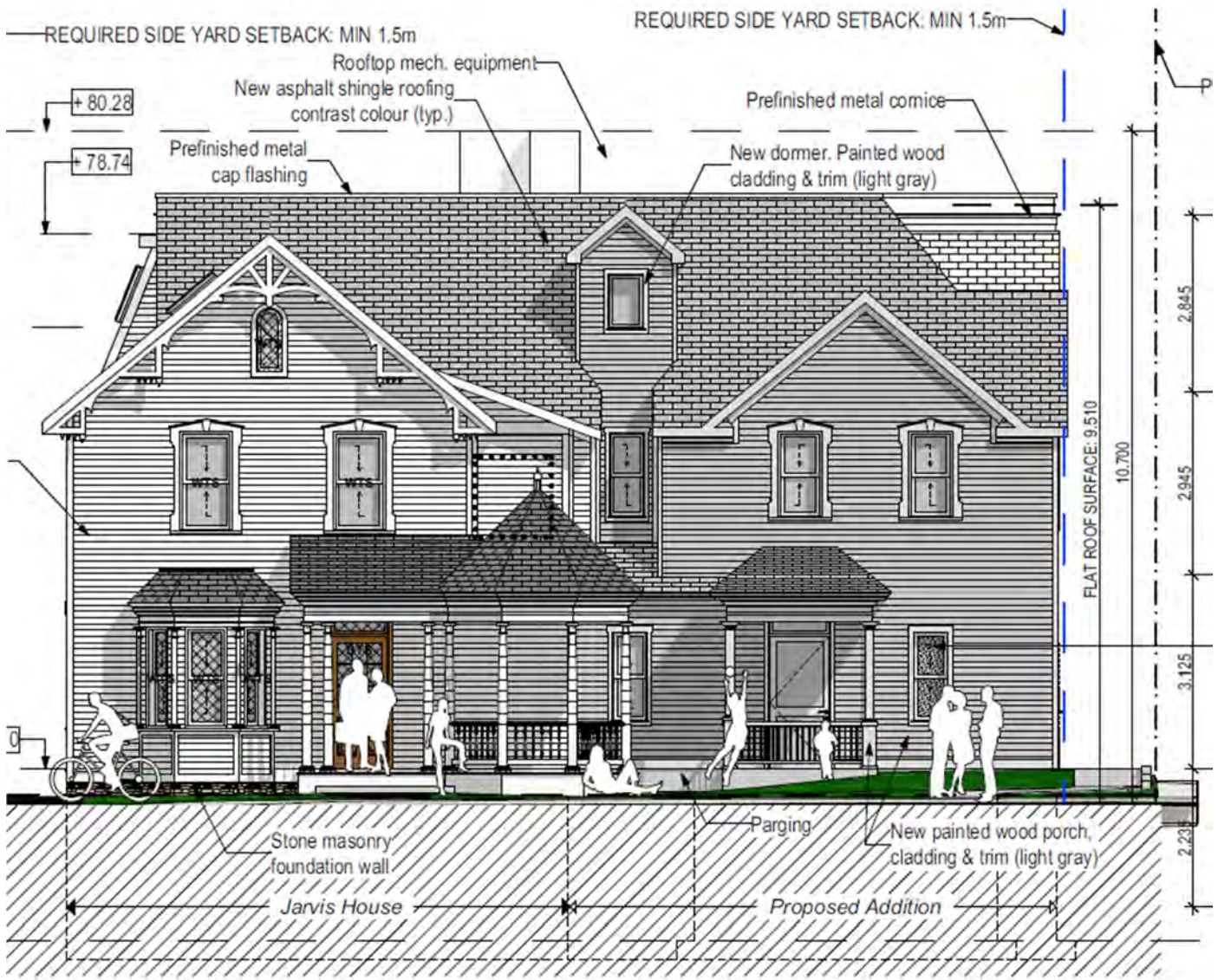


**Figure 11:** The existing condition (left) and proposed condition (right). The demolition of the staircase will impact the existing porch. Portions of the porch at the top-right will need to be re-built.

The rebuilding of the impacted elements should be treated as full heritage *restoration*:

- All profiles should be fully re-created of the same materials (columns, capitals, cornice, rails, etc.). The carpentry work should be executed by skilled tradespeople with experience in wood restoration techniques.
- The demolition work should be carefully executed and the areas around the porch should be dismantled by hand.
- The existing porch should be protected from the demolition work.

As a *restoration*, this is one element where the new work should not be “distinct from” the existing. The restored portions of the porch should only be distinguishable from the original upon close inspection by a professional. In other words, the porch should be restored to its original condition before the addition of the staircase, including all trim, mouldings, etc.



**Figure 12:** The proposed Stewart Street elevation. (updated May 25, 2021) The revised design includes wood window trim and additional ornamentation that does not replicate the original design. **(Revised July 28, 2021)**



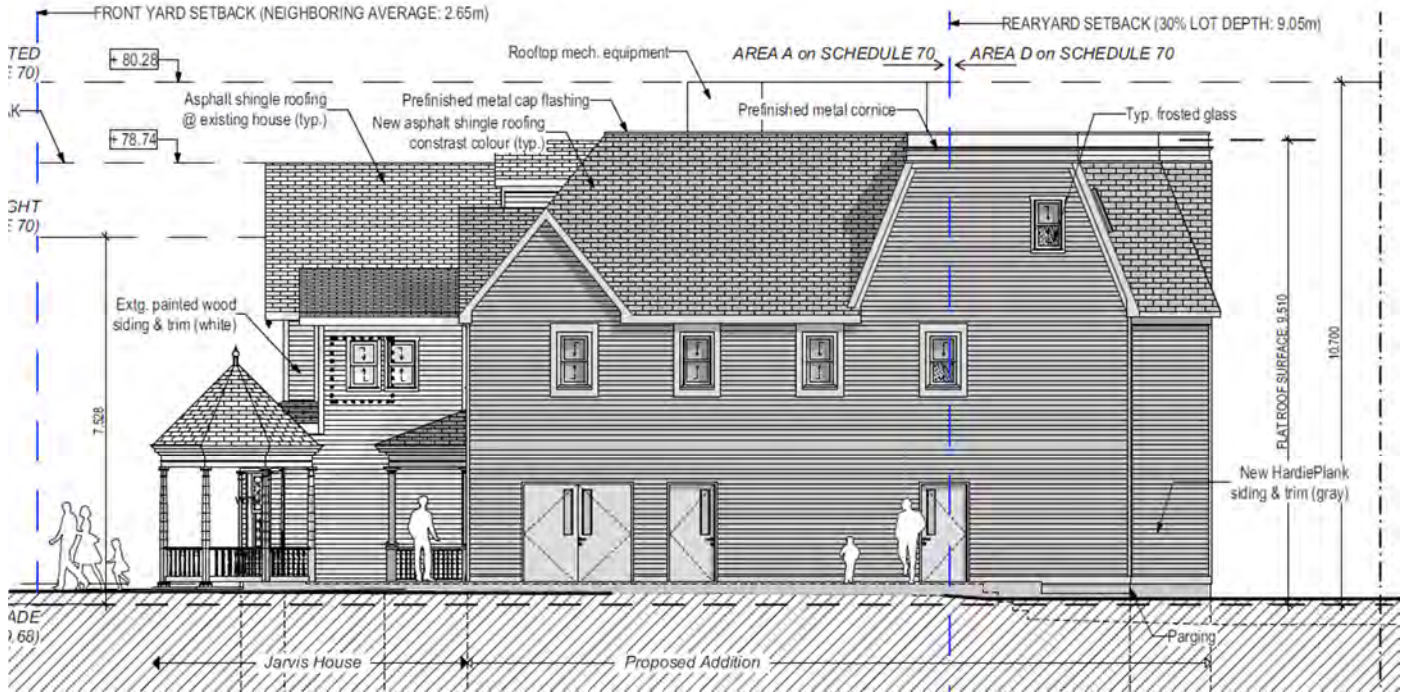


Figure 13: The proposed east elevation – showing the proposed setback between Jarvis House and the new addition. (Revised July 28, 2021)

## **7. CONCLUSIONS**

The Consultant is of the opinion that the proposed development meets the guidelines set out in the *Standards and Guidelines for the Conservation of Historic Places in Canada*, and the Sandy Hill Conservation District's conservation plan. We would nonetheless encourage the Client to pay additional attention to the materials and detailing on the Stewart Street elevation of the proposed addition, as outlined above.

**Overall, the proposed development, as represented in the provided drawings, is compatible with the heritage building and district.**

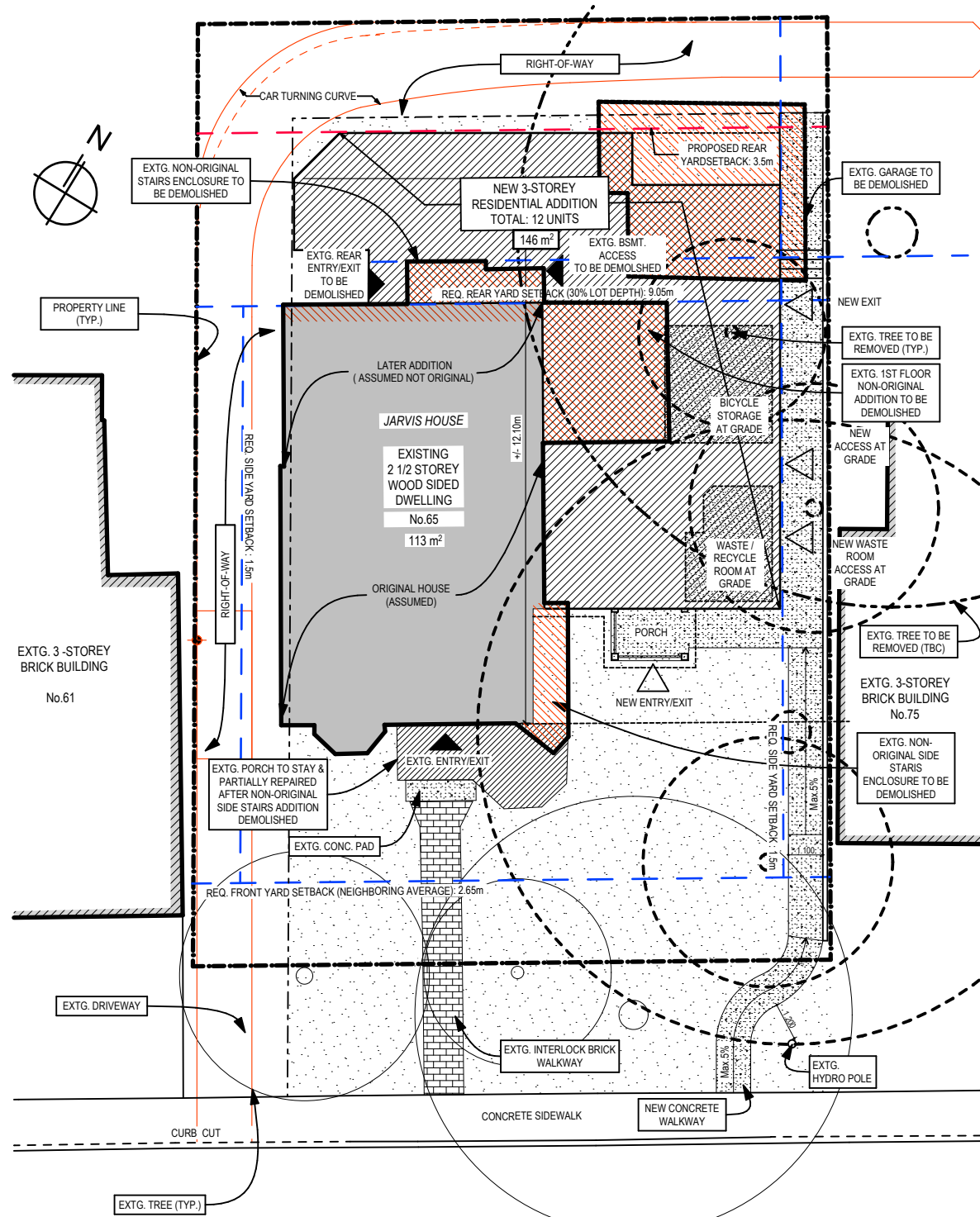
Sincerely,



**STEFAN GINGRAS**

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Principal, Juxta Architects  
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stefan@juxtaarchitects.com





**1**  
HP01  
**SITE PLAN - PROPOSED**  
SCALE: 1:200



**2**  
HP01  
**CONTEXT MAP**  
NOT TO SCALE

**PROPOSED GROSS FLOOR AREA (ZONING):**

BSMT:	57.70 m <sup>2</sup>
1ST FLOOR:	58.70 m <sup>2</sup>
2ND FLOOR:	100.10 m <sup>2</sup>
3RD FLOOR:	67.40 m <sup>2</sup>
<b>TOTAL PROP.:</b>	<b>283.90 m<sup>2</sup></b>

**EXISTING GROSS FLOOR AREA (ZONING):**

BSMT:	70.50 m <sup>2</sup>
1ST FLOOR:	91.30 m <sup>2</sup>
2ND FLOOR:	99.70 m <sup>2</sup>
3RD FLOOR:	62.20 m <sup>2</sup>
<b>TOTAL EXTG.:</b>	<b>323.70 m<sup>2</sup></b>

**GROSS BUILDING AREA (O.B.C.):**

BSMT:	259.00 m <sup>2</sup>
1ST FLOOR:	259.50 m <sup>2</sup>
2ND FLOOR:	256.00 m <sup>2</sup>
3RD FLOOR:	185.00 m <sup>2</sup>
<b>TOTAL:</b>	<b>959.50 m<sup>2</sup></b>

65 Stewart St.      Zoning: R4UD S70  
Ottawa ON K1N6H9      Mature Neighborhood and Heritage Overlays

Provision	Requirement	Provided	BY-law	Relief
Min Front Width	15m	20.4m	Table 162A-R4	-
Min Lot Area	450 m <sup>2</sup>	612 m <sup>2</sup>	Table 162A-R4	-
Min Front Yard Setback	Neighboring average: 2.65 m	6.75 m	139(3)(a)(i)	-
Min Interior Side Yard	1.5 m	2.78m (West)+ 1.5 m (East)	Table 162A-R4UD	-
Min Rear Yard Setback	30% lot depth including 25% of lot area, in this case 9.05m	3.5 m	161(11)(iii)	✓
Max Height	Backyard (Area D): 10.7 m In all other circumstances (Area A): existing building height, in this case 7.53m	Area A : 9.51m (10.7m fto top of projections above roof surface )	Schedule 70, Section 64	✓
Lanscaped Area	30% of the lot area	30.3% ( Hard: 60.2m <sup>2</sup> , Soft: 124.9m <sup>2</sup> )	161(8)	-
Soft Landscaping	Rear yard: - min. 50% of the rear yard. - min. 25m <sup>2</sup> one aggregate rectangular area whose longer dimension is not more than twice its shorter dimension, for the purpose of tree planting. Side yard: - Any part not occupied by accessory buildings and structures, permitted projections, bicycle parking and Isles, hardscaped paths of travel for waste and recycling management, pedestrian walkways, permitted driveways and parking exclusion fixtures. Front yard: - Minimum 20% - Be equipped with solid, permanent fixtures sufficient to prevent motor vehicle parking such as: bicycle parking, benches, bollards, ornamental fences or garden walls, raised planters, trees, wheel chair lifting devices.	9m <sup>2</sup> 10.7m <sup>2</sup> (partial rear yard and abutting interior side yard)	161(13)(b)(iii) 161(13)(b)(iv)	✓ ✓
		0.7 m <sup>2</sup> (west) 22.6m <sup>2</sup> (east)	161(13)	-
Bicycle Parking	0.5/ unit. In this case 12x0.5=6 spaces  Bicycle parking spaces must be located in order to provide convenient access to main entrances or well-used areas.	75.4% Trees&Boulders	(13)(d) & T. 161 (13)(e)	-
		14 Horizontal (Stacked parking)	Section 111	-
Car parking	Not required in Area Z on Schedule 1A (TBC)	-	139 (7)(a) & 101(2)	-
Visitor car parking	Not required (0.1/unit required after first 12 units: 0.4)	-	139 (7)(a) & 102(2)	-
Addition	The height of the walls and the height and slope of the roof of the addition do not exceed those of the building	-	60(3)(a)	✓
	The side yard setback of the addition is at least 60 cm. greater than that of the wall of the building located closest to the side lot line	1.5m to side lot line	60(3)(b)(i)	✓
	It is located entirely within the rear yard, or in the interior yard abutting the rear yard and complies with the rear yard setback of the underlying zone	Located in rear yard and interior side yard	60(3)(b)(ii)	✓
2 or more bedroom units	min 25% of total units, in this case 3	6	161(14)(b)	-
Total Amenity Area	No minimum	155.7m <sup>2</sup>	-	-
Communal Amenity Area	No minimum	146.8m <sup>2</sup>	-	-

**3**  
HP01  
**ZONING MATRIX**  
NOT TO SCALE



1



2



3



4



5



6



7



8

**LEGEND**

1. S-W STREET VIEW PARTIALLY SCREENED BY EXISTING TREES ON THE PROPERTY.
2. PARTIAL VIEW OF FRONT ELEV.
3. POLYGONAL FRONT PORCH WITH ORNAMENTAL CORNICE, COLUMNS AND RAILING.
4. FRONT BARGEBOARD DETAIL.
5. BAY WINDOW FEUTING PATTERNED LEADED GLASS.
6. FRONT PORCH CORNICE AND COLUMN DETAIL.
7. FRONT PORCH WTH TRANSOM AND LEADED GLASS.
8. PARTIAL S-E VIEW OF THE POLYGONAL PORCH (DETAIL)



9



10



11



12



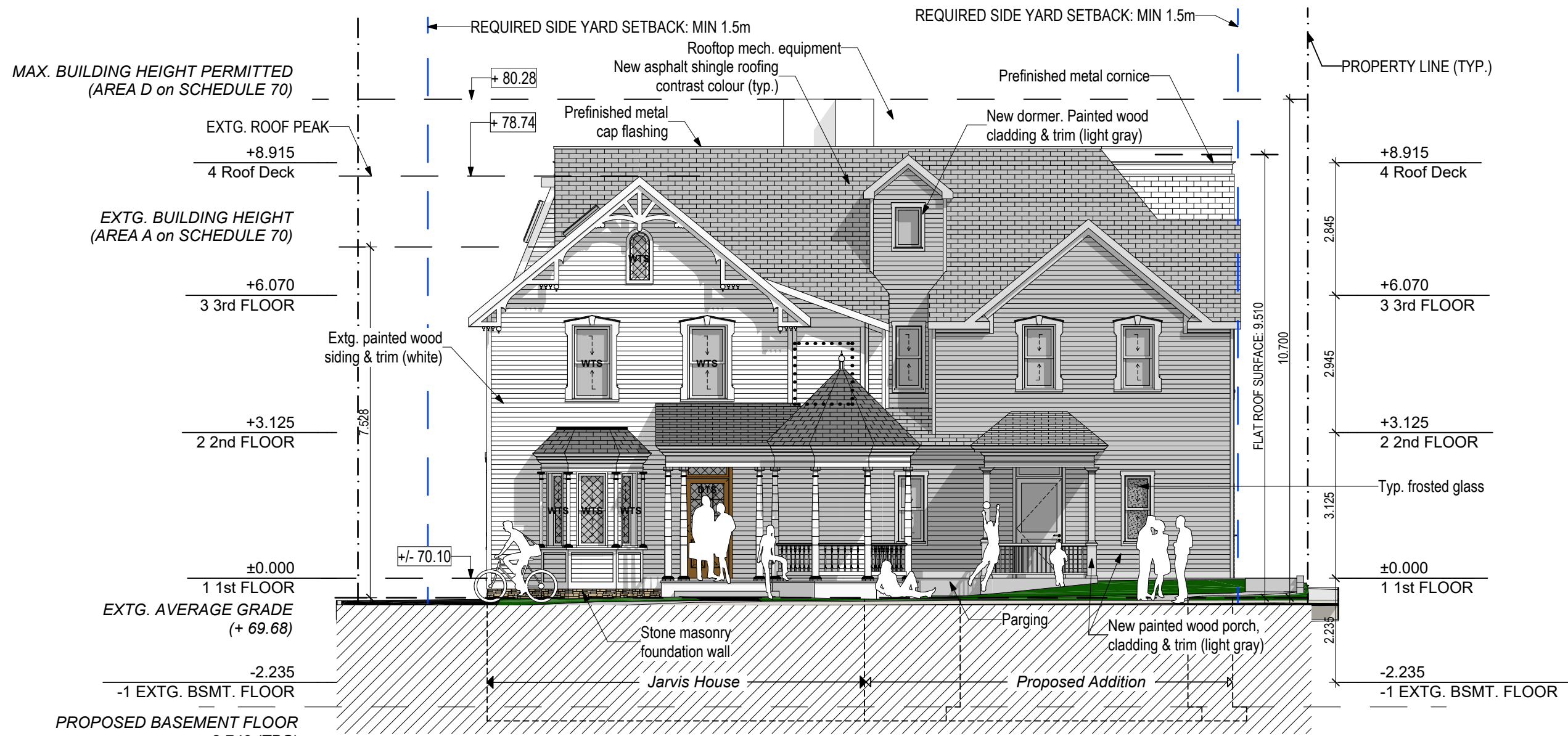
13



14

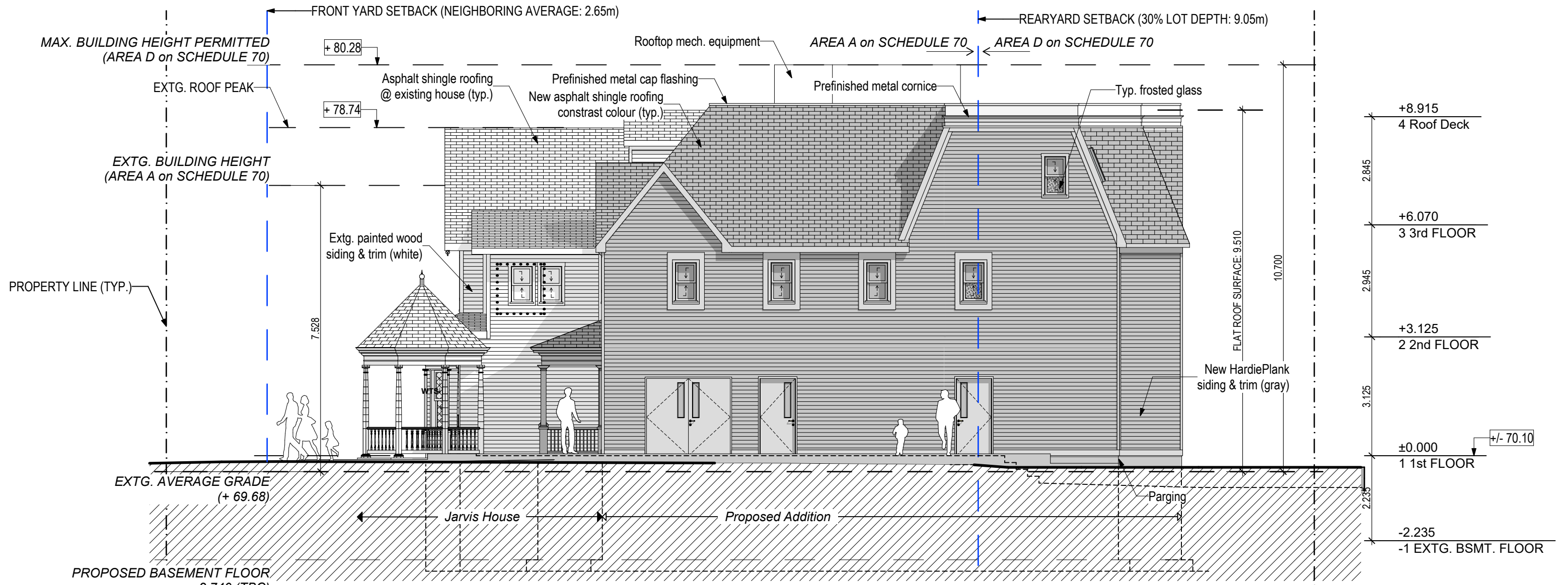
**LEGEND**

- 9. S-E VIEW OF POLYGONAL FRONT PORCH.
- 10. PARTIAL VIEW OF EAST ELEVATION WITH EXTERIOR STAIR ADDITION.
- 11. PARTIAL VIEW OF EAST ELEVATION WITH SMALL EAST ADDITION.
- 12. PARTIAL VIEW OF NORTH ELEVATION WITH SMALL REAR ENTRANCE ADDITION .
- 13. PARTIAL VIEW OF NORTH ELEVATION WITH SMALL REAR ENTRANCE ADDITION AND 2-CAR GARAGE .
- 14. S-W VIEW: WEST ELEVATION AND SMALL REAR ENTRANCE ADDITION.



**Notes:** 1. All windows to be new. Opening size, leaded glass (shown hatched) and exterior trim of windows noted **WTS** to remain.  
 2. **DTS** door & transom w/ leaded glass (shown hatched) to remain  
 3. Dotted lines denote infills of openings assumed not original. New infill cladding to match existing.

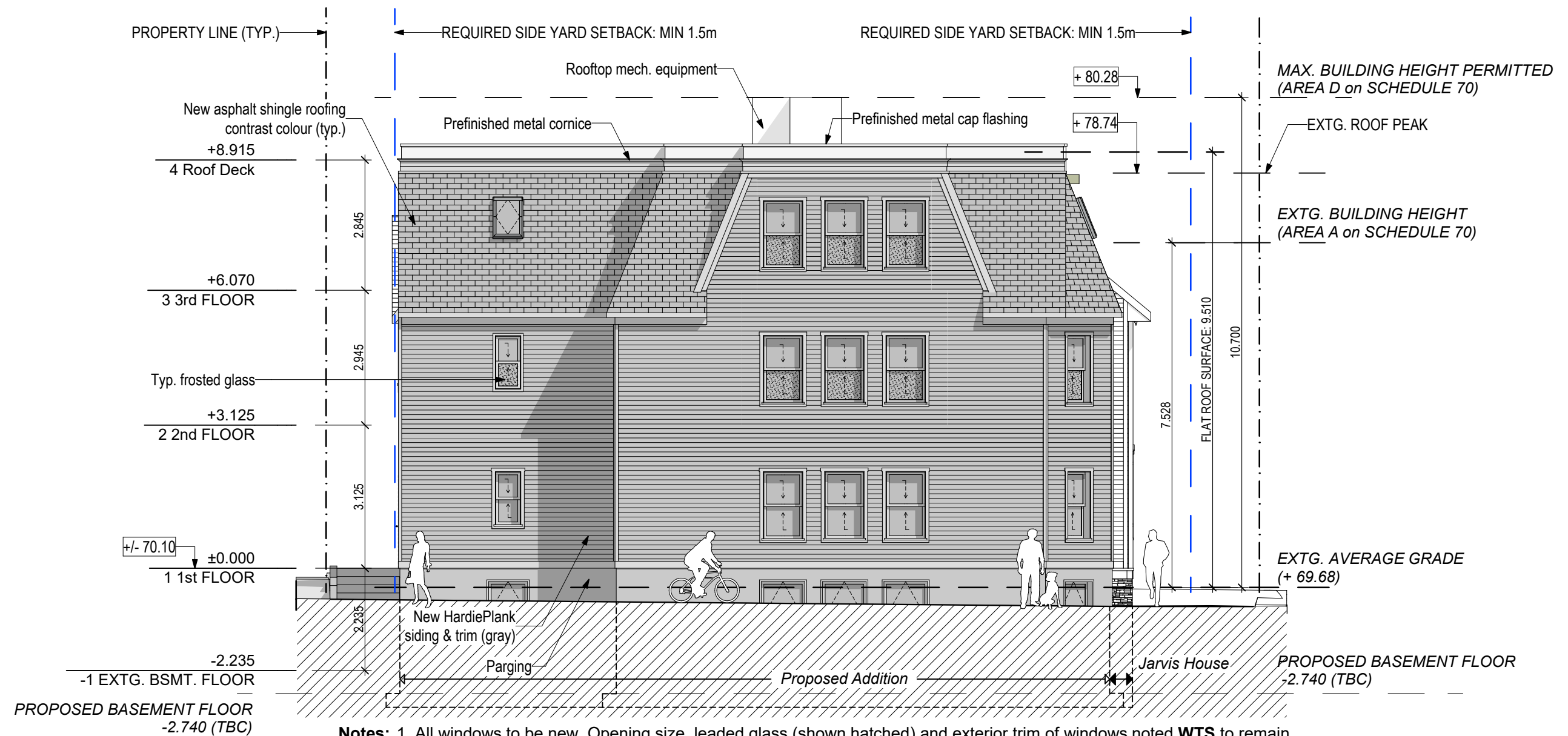
SCALE: 1:100

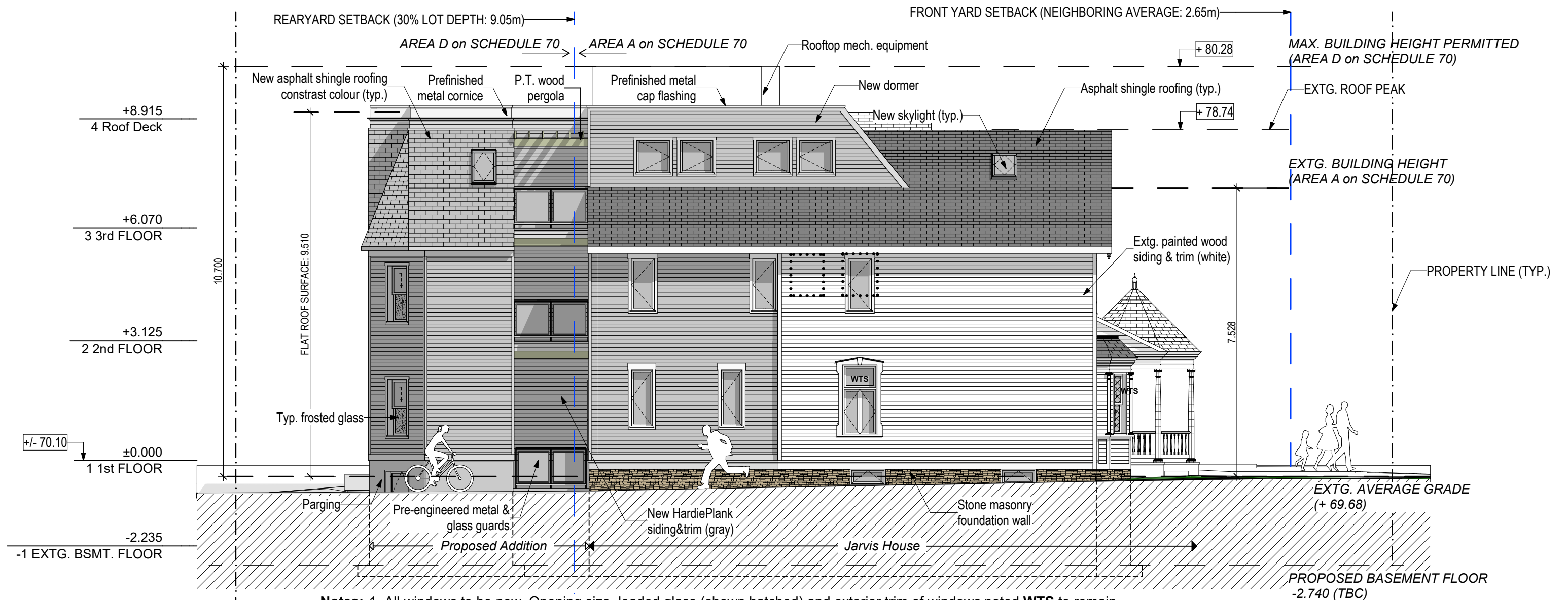


- Notes:**
1. All windows to be new. Opening size, leaded glass (shown hatched) and exterior trim of windows noted **WTS** to remain.
  2. **DTS** door & transom w/ leaded glass (shown hatched) to remain
  3. Dotted lines denote infills of openings assumed not original. New infill cladding to match existing.

SCALE: 1:100







**Notes:** 1. All windows to be new. Opening size, leaded glass (shown hatched) and exterior trim of windows noted **WTS** to remain.  
 2. **DTS** door & transom w/ leaded glass (shown hatched) to remain  
 3. Dotted lines denote infills of openings assumed not original. New infill cladding to match existing.

SCALE: 1:100



**NOTE:** EXISTING TREES TO STAY SHOWN TRANSPARENT FOR CLARITY



**NOTE:** EXISTING TREES TO STAY NOT SHOWN FOR CLARITY



**NOTE:** EXISTING TREES TO STAY NOT SHOWN FOR CLARITY

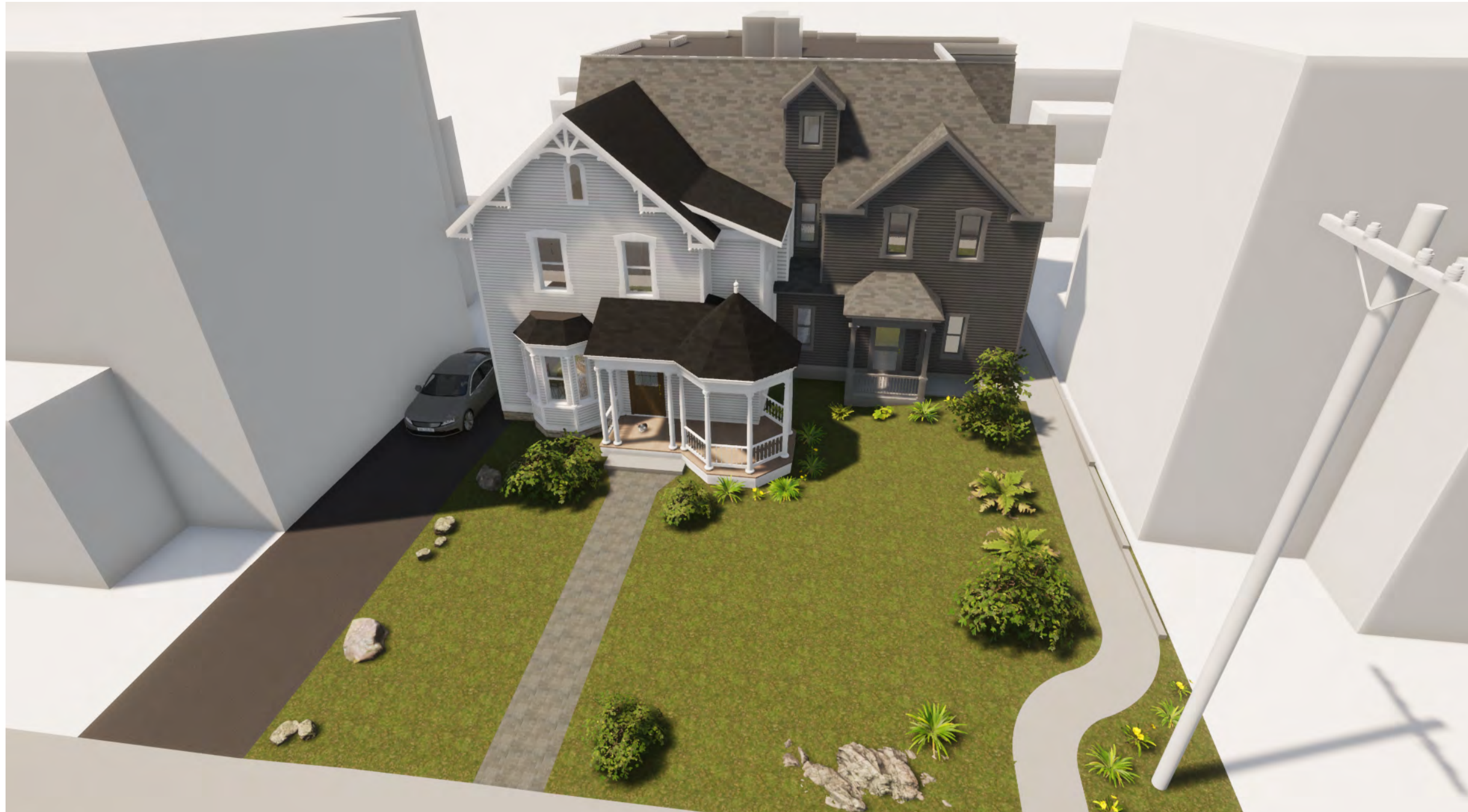






**NOTE:** EXISTING TREES TO STAY SHOWN TRANSPARENT FOR CLARITY





**NOTE:** EXISTING TREES TO STAY SHOWN TRANSPARENT FOR CLARITY



**NOTE:** EXISTING TREES TO STAY SHOWN TRANSPARENT FOR CLARITY



**NOTE:** EXISTING TREES TO STAY SHOWN TRANSPARENT FOR CLARITY



## VI. HERITAGE CONSERVATION DISTRICT PLAN

### VI.1 Boundaries of Proposed District:

The boundaries of the proposed Sandy Hill West Heritage Conservation District are outlined on the attached map.

The west boundary is Waller Street, which acts as a strong dividing line between the small scale heritage character of this area and the larger scale developments to the west.

The north boundary is a line midway between Besserer and Daly. Although some individual buildings of historical and architectural interest survive along Besserer, it was felt that there is not the same critical mass to define a heritage character as exists along Daly. The redevelopment of Besserer over time may incorporate some of these existing buildings. As indicated above, a recommendation has been made to review zoning provisions along Besserer to ensure that any future developments are compatible with the proposed Heritage Conservation District.

The east boundary is King Edward Avenue. As indicated, area residents indicated a strong sense of continuity across King Edward to the heritage residential character of the rest of Sandy Hill. This boundary is seen as one which might eventually expand to connect with existing Heritage Conservation Districts within the Sandy Hill community.

The south boundary is a line midway between Stewart and Séraphin Marion on the block between Waller and Cumberland, then along Wilbrod east from Cumberland as far as Ring Lane, and then dipping down to include Ring Lane and the Lutheran Church properties on the south side of Wilbrod. This south boundary leaves out of the district the University of Ottawa property for which a detailed series of proposals have been developed as part of the current Master Plan exercise. These proposals are seen as somewhat independent but compatible with the District designation, and will extend to cover the entire campus area. The only University of Ottawa property within the proposed District is along Stewart Street, where it was felt that existing and proposed University initiatives must be closely interrelated with the existing heritage residential properties on this street. The boundary also excludes the major properties of the Oblate Fathers, although again compatible development is important, particularly along the south side of Wilbrod and adjacent to Ring Lane.

These boundaries enclose an area that has a very high degree of surviving buildings of architectural and historical interest. It contains an important mix of residential and institutional buildings, with some commercial. It is felt to be of sufficient size to generate community involvement. The emphasis on the heritage residential and institutional component should encourage links with the larger Sandy Hill community associations.

## VI.2 Heritage Character Statement:

The following Heritage Character Statement applies to the overall study area, the context in which the proposed Heritage Conservation District is situated.

Unlike many other heritage areas, Sandy Hill West is not a strongly defined community. Instead, it is a buffer zone and meeting place between strongly defined areas.

The built form of Sandy Hill West is outstanding for its high proportion of early building stock. Most of this form was created in the 1880-1920 period, with contributions from the surrounding earlier 1840-80 and later 1920-50 periods.

The building uses in Sandy Hill West are primarily residential in character. The size and functional scale of the residential stock represented, however, is very broad. It varies from small 1 1/2 storey working-class single-family houses which are vernacular and builder inspired, to large ornate villas and multi-family row houses designed by architects for the upper classes. Apartments, both tenement and low-rise, are also found in this area. In short, the area contains examples of most pre-1940 residential building types - even though many of these structures are no longer used for their original purposes.

One of the unique features of this area is the number of nineteenth century churches it contains. When it was first settled, Sandy Hill West was host to churches of many denominations which served the broad constituency of Ottawa East or, in some cases, the entire city. Most of these institutions have survived, and many have retained links across the city.

The single major institution in this area, the University of Ottawa, is significant in its own right as Canada's first bilingual university. The historical themes that describe its past are all major ones - the contribution of the Oblate Order to community development and education in Canada, the history of French education in the province of Ontario, and the contribution of bilingualism and French/English relations to the development of our nation. Many of these are enriched by their reflection in the history of this area, for the university and Sandy Hill West have developed together.

Sandy Hill West has never existed or developed in a vacuum. Its past is linked to the histories of Canada, of Ontario, and of Ottawa. Its development reflects the major social, cultural and economic patterns of the Byward Market and Rideau St., linked as it was to the mainstream development of Lowertown. Its population is associated with the major political developments of the capital - its establishment, its major figures, and its bureaucracy. The residential patterns and community facilities evident in Sandy Hill West also reflect and share those of the Sandy Hill area.

Sandy Hill West is a transition area. It is a small eight-block area where the diverse themes of the surrounding areas meet, mix, and emerge as distinct entities on opposing borders. As such, this area holds intact remnants of many divergent parts of our cultural heritage - elements that have often disappeared in their purer contexts.

### VI.3 Management Issues

The creation of a Heritage Conservation District does not in itself guarantee the protection and enhancement of heritage character. Rather, it introduces mechanisms to assist property owners, residents, city staff, and other interested parties in maintaining the historical and cultural values of the neighbourhood while introducing sympathetic contemporary elements.

In terms of protecting what exists, the following management issues apply:

**.1 Repair and Restoration of Existing Buildings**

Technical and financial assistance is made available to property owners within a designated Heritage Conservation District. This assistance is used for approved repair and restoration activities designed to preserve and enhance the existing heritage value of the property.

Guidelines for such activity are outlined in Section VI.4.

**.2 Alterations**

Under the Ontario Heritage Act, review of alterations is introduced as a control mechanism within a Heritage Conservation District. The intent of the review is to assist property owners in conserving the heritage value of both their own property and the district as a whole. The guidelines in Section VI.4 provide the basic principles that would be applied in the case of design reviews.

**.3 Demolition**

Demolition control is also introduced in a Heritage Conservation District as a control mechanism under the Ontario Heritage Act. The control is not absolute. An application for demolition is reviewed by the City, and if considered detrimental there is a period of 270 days during which alternatives are explored. If no compromise can be reached, then either the demolition proceeds or the City in rare cases may decide to purchase or expropriate.

In terms of enhancement, other management issues apply:

**.4 Zoning**

Existing zoning is usually reviewed following designation of an area as a Heritage Conservation District. This review is done to ensure that height and density limits, land use patterns, and other zoning controls are compatible with the heritage character of the area.

Height and density limits in a heritage district are set to respect the pattern established by existing heritage properties. This ensures some level of architectural consistency between these properties and new construction, and protects against development pressures to tear down existing heritage buildings and replace them with new buildings to much higher height or density limits. Setbacks and patterns of open space are also important characteristics.

The proposal to change much of the area from CAH (Central Heritage Area) to HR (Heritage Residential) zoning will achieve some of these objectives.

**.5 Streetscape Improvements**

Guidelines for streetscape improvements related to the heritage character of the area such as improvements to vehicular traffic and parking, pedestrian traffic and amenities, green space, street trees, utility lines, lighting, sidewalk design, and signage are outlined in Section VI.5

**.6 Design Coordination - New Construction**

The pattern, scale and detail of new construction in a heritage conservation district are subject to review. The purpose of design and development guidelines and reviews is to

ensure that new work is compatible with the character of the area. The intent is not direct imitation of historic precedent, but good contemporary design which blends well with the existing quality of design and detailing.

The Sandy Hill West district is distinctive because of the unusually rich and continuous pattern of older buildings. It is important that in the few vacant parcels that do exist, or where replacements are to be made, that this pattern be respected. There are very few areas in Ottawa which have such a wealth of examples from the earliest days of Bytown to the present.

Guidelines for new construction and infill are contained in the Design Guidelines in section VI.4 .

In addition to the management issues listed above, the recommendation has been made that the City of Ottawa coordinate its management of the Sandy Hill West Heritage Conservation District with all other existing and proposed Heritage Conservation Districts within Sandy Hill. This management activity should closely involve the various community groups and other associations already involved in the area. There is considerable pride in the diversity of tangible and intangible resources that give Sandy Hill its unique character. Part of this character is the wealth of heritage resources that have survived despite the proximity to the downtown core. Sandy Hill West is an essential component of any long term strategy for survival of the larger area.



## VI.4 Design Guidelines

### Introduction

The role of the City of Ottawa and other levels of government is less direct when dealing with individual properties than when dealing with streetscape improvements. However, the design review function is an important part of managing change within a Heritage Conservation District. The following guidelines are suggested as criteria for design review.

There are two types of guidelines. The first are for existing properties, and concentrate on preserving their heritage qualities. The second are for new construction, and are intended to encourage compatible new development in the few vacant lots and infill sites that exist within the area.

#### VI.4.1 Building Inventory

A detailed inventory of the buildings in the district has been carried out as part of the research phase of the project. This inventory identifies a wide range of building types, styles and materials from the earliest phases of Ottawa development to the present. The specific information on each building, which is maintained as part of the information base for administration of the Heritage Conservation District, is available to property owners and others when conservation, restoration or redevelopment are being considered.

#### VI.4.2 General Principles

The specific guidelines are based on the following general principles:

1. The overlay of residential and institutional uses, with additional commercial and industrial activity, has been characteristic of the district since its earliest days and continues to be an essential feature which should be preserved if possible.
2. These patterns of land use are clearly reflected in the built form, with the residential uses characterized by small scale single and multiple unit buildings and the institutional uses characterized by more stylized, larger buildings which nonetheless share similar materials and detailing. The commercial and industrial uses occur within the small scale residential pattern of building size and lot arrangement. This correspondence between land use and building type is an important characteristic to be maintained.
3. Because there are so few vacant lots within the proposed district, the older buildings exist not only as individual fragments of architectural and historical interest, but also as components of significant streetscapes. It is important that this sense of historical and physical continuity be maintained and enhanced.
4. A number of buildings within the area have suffered from inappropriate but relatively superficial alterations over the years. With designation as a Heritage Conservation District, the future of the district should be stable enough to make restoration and repair of many properties a worthwhile investment. With appropriate historical research, there is the opportunity for significant upgrading of some of the properties at relatively low cost.

## VI.4.3 Specific Guidelines:

### VI.4.3.1 Maintenance and Repair

Conservation, through maintenance and repair, is the most important mechanism for preserving the character of individual heritage properties, and, by extension, the character of the District. Many of the buildings in the Sandy Hill West area have been maintained and repaired on a regular basis for a century and more. This is why there is such a high percentage of early buildings still standing. If this tradition is kept up, the area will continue to have a rich and cohesive character.

#### Recommendations:

1. Inspection of heritage properties by building owners/tenants should be encouraged on a regular basis. Ideal times for inspection are the spring and fall, immediately after a heavy rainstorm. A checklist should be used, starting at the foundation and moving up to the roof, with particular attention to signs of structural movement, water infiltration, decayed or loose materials, and deterioration of finishes.
2. Depending on the seriousness of the problem, city staff or professionals in the conservation field should be consulted as appropriate. It is important to correct the causes of problems before spending money on repairing the results of the problems.
3. The use of correct materials, sizes, and details is essential. Original or early material should always be used as a reference. Substitute materials should be avoided. In general, it is better to repair than replace.
4. The maintenance and repair techniques are different depending on both the age of the property and its type of construction. More detailed guidelines for maintenance and repair follow.



65 Stewart Street

#### VI.4.3.1.2 Maintenance and Repair: Wood Frame Buildings

Wood frame buildings appear throughout the history of Sandy Hill West area. Mid-nineteenth century to early twentieth century examples are of balloon frame. More recent buildings are likely to be of platform frame construction.

Wood frame buildings can be clad in wood siding, imitation wood siding of vinyl or aluminum, or in brick veneer.

##### Recommendations:

1. These buildings often deteriorate at the base, particularly where the first floor level is at or near grade. These areas should be inspected, and if necessary new treated wood used to splice the bottoms of the studs and provide a new sill plate. Surface drainage should be away from the building.
2. Wood siding should be repaired rather than replaced, if possible. The use of aluminum or steel siding is not recommended. New trim boards at corners and around windows and doors should match the existing dimensions.
3. If insulation is added to the cavity space, it should be provided with a proper vapour barrier.
4. Painting to historic colours can add to the heritage value of a home. A professional paint analyst should be consulted to determine the pattern of colours used through the years.



84 Stewart Street

### VI.4.3.1.3 Maintenance and Repair: Masonry Buildings

Some of the prominent early residences in Sandy Hill West were built of stone. By later in the century brick had replaced stone except for foundation walls on all residential projects in the area. Solid brick walls soon gave way to brick veneer on wood frame (see below). Stone however continued to be used for institutional buildings, well into the mid-twentieth century. By the end of this period, stone was functioning as an ashlar facing over a reinforced concrete or steel structure. .

#### Recommendations:

1. Structural defects in masonry construction can usually be noted by diagonal or vertical cracks along the mortar joints. Such problems should be corrected before other work is carried out.
2. Masonry must be repointed regularly to avoid water infiltration and subsequent damage. Repointing should be done by raking out the loose and friable mortar to sufficient depth to give the new mortar a good key. Soft mortars must be used. Type H masonry cements such as Federal White or St. Lawrence Type H are recommended.
3. Cleaning of brick and most types of stone should be done with water washes and rinses. The use of chemicals requires careful environmental monitoring. The use of abrasives may be acceptable under the direction of a qualified conservator to avoid undue damage.



213 Wilbrod

#### VI.4.3.1.4 Maintenance and repair: Brick veneer

Brick was used as a siding material in the Sandy Hill area from the mid-nineteenth century on. By the late nineteenth century, it was generally a veneer over wood frame, and was often used to create decorative details. Patterns in the brickwork from different periods contribute to the heritage character of the district.

The extensive use of brick veneer in this area makes it one of the unifying characteristics of the Sandy Hill West district.

#### Recommendations:

1. If a veneer of brick is pulling away from the wall behind, a structural investigation should be carried out. It may be necessary to pull in the veneer using wood walers. If the original ties or nails have rusted out, new galvanized or stainless steel fasteners can be inserted through the mortar joints. Repair is preferable to replacement.
2. Repointing should be done using a soft (type H) mortar. A hard portland cement mortar has a limited life span in the Ottawa climate, and will damage the brick. Hand tools should be used to rake out the joints. Protruding sections of decorative work or corbelling should have a sloped mortar cap.
3. For repairs involving replacement brick, it is important to match both size and colour. In older buildings, when a match is difficult, it is sometimes possible to reuse brick from a hidden location elsewhere on the building.
4. For cleaning of brickwork, a reputable cleaning contractor should be engaged using low and medium pressure water or carefully controlled abrasive blasting. Test areas should be prepared and inspected before final approval can be expected.

#### Walers:

*The horizontal timbers placed on the face of the plank supporting the walls, in order to hold them together and kept in place by struts running from wall to wall.*



71 Daly Avenue

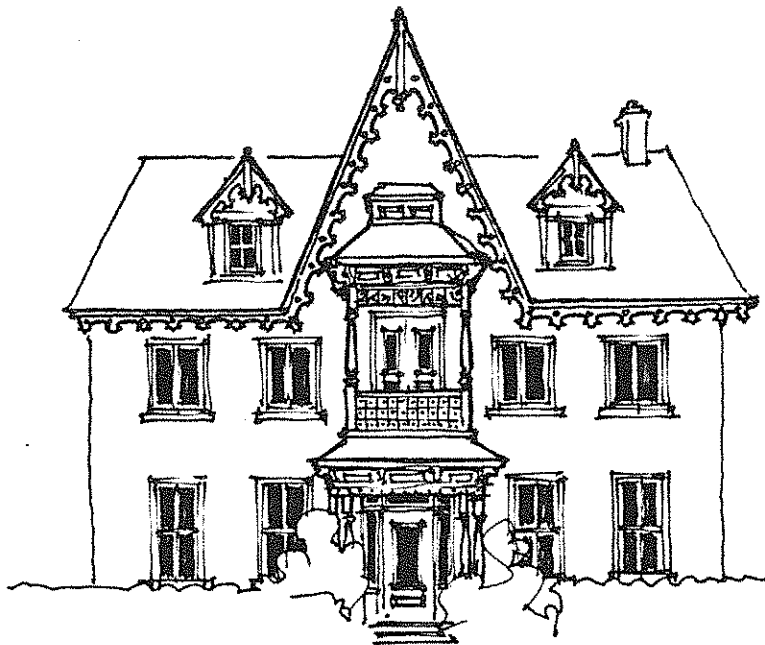
#### VI.4.3.1 Maintenance and repair: Decorative wood elements

Existing decorative wood elements add considerably to the character of individual buildings and larger streetscapes. Before the 1870s, these elements were planed and shaped using hand tools, with extensive use of a beaded edge detail. By the late nineteenth century, there was extensive use of milled lumber, including turned columns and spindles, and various types of fretwork. These show up in porches, balusters, vergeboards and door and window trim. By the early twentieth century, turned work was less in favour, and the emphasis was on squared columns and spindles, with geometric trim details. Often these elements are found as replacement parts on earlier porches and additions.

Decorative wood details are found on wood-frame buildings, but also on the extensive inventory of brick-veneer buildings of the late nineteenth and early twentieth century.

##### Recommendations:

1. Trim elements should be repaired rather than replaced whenever possible.
2. If there are initial signs of rot, the source of moisture should be identified and corrected. Then a preservative should be brushed on, particularly on any exposed end-grain, and the wood repainted.
3. For more advanced decay, the piece should be cut back to sound wood and new material spliced in. New work should match the dimensions and profile of the original. To get a good match, a piece of the original can be taken to one of the milling companies in the Ottawa area, and they can be asked to produce an accurate replica.
4. When there are trim elements from different periods in good condition, they should be retained as part of the evolution of the building.
5. All refastening of decorative woodwork should be done with galvanized nails or wood dowels. Holes should be predrilled for nailing, to avoid splitting old wood.



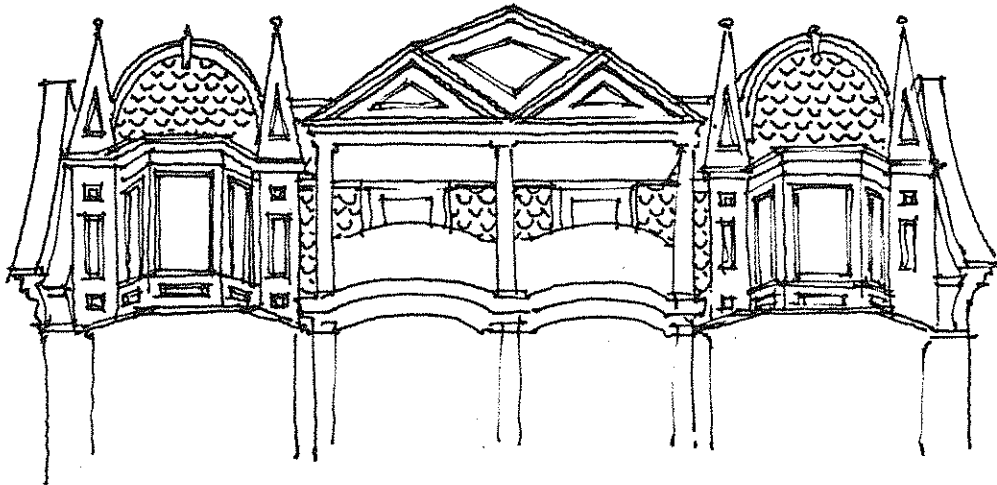
66-68 Stewart Street

#### VI.4.3.1.6 Maintenance and repair: Pressed metal elements

Decorative pressed metal elements became available in Ottawa in the 1890s, and were widely used through to the First World War. Their popularity declined after that. The earliest panels were of coated iron; later they were of coated and galvanized steel, with zinc for more ornamental work. In the Sandy Hill West area, pressed metal occurs most frequently as decorative cornices on residential buildings. There are a few examples of pressed metal shingle roofs.

##### Recommendations:

1. Examples of pressed metal work should be retained wherever possible. It is extremely expensive to duplicate original patterns in metal, and substitute materials such as fiberglass do not last well in exterior environments.
2. Cornices and other exposed elements should be inspected to ensure that water is not getting behind the metal and causing rusting of the metal as well as deterioration of the wood or metal supports.
3. All metal surfaces should be repainted regularly, with appropriate metal primers and paints.
4. Replacement parts should be fabricated to the same profile, and with the same material. Contact between dissimilar metals must be avoided.



46-48 Daly Avenue

#### VI.4.3.1.7 Maintenance and repair: Wood shingle roofing

Wood shingle roofing would have been common throughout the Sandy Hill West area from the time of the first settlement. The most common material would have been eastern white cedar, although a variety of other woods were used. As soon as sawmills were operating, sawn shingles were used as opposed to hand split shakes. The use of split shakes is historically inappropriate in an urban setting such as this.

##### Recommendations:

1. Evidence of earlier wood shingle roofing should be documented and used as a reference if reroofing of an older property is required.
2. Eastern white cedar shingles should be used when possible rather than western red cedar, to maintain more historical accuracy. The detailing should be traditional, with 4 1/2" to 5" exposure, cedar ridge boards, and *tilting fillets* at the gable eaves.
3. Coating of the shingles with coloured or clear stains was a common practice at the turn-of-the-century, and may be appropriate in a restoration project.
4. Valley flashing and other metalwork should be kept to a minimum when working with older properties, within the bounds of good practice. Closed valleys can be considered when underlain with a self-sealing membrane.

*Tilting Fillet or Cant Strip:*  
A board placed under the first row of shingles on the wall or roof of a house to give it a flare.



90 Daly / St. Paul's Eastern United Church



#### VI.4.3.1.8 Maintenance and repair: Metal roofing

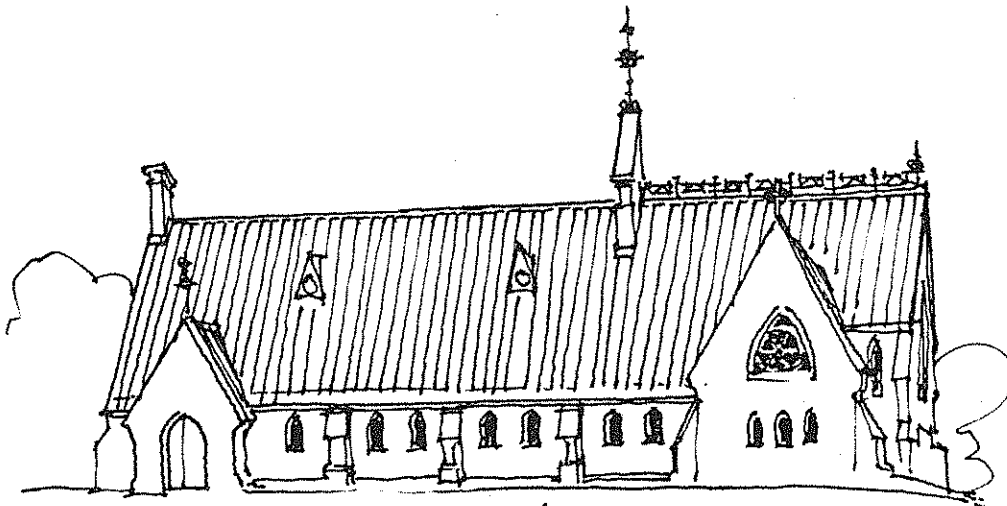
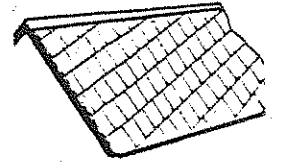
Some examples of sheet metal roofing survive in the Sandy Hill West area. Early tin-plate and *terne-plate* roofing was common in Quebec and Montreal and available in Ottawa. This was often applied in shingles laid on a slight diagonal, called *tôle à la canadienne*. Copper roofing was used on institutional properties. By the turn of the century, various steel corrugated and flat roofing sheets and shingles were available. More recent metal roofing materials are available in anodized or baked on finishes in a range of colours.

**Tern-Plate:**  
Steel plated, used for roofing, coated with an alloy containing up to 10% tin the remainder being lead.

*tôle à la canadienne:*

#### Recommendations:

1. Metal roofing should be conserved through a regular program of maintenance and repainting with appropriate metal paints.
2. For repainting, the metal surface should be cleaned and primed before the final paint finish is applied. The cleaning should occur immediately prior to priming, to ensure that rusting does not occur while the bare metal is exposed.
3. If repair is required, the same material should be used to avoid the decay caused by dissimilar metals in contact, or appropriate measures used for isolation.
4. For replacement, the traditional detailing should be maintained, or an earlier detail should be used if the documentation exists. If the roof was of standing seam, this detail should be maintained rather than introducing alternatives such as a batten roof or a corrugated sheet. The traditional *tôle à canadienne* can be reproduced using a contemporary substitute which uses a larger shingle folded to give the appearance of the smaller originals.



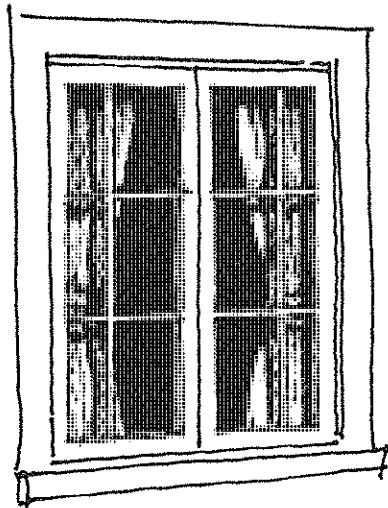
125 Daly Avenue / St. Alban's

#### VI.4.3.1.9 Maintenance and repair: Windows

Early buildings were fitted with multipaned windows. The sash was built to accommodate the available glass sizes, which were standard. In the Sandy Hill West area, both casement and double hung sash were common. By the late nineteenth century, larger glass sizes were available, and two over two and one over one windows became common. Store windows became single sheets of heavy plate glass, often with smaller paned transoms above. In the twentieth century, glass was simply cut to any size desired. A number of buildings in the area from the mid-twentieth century exhibit the use of glass block as a decorative feature.

##### Recommendations:

1. Surviving examples of early windows in the Sandy Hill West area are becoming increasingly rare and should be retained and repaired wherever possible.
2. Increased energy efficiency can often be achieved by adding interior or exterior storms, as opposed to full replacement sash which involves destroying the original.
3. If replacement windows are required, every effort should be made to ensure accurate reproduction of size, shape and profile. Examples of an existing sash can be taken to a millwork shop for duplication if necessary.
4. Replication of multi-paned windows should be done with single glazing in order not to introduce an oversize muntin bar. A separate storm panel can be fitted to the inside or outside of the sash if required.



#### VI.4.3.1.10 Maintenance and repair: Doors

Early doors were generally of four or six panel design, although simple plank construction was used in more modest locations. Front doors might have raised *bolection mouldings* and *fielded panels*. Only towards the end of the century was glass introduced into the door itself, where previously it had been limited to transoms and sidelights. High style Victorian doors at the end of the nineteenth century had elaborate applied mouldings and decorative features, and often incorporated bevelled or etched glass. Early twentieth century doors became simpler. All varieties of doors, including some good examples from the Art Deco period, exist within the Sandy Hill West area.

*Bolection moulding:*

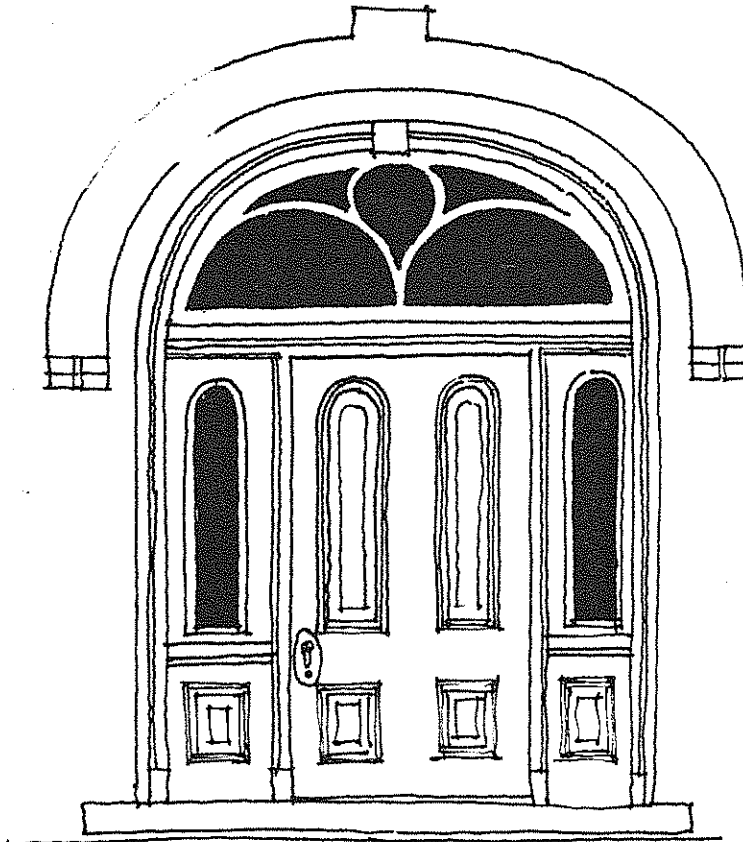
A panel moulding raised above the framing and rebated round it, often planted round door panels.

*Fielded panel:*

Raised panel.

#### Recommendations:

1. As with windows, good surviving examples of early doors are increasingly rare, and should be protected and repaired wherever possible. Qualified contractors for this kind of work should be sought out.
2. If repair or replacement is required, material that is accurate in shape, dimension, and profile should be obtained from local millwork shops.
3. The original finish and colours on doors should be investigated and restored when appropriate.



155-157 Wilbrod Street

### VI.4.3.2 Restoration

Restoration is the recovery of the earlier form, material and detailing of a heritage resource. It has to be undertaken with care, in order not to end up confusing or falsifying the historical record. Many buildings in the Sandy Hill West area have important features that are hidden under later modifications, or that have deteriorated beyond recall. In these cases, restoration can enhance both the building itself and its immediate streetscape.

#### **Recommendations:**

1. Restoration must be based on accurate historical documentation. This documentation may be a combination of photographs, drawings, written reports and oral accounts, as well as the physical evidence itself.
2. When restoring early and mid-nineteenth century buildings, it is important to remember that these are still essentially hand-built structures at the tail end of medieval traditions of building. Accurate duplication is never possible; it is therefore very important to keep as much original material in place as is at all possible, and to concentrate on stabilizing and consolidating the various components of the property.
3. When restoring late nineteenth and turn-of-the-century buildings, there is a great deal more machine-made material involved. In this case, accurate duplication may not be as difficult; in many cases, the actual methods of production have changed very little.
4. When restoring twentieth century buildings, there is the problem of composite materials, which become increasingly popular. These materials may be the result of patented or specialized processes which no longer exist. The problem of substitute materials and methods becomes a much more prominent concern, as well as the use of more sophisticated analysis and repair techniques.
5. Restoration of individual buildings in Sandy Hill West should respect the evolution that each building and the area as a whole has undergone. The contribution of various periods to the form of the building should always be recognized.
6. A restoration project should always consider a building in relation to its setting, whether natural or man-made, and its interior.

### VI.4.3.3 Rehabilitation and Adaptive Re-use

Rehabilitation is the modification of a heritage building to contemporary functional standards. It may involve some restoration, but also introduces new forms and materials to accommodate new requirements. Sensitive rehabilitation achieves these functional goals while protecting and even enhancing the qualities that give the property its unique heritage character.

Adaptive reuse involves a change in use or occupancy, often from an original residential or mixed use to a retail or other commercial use. Adaptive reuse will be limited in the Sandy Hill West area, because most of the buildings will retain their original residential or institutional uses.

#### **Recommendations:**

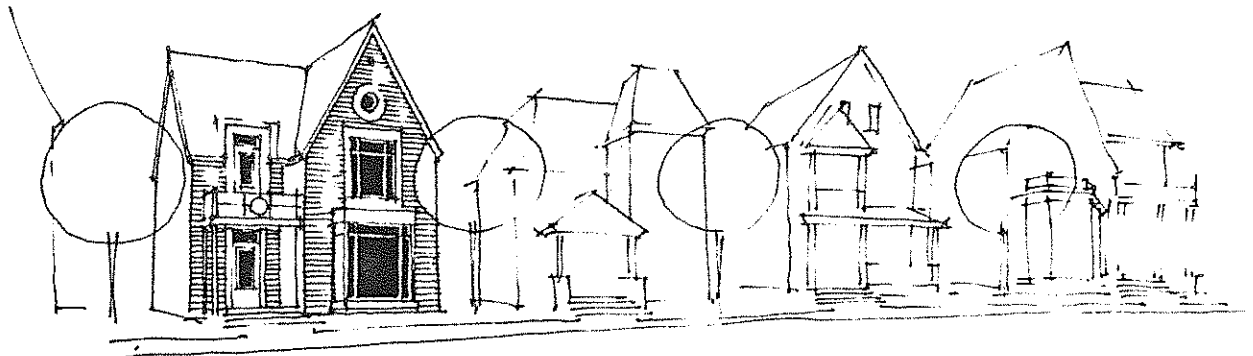
1. As with restoration, good rehabilitation is based on good historical documentation.
2. If new or modified uses are proposed, it should first be determined whether these can be contained within the heritage property without undue damage to the layout or finishes.
3. Structural upgrading, if required, should respect the original design intentions and reinforce these rather than working independently of them.
4. New mechanical and electrical services and other environmental control systems including insulation should be introduced in ways which do not upset the existing equilibrium or create new possibilities for deterioration.
5. Architectural layout and finishes should be respected, both inside and outside a property.
6. Distinctive features and good examples of design and craftsmanship should be retained. The contribution of all periods to the history of the property should be respected.
7. New work should be of its own time, but subservient to the heritage character of the existing property. It should take its form and direction from the history of the property itself.

#### VI.4.3.4 Infill: Residential

Infill in residential areas includes both additions to existing properties and new developments on vacant land. The infill process has been going on in Sandy Hill West since the nineteenth century, in response to demands for housing near the downtown core.

##### Recommendations:

1. The infill in areas zoned residential or mixed use must respect the heritage character of the area as a whole as well as the more immediate environment.
2. In the case of replacement, the new work should duplicate the mass, lot location and scale of the existing, as set out in HR (Heritage Residential) zoning requirements. For new work on vacant sites, the front and side yard setback should be the same as for adjacent heritage properties.
3. Small lot development should be encouraged, rather than large scale redevelopment through land assembly.
4. The building form should respect the massing of adjacent heritage properties. For row housing and other low-scale, multi-unit developments, the side gable and flat roof forms have traditionally been used in the area and continue to be the most viable options. For single units, the front gable form is also appropriate. Materials and detailing should respect and reinforce the character of what exists, without direct imitation. Brick veneer, wood trim, strong cornice lines, carefully proportioned window and door placement, and porches and balconies are all elements that can be incorporated in new construction.
5. Some mixed use developments have occurred in the area, and can be considered on a case by case basis.

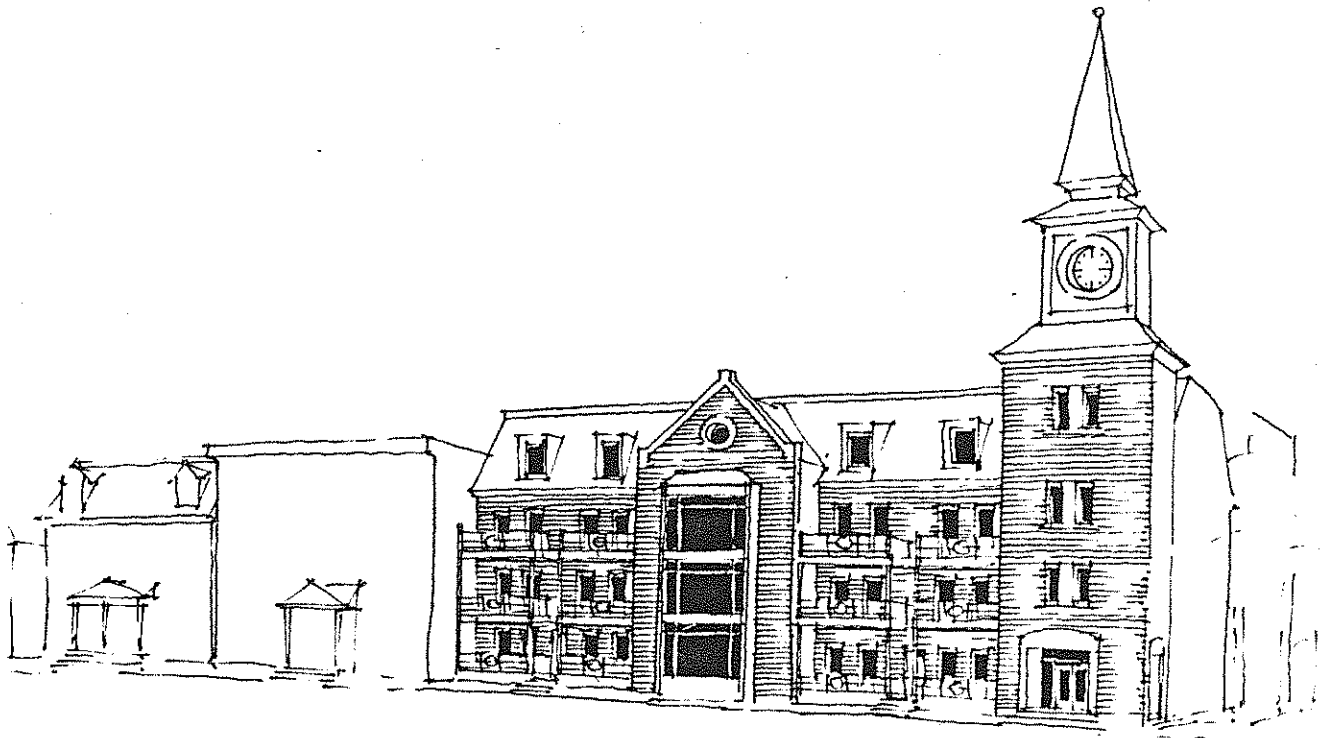


#### VI.4.3.5 Infill: Institutional

Institutional buildings have provided a complementary use to the predominantly residential character of the neighbourhood. The presence of significant institutional properties goes back to the earliest days of Sandy Hill West. On-going institutional presence is to be expected, and may result in infill projects.

##### Recommendations:

1. Institutional uses should be encouraged which continue the religious, educational and cultural patterns established in the nineteenth century.
2. Infill projects for institutional use should be compatible in physical form and in functional character with the neighbouring residential properties.
3. In physical form, they may be of a somewhat larger scale, but with compatible massing, materials and detailing to provide continuity. Their front yard setbacks should be the same as for neighbouring heritage properties, with allowance for appropriate plantings and other streetscape improvements. Institutional infill projects on corner lots may have small spires or other markers to provide a point of reference.
4. In functional character, they should respect the need of adjacent residential properties for privacy, quiet, and unencumbered pedestrian and vehicular movement. Parking areas in new projects should be screened.



## VI.5 Streetscape and Green Space Improvement Guidelines

### VI.5.1 Framework

This section provides an overview of the intent and principles for future management and development of the streetscapes and green spaces of Sandy Hill West.

#### VI.5.1.1 Conservation and Design Intent

With regard to the area's streetscapes and open spaces, the overall intent is to contribute to continuity of the district's sense of place by:

- conserving what remains of Sandy Hill West's streetscape and green space heritage,
- encouraging new development that compliments the heritage qualities of the area, and creates a comfortable, safe and pleasant street environment.

#### VI.5.1.2 Principles

The following principles provide a framework for the proposed guidelines for management and development of the streetscapes and green spaces of the district:

1. The existing and historic character and form of the streetscapes and green spaces of the district are integrally related to the area's street patterns, the relationship of buildings to front lot lines, and the height, form and mass of adjacent buildings. To conserve the overall character of the streetscapes and green spaces, the street and building patterns must be recognized and conserved.
2. Sandy Hill West has historically been a place of change and evolution in the streetscape. Change has often taken place in response to safety requirements of pedestrians and vehicular users, and in response to changes in technology. Rather than restoring the streetscapes to a previous style from the past, development should reflect continuing evolution and innovation in response to safety and technology.
3. Changes and innovation within the streetscapes of Sandy Hill West have historically taken place incrementally, from street to street and block to block. This has allowed the neighbourhood to reflect an accumulation of elements and innovations, providing visible continuity with the past. This pattern and pace of change should be respected.
4. Residential, institutional and commercial land use and building forms are part of the history of this district and are presently found within the neighbourhood. Streetscape details such as planting, signage, fencing and paving have varied, often reflecting a response to adjacent land use. This pattern of variety in the details of the area's streetscapes should be recognized.
5. Streetscape elements, especially canopy trees, have traditionally provided a strong sense of visual continuity within Sandy Hill West. There is a need for greater continuity within the streetscapes in order to unify the visual character of the district and to respect the historic character of the streetscapes.
6. Until recent decades, this neighbourhood had more direct physical links to Lowertown, Sandy Hill and the Ottawa University campus through continuity in traffic patterns and pedestrian routes. Visually, it was more strongly linked to Sandy Hill through a canopy of street trees. Physical and visual connections to these adjacent neighbourhoods should be enhanced.
7. Although the streetscapes have been places of change, the green spaces have remained relatively constant. The size, location, simplicity and tranquility of the few green spaces in this district are major characteristics to be conserved.



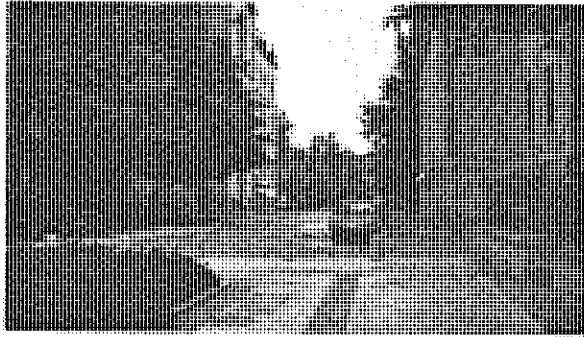
### VI.5.1.3 **Extent of Area to which Guidelines are Intended**

The guidelines provided here are intended to apply to the area included within the boundaries of Besserer, King Edward, Laurier and Waller. Although parts of this area may not be included in the proposed heritage district, much of the streetscape is within public ownership. Because of this, the City has the opportunity to influence management and development of some aspects of the streetscape in a manner which will conserve the heritage character of this district, whether or not there is a designated heritage district.

## VI.5.2 Design Guidelines

This section provides guidelines for management and development of specific elements which influence the streetscapes and green spaces of Sandy Hill West.

### VI.5.2.1 Block Pattern

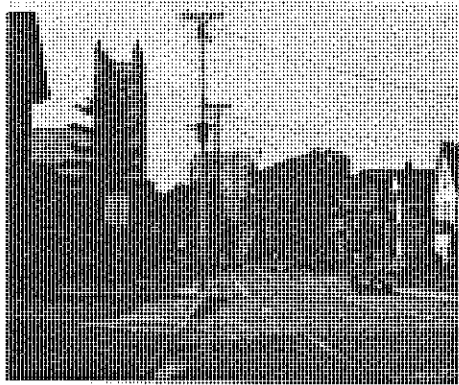


The pattern of streets and the blocks that they delineate are the oldest streetscape elements still existing in Sandy Hill West. The later introduction of Ring Street and the closing of Wilbrod and Daly at Waller Avenue, while modifying this pattern, do not threaten its integrity.

#### **Recommendations:**

1. In general, all development within Sandy Hill West should respect the integrity of the existing block pattern which is the underlying element responsible for the nineteenth century .
2. The City of Ottawa should encourage the University of Ottawa to maintain the integrity of Wilbrod Street as either a vehicular or pedestrian thoroughfare. Location of buildings or barriers which disrupt the existing street pattern should be discouraged.

## VI.5.2.2 Scale of Development

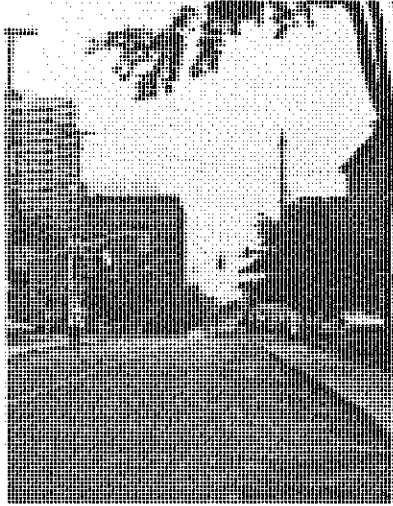


Historically the scale of development in Sandy Hill West was that of a nineteenth century residential neighbourhood, despite commercial uses that have been present in the neighbourhood since its settlement. Until the 1960s and 70s, the only exceptions to the overall residential scale have been the institutional buildings like Tabaret Hall and the various churches.

### **Recommendations:**

1. New development should respect the hierarchy of scale that was established within Sandy Hill West shortly after the turn of the century. This hierarchy begins with Tabaret Hall as the most imposing building in the district, followed by the smaller but still imposing churches, and followed finally by the residential and commercial buildings.
2. It should be recognized that development of any modern building within or adjacent to the district that would be larger than Tabaret Hall in either size or prominence would significantly change the character of the neighbourhood.
3. Any new residential development within Sandy Hill West should be in keeping with the traditional scale of residential development in the district. This recommendation would specifically exclude highrise development within the district, but would not exclude other forms of multi-unit housing depending on the specific design.
4. New commercial development should also conform to the existing scale of residential development, as has been the tradition within Sandy Hill West.

### VI.5.2.3 Vehicular Traffic Within the Area

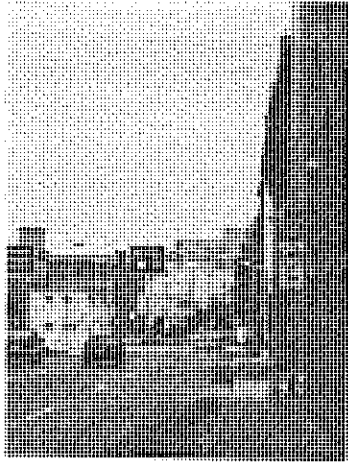


Although surrounded by streets with large volumes of vehicles which are often travelling at high speeds, the interior streets of Sandy Hill West are quieter and comfortably accommodate pedestrians, cyclists and local traffic. Stewart Street lost some of its residential character when converted to a one-way route with restricted on-street parking. Illegal truck traffic and restricted parking on Cumberland Street has also changed its former residential character.

#### **Recommendations:**

1. All streets within the interior of Sandy Hill West should be maintained as local residential thoroughfares.
2. Consideration should be given to increasing on-street parking or introducing traffic-calming techniques on Stewart Street in order to slow vehicular traffic and re-establish a more residential character on this street.
3. Police surveillance should be increased during the day time to ticket trucks illegally using Cumberland as a through-route.
4. Consideration should be given to accommodating additional parking along Cumberland Street to slow car speeds and discourage truck traffic. An exception to this would be along the west side of Cumberland Street in front of Tabaret Hall, where no parking should be permitted in order not to detract from the prominence of Tabaret Hall and the adjacent green space.

#### VI.5.2.4 Pedestrian Access to Sandy Hill West

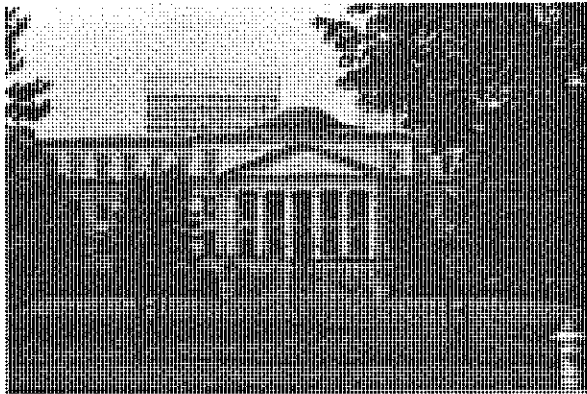


The streets immediately surrounding Sandy Hill West (Laurier East, Waller, Besserer and King Edward) are designed to facilitate vehicular movement, to the detriment of pedestrians either walking along or attempting to cross these streets. This serves to isolate the district from nearby areas with which it was traditionally connected: Rideau Street, the Byward Market, and the rest of Sandy Hill to the east and south.

##### **Recommendations:**

1. With changes slated for the Rideau Bus Mall there may be opportunities to look again at the traffic pattern of Waller and Besserer Streets to give more prominence to pedestrian comfort and safety, and less prominence to vehicular movement.
2. The planned redevelopment of the north end of King Edward Avenue may also offer impetus to upgrading the pedestrian environment along the section of King Edward that borders Sandy Hill West.
3. Consideration should be given to synchronizing stoplights on streets surrounding Sandy Hill West (King Edward, Besserer and Waller Streets particularly) to prevent vehicular traffic from building up speed along these routes.
4. Traffic signal timing at intersections should be adjusted to permit safer and more pedestrian-friendly crossings. Intersections that particularly need attention are Daly at Waller, and Besserer at Waller.
5. Providing a pedestrian crossing on the south side of Besserer at Waller that continues pedestrian desire lines from Besserer Street to the Rideau Centre entrance would strengthen the pedestrian connection between the Rideau Centre and Sandy Hill West.
6. Consideration should be given to providing a pedestrian crossing of Laurier Avenue East where it intersects Ring Street to reinforce the pedestrian connection between the University of Ottawa and Sandy Hill West.
7. Canopy trees planted along both sides of Laurier Street East where it borders Sandy Hill West would further enhance the pedestrian experience of this street, while serving to link the north end of the University of Ottawa to the main part of the campus.

#### VI.5.2.5 Publicly Accessible Green Space

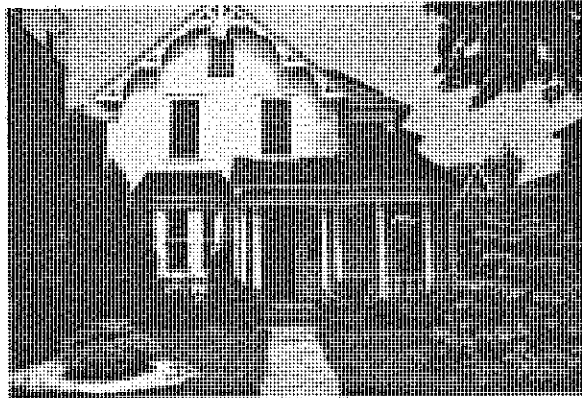


As a neighbourhood without public parks, the institutionally-owned green spaces which are accessible to the public assume greater importance as amenity space than would otherwise be the case. The historic publicly accessible green spaces within Sandy Hill West include the campus green in front of Tabaret Hall, the side yard of the Church of St. Alban the Martyr, and the green space at the southwest corner of Daly and King Edward. The green space on Laurier Avenue West adjacent to St. Joseph's Parish Hall is a more recently created green space which reflects the pattern of institutionally-owned open space; it also contributes positively to the neighbourhood.

#### **Recommendations:**

1. The institutional owners of these green spaces should be made aware of their importance to the community and to the overall heritage character of Sandy Hill West.
2. The City of Ottawa should make their expertise available and consider granting available funds at the request of these institutions for the restoration and periodic refurbishment of those green spaces within the designated Heritage Conservation District.
3. The University of Ottawa should be encouraged to respect the historic nature of the green space in front of Tabaret Hall as a passive, tranquil place providing respite for residents and the University community. Significant alterations to the character of this space would result from proposals such as removing the perimeter fence or introducing walkways across the lawn.

### VI.5.2.6 Private Green Space

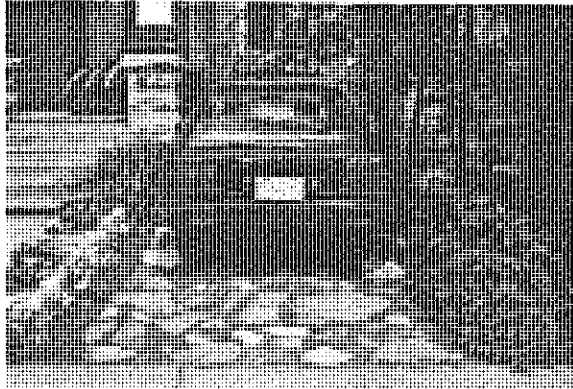


Large front lawns in front of single family homes and small multi-unit residential buildings are not common in this district. Many of those that exist are associated with buildings that have survived from the past. These private lawns provide welcome green space.

#### **Recommendations:**

1. The private owners of these green spaces should be made aware of their importance to the community and to the overall heritage character of Sandy Hill West.
2. The City of Ottawa should make their expertise available and consider granting available funds at the request of these residential property owners for the restoration and periodic refurbishment of these green spaces.
3. Any new construction on these properties should respect existing building setbacks in order to protect these green spaces.

### VI.5.2.7 Front Yard and Surface Parking



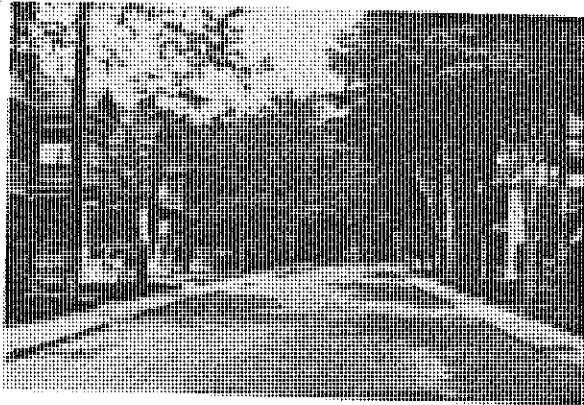
Front yard parking is a recently introduced phenomenon. Besides being unattractive, it disrupts the traditional relationship of buildings to adjacent streets. Large parking lots are without historic precedent and call attention to major destruction in the built fabric of the district.

**Recommendations:**

1. The City of Ottawa should restrict the development of any front yard parking in the future.
2. To encourage the removal of existing front yard parking, incentives should be offered such as restoration grants or granting year-round on-street parking privileges to those residents of Sandy Hill West who do not have access to rear yard parking space.
3. Front-yard parking which is to remain should be minimal in area and its impact reduced through the use of plant material or other screening techniques.
4. The City should require canopy tree planting along the street frontage of all surface parking lots to provide better continuity with adjacent streetscapes.
5. The City should require extensive eye level screening of all existing parking lots, including temporary lots, using appropriate shrubs, walls or fencing.



## VI.5.2.8 Street Trees



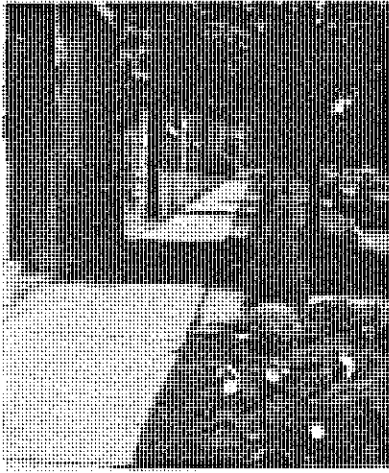
A canopy of street trees was a strong unifying element within the historic streetscapes of Sandy Hill West. In addition to unifying the district itself, this canopy created a visually discernible link to the rest of Sandy Hill, and helped to distinguish the district from the commercial character of Lowertown.

### Recommendations:

1. Efforts should be made to preserve all existing trees throughout Sandy Hill West.
2. An extensive street tree planting program should be carried out throughout the district. Trees should be planted on 8 to 15 metre centres, and be of a species that will provide overhead canopy along roadways as well as the sidewalks, and visibility of the ground floors of adjacent buildings.
3. Where possible, tree species should be the same on both sides of the street and along the length of a given block. Where there is no public boulevard, property owners should be encouraged to allow the City to plant street trees on their properties.
4. Appropriate tree species are ones which respect the history and character of the district and recognize the limitations of existing site conditions. Ideally, trees should be selected which develop into canopy trees, are environmentally hardy, and have green summer leaves. Species to be considered include sugar, red, silver and columnar maples, red oak, butternut, basswood, ash, Siberian elm and disease resistant varieties of American elm.
5. Metal tree guards should be considered for use throughout the district to provide protection for street trees. Tree guards should be functional in design and simple in detail.



### VI.5.2.9 Boulevard Planting

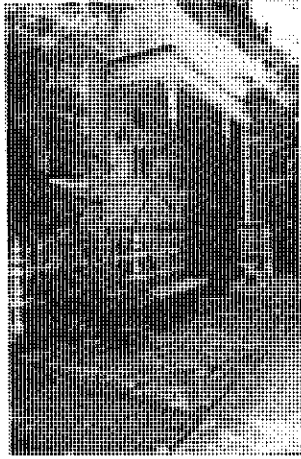


Grass is the traditional material for use in boulevards (the area between the curb and sidewalk). Some residents have introduced other plant material such as flowers and low shrubs in boulevard areas. This reflects the area's pattern of streetscape variety in response to adjacent land use.

#### **Recommendations:**

1. The use of grass in boulevards should be encouraged, although the use of low maintenance species should be considered.
2. Planting the boulevards with annuals and shrubs is compatible with the neighbourhood character as long as plant species are relatively low in height and plantings are subdominant to adjacent canopy trees.

## VI.5.2.10 Streetscape Ornamentation

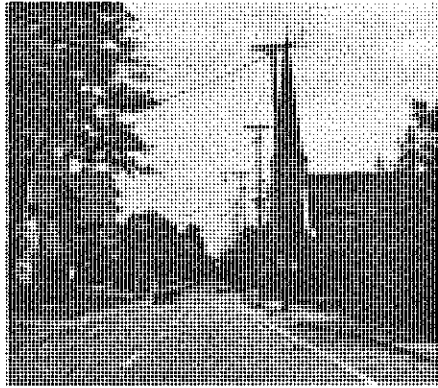


There is a great deal of ornamentation found in the front yards of commercial, residential and institutional properties. Annuals and perennials, flowering shrubs, movable planters, benches, statuary, ornamental fencing and flower planters attached to front facades are all part of the existing streetscape. Ornamentation of this type has been part of the fabric of the district since the turn of the century.

### **Recommendations:**

1. Encourage property owners to maintain and develop streetscape ornamentation, planters and plant beds as part of their front yards.
2. In new development, encourage streetscape ornamentation to be incorporated into front yards.

### VI.5.2.11 Overhead Utility Lines

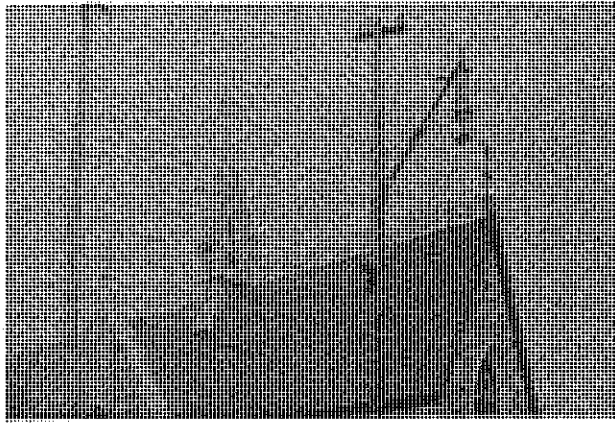


Overhead utility lines have been present in Sandy Hill West for over a hundred years. However, they severely restrict the location of street trees which are more important in creating a comfortable environment and perpetuating the historic character of the district. Burial of overhead utilities reflects the pattern of change in response to technological innovation.

**Recommendations:**

1. As a part of a street tree planting program, overhead lines should be relocated to underground conduits.
2. The relocation of utility lines should take place prior to street tree planting in order to minimize damage to new trees.
3. Where utility lines are buried, all utilities should be consolidated and located so as to optimize opportunities for tree root growth.

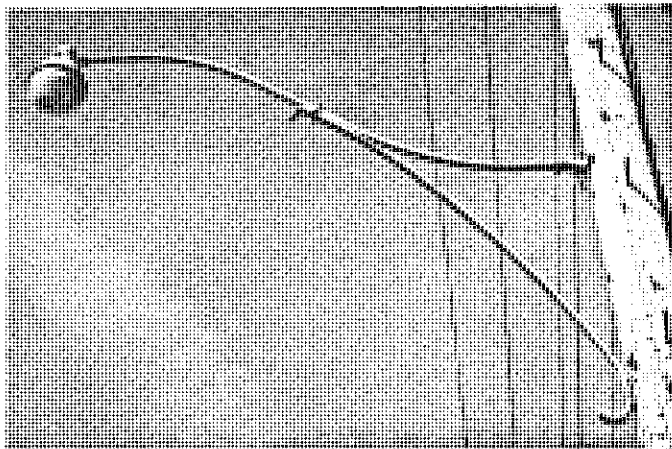
## VI.5.2.12 Street Lights



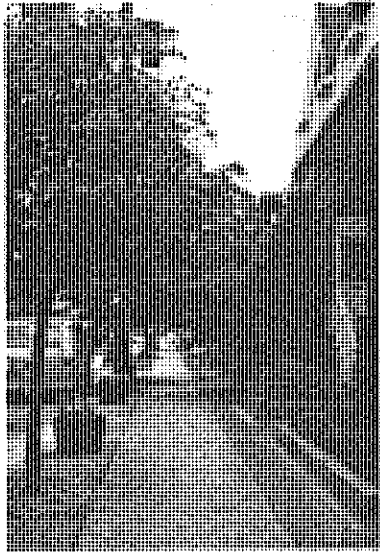
Street lights exist to enhance the public safety of streets and sidewalks. Their location and strength of illumination are determined by that primary function. The design of the light fixture itself contributes to the overall character of the street.

### Recommendations:

1. Lighting levels along every street should be consistent and of high enough intensity to illuminate roadways as well as to create a sense of security and safety for pedestrians.
2. As street lighting is updated throughout the district, consideration should be given to use of metal halide lighting, as the colour of its light is warmer and more friendly than high pressure sodium.
3. Illumination levels should not exceed that which is necessary in a residential neighbourhood.
4. Electric lighting fixtures extending out from wooden poles have been the style of light fixture in Sandy Hill West since late 1800s (Refer to photo above). The design of new street lights may be contemporary in character, but should also reflect the overall style traditionally used in the area. Street lights having an appropriate character are those presently located on Stewart Street (Refer to photo below). The earlier style (Refer to photo above) may be appropriate to reconstruct if adequate documentation exists and if the design meets contemporary in performance standards.
5. An acorn-shaped lamp fixture is more compatible with the heritage character of the area than is a cobra-head luminaire.



### VI.5.2.13 Sidewalk Design

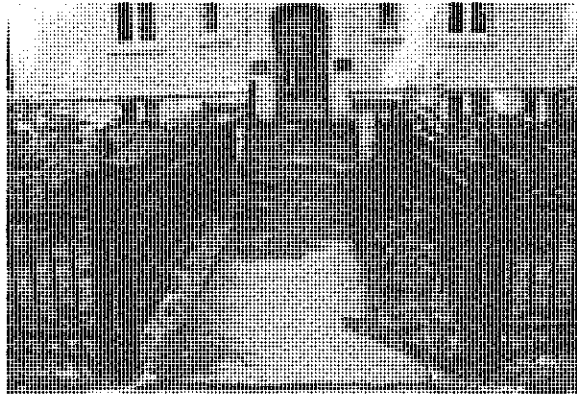


Sidewalk design within the district was standardized by the turn of the century with regard to width and construction materials. As with most other streetscape elements in Sandy Hill West, sidewalks were functional rather than decorative.

#### **Recommendations:**

1. Within the interior area of Sandy Hill West, the 1500 mm wide poured-in-place concrete sidewalk is the traditional pedestrian walkway. Concrete sidewalks of that dimension should be reinstated to replace all existing asphalt sidewalks.
2. Along streets on the periphery of Sandy Hill West, poured-in-place concrete is the traditional material for sidewalks. The width of these walkways may vary considerably depending on existing needs and conditions.
3. Scoring patterns on sidewalks should be simple and reflect City of Ottawa standards, (i.e. joints every 3600 mm and scoring every 1800 mm). More elaborate scoring patterns (e.g. running bond scoring every 600 mm) on wider sidewalks are not in keeping with the heritage character of the district.
4. To facilitate access for the disabled, all corners should provide depressed curbs and have textural change (e.g. using scoring and rough textured concrete).
5. Concrete unit pavers and clay brick are not part of the tradition of public walkway materials found in Sandy Hill West. If alternative pavements are used in boulevards and on entrance walks, these pavements should not interrupt the continuity of the main sidewalk.

#### VI.5.2.14 Fencing

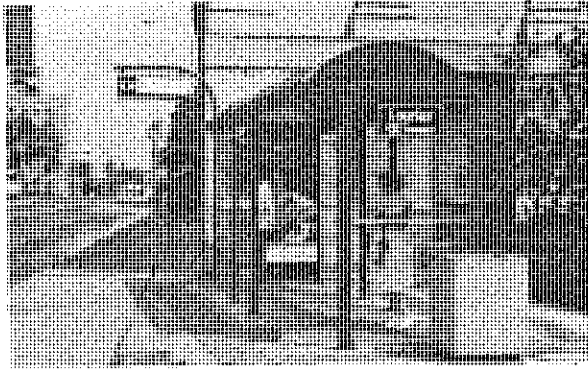


Front boundary fencing within Sandy Hill West has traditionally been of several types. Fencing associated with residential properties has typically been of either metal or wood construction. Adjacent to church and University properties, fencing has typically been more elaborate: ornamental wrought-iron fencing is an example. Both diagonal fencing and low pipe rail fencing were used on corner lots to control pedestrian movement. Fencing on side and rear property lines has more typically been of wood construction.

#### **Recommendations:**

1. Efforts should be made to conserve existing boundary fencing, especially that which is associated with the Tabaret Hall green space and the various church properties. More modest pipe rail fences should also be conserved, serving as reminders of the functional nature of fencing in this neighbourhood.
2. New boundary fencing for front and side yards is appropriate in association with residential and commercial properties. Suitable materials include wood pickets, wrought iron, pipe rail, and metal picket fencing. Wood pickets and wood board fencing are more appropriate for rear yard fencing.
3. New boundary fencing in association with institutional properties is appropriate. Suitable materials include metal pickets, wrought iron, and wrought iron in combination with stone walls.
4. Diagonal fencing on corner lots is appropriate.

## VI.5.2.15 Site Furnishings



Site furnishings include elements that contribute toward human comfort and convenience within the streetscape, such as benches, trash receptacles, bollards, planters, bicycle racks, as well as newspaper and post boxes. There is no clear historic precedent for street furnishings within this district.

### **Recommendations:**

1. In general, any site furnishing should be simple and highly functional, reflecting the character of sidewalks, fencing and other streetscape elements that are known to have existed within the neighbourhood.
2. Site furniture should be the same as any proposed or being used in the Sandy Hill Heritage Districts, east of King Edward, strengthening the connection between these two areas.
3. The City should provide (or encourage shopkeepers to provide) trash receptacles outside convenience stores and take-out food vendors, as well as at bus stops, in order to minimize littering in the district.
4. Co-ordination and grouping of site furniture on simple poured-in-place concrete pads would minimize clutter and confusion in the streetscape, especially at intersections.



#### VI.5.2.16 Storefront Signage

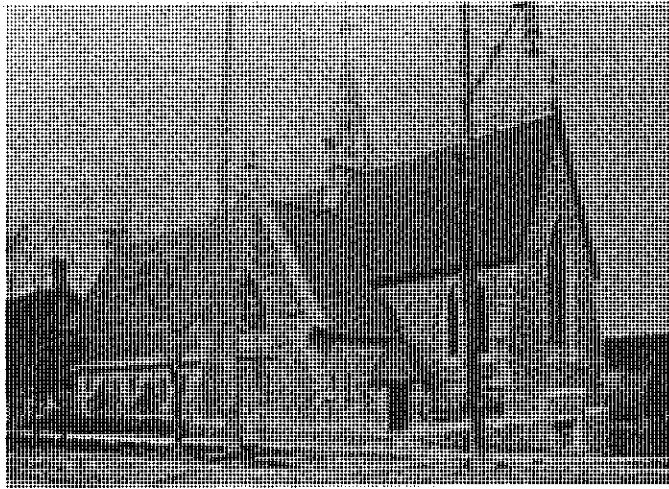


Commercial buildings have been a small but persistent presence in Sandy Hill West throughout its history. Commercial storefront signage presently varies widely as to style and range of material.

**Recommendations:**

1. Location of signage may vary: on awnings, painted on building facades, perpendicular to building faces, or located above store windows.
2. Materials may vary, but in all cases should complement the character of the building facade.
3. Where documentary evidence or markings on the facade indicate the style or location of former signage, such evidence should be used as a basis for the development of new signage.
4. The lighting of commercial signage in Sandy Hill West should be discreet, recognizing the primarily residential character of the neighbourhood.

## VI.5.2.17 Façade and Area Lighting



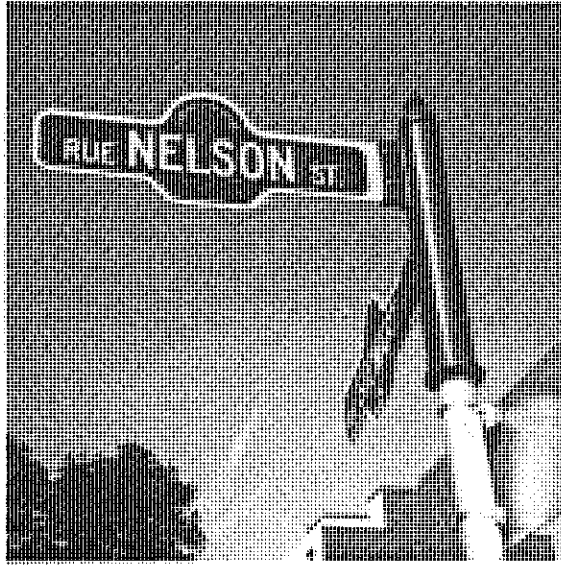
Façade lighting is not traditional to Sandy Hill West with its simple, functional streetscape character. Site area lighting did not exist until large areas of the district were converted to surface parking.

### Recommendations:

1. Façade lighting of institutional buildings should be modest in scope. Care should be taken to ensure that façade lighting does not dominate the streetscape at night, or spill over onto residential properties.
2. Façade lighting of commercial buildings should be modest to prevent dominating the adjacent streetscape.
3. Lighting of commercial storefront signage should take into account existing building and streetscape character. Neon lighting or incandescent lights placed in front of signs are generally appropriate forms of lighting commercial signs. Signs which are backlit should be discouraged or modest in size.
4. Care should be taken with the lighting of parking lots and other open spaces to ensure that the intensity of illumination is high enough to provide for user safety without dominating the streetscape and without spilling over onto adjoining properties.
5. Historic research has not revealed any precedents for area lighting within the study area. An appropriate light fixture for area lighting might be based upon the style of overhead street light found in the area at the turn of the century.



## VI.5.2.18 Street Signs

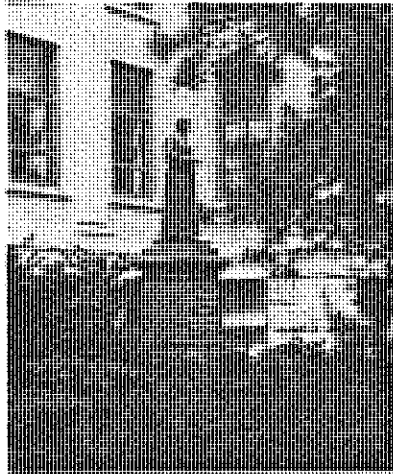


The City of Ottawa has a tradition of developing distinctive street signs which contribute to creating a sense of identity within a district.

**Recommendation:**

1. In order to emphasize Sandy Hill West's connection to the rest of Sandy Hill, consideration should be given to extending the use of the existing Sandy Hill street signage system into Sandy Hill West.

## VI.5.2.19 Art in the Landscape



The statue of Father Tabaret provides an historic precedent for art within an institutional landscape of the neighbourhood. A modern day counterpart is that of the wooden figures in the public landscape associated with the fire station on Laurier Avenue East.

### **Recommendation:**

1. All institutional and public properties in Sandy Hill West provide opportunities for the incorporation of art in the landscape. Suitable subjects, based on historic and contemporary precedents, might include references to people or events associated with the area's institutions and public facilities.





CITY OF OTTAWA  
DEPARTMENT OF PLANNING & DEVELOPMENT  
COMMUNITY PLANNING BRANCH

HERITAGE SURVEY  
AND  
EVALUATION FORM

BUILDING FILE NO.  
PD 43: STEWART 65  
HERITAGE DISTRICT FILE NO.  
PD 4302-5-1:

Municipal Address: 65 Stewart St.  
Building Name: JARVIS HOUSE  
Legal Description: P6 L7 Stewart S  
Date of Construction: 1886  
Original Use: residential, single  
Present Use: residential, multiple  
Present Zoning: CAN-X #4\*  
Planning Area: Sandy Hill West

Lot: 7 Block: (FIP 141)50/21 Plan: 6  
Additions: 1981  
Original Owner: Samuel Jason Jarvis  
Present Owner: Romeo Lafleche

PHASE ONE SURVEY

Potential Significance	Considerable	Some	Limited	None
	(Pre- 1880	( 1880 to 1920	( 1920 to 1950)	( 1950 to present)
History				
(Date of Construction)	3	2	1	0
Architecture	3	2	1	0
Environment	3	2	1	0
(Landmark or Design compatibility)				
Phase One Survey Score		/9		Prepared By:
Potential Heritage Building		Yes/No		
Potential Heritage District		Yes/No		

PHASE TWO EVALUATION RESULTS  
(Summarized from Page 4)

Category 1 2 3 4

Part V Definite Yes/No  
Part IV Potential Yes/No

If PART IV, By-law/Date:  
324-81

If PART V:

HERITAGE DISTRICT NAME:  
Sandy Hill West

BY-LAW/DATE:

COMMENTS:



PHOTO DATE: May 1992  
VIEW: N  
SOURCE: E. Beevey  
NEGATIVE NUMBER: CAO

**HISTORY**

PREPARED BY: M. Carter

DATE: May 1992

**Date of Construction:** 1886

\* Factual/Estimated

**Sources:** First listing City of Ottawa Directory (Toronto: Night Directory Co. 1886), p.79 confirmed by bracketting 1878 and 1901 fire insurance plans.**Trends:****Area links, late nineteenth century**

This house was originally built as a residence for photographer Samuel Jason Jarvis. In the years immediately following its construction, Jarvis also built two investment properties on the same block of Stewart on the opposite side of the street. One was a triple row house, #88-94 Stewart (1889-90), and the other was #98 Stewart, a double residence with an accompanying store (1889).

**Construction industry in this area, turn of the century**

In 1889 an article in the Ottawa Journal records that brickwork for the building Jarvis built at 98 Stewart St. was by F. McCullough, carpentry by S. Davis. Since this building was constructed only three years earlier, it is probable the same craftsmen were employed.

**Association with religious institution**

In ~~the~~ 1893, Jarvis sold this house to the Eastern Methodist Church for use as the church manse. It continued to be used for this purpose until the mid 1920s (about 1926) when the Eastern Methodist Church closed its doors.

**Area links, turn of the century**

This building was not adjacent to the Eastern Methodist Church as were the manses of most other churches in the area. It is, therefore, not part of a complex. It was, nevertheless, linked to the church in use and occupancy patterns.

**Subdivision, 1944**

This building was subdivided as a duplex in 1944. This is an example of the residential subdivision that was common in this neighbourhood in the 1920-60 period.

**Events:****Persons/Institutions: Samuel Jason Jarvis, photographer**

S. Jason Jarvis<sup>He</sup> the original owner and occupant of this house, ~~who~~ owned the Jarvis Photographic Studio on Sparks (later on Bank St), early in the 20th century. He was responsible for much of the evidence we have on Ottawa at the turn of the century.

**Eastern Methodist Church**

The Eastern Methodist Church was located on the corner of Besserer and King Edward St. on property that is now occupied by the Ottawa Little Theatre. While parts of the church were incorporated into the Ottawa Little Theatre building, little evidence of the Eastern Methodist Church remains on the church site today. Founded in 1873, the Eastern Methodist Church built its first church in Ottawa in this area and continued as a religious institution here until it was absorbed by the Presbyterian Church in 1925. Its congregation was a daughter church of the Dominion Methodist Church on Metcalfe St., founded to serve the Methodists of eastern Ottawa. It played an important role in establishing the tone and early functional orientation of this part of Sandy Hill. This residence, which was its manse, is the only substantive reminder of its existence. Indeed, the attractive polygonal verandah which distinguishes this residence, dates from the time it was a manse.

**Summary/Comments On Historical Significance:** An important surviving remnant of the Eastern Methodist Church which was once a major institution in this area. This house also has an early connection to Ottawa photographer S.J. Jarvis who had an important influence on the development of this block of Stewart St. in that he built three of its substantial buildings.

**Historical Sources (Coded):** Ottawa Planning Department, File #PD04-OHD4300. Askwith Map1915 (NMC 17888), FIPs 1878, 1901, 1912 1922, 1948, 1956. NCC. "Ottawa and Environs". 1975, OPD Heritage Map 1979, and PB818, 1990. OPD Building Permits #13344, 6 October 1944; #13737, 8 May 1953. Night Directory Co. City of Ottawa Directories, 1886 p.79, 1902 p.125, 1912 p.151-2, 1922 p.180-81, 1956 p.291-92, 1975 p.493, 1990 p.550. City of Ottawa. Assessment Roll of St. George's Ward (Ottawa:1895, 1911). Ottawa Journal, 22 June 1889. Heritage Ottawa. Walking Tour of Sandy Hill (c1982), p.57. Eastern Methodist Church, Ottawa, Souvenir Golden Jubilee, 1873-1923.

**ARCHITECTURE**

PREPARED BY: J. Smith

DATE: July 1992

**Architectural Design** (Plan, Storeys, Roof, Windows, Materials, Details, Etc.): 2 1/2-storey front gable residence with a variety of additions, wood frame, cove siding, decorative bargeboard, shouldered window trim, projecting bay, porch with octagonal bay.

**Architectural Style:** Queen Anne vernacular

**Designer/Builder/Architect:****Architectural Integrity (Alterations):**

**Original building:** .- 2 storey wood house with large 2 storey rear addition and small one storey rear addition. Bay window on front facade. Two small porches, east and south facades.

**1901-1912-** polygonal verandah added. Larger rear verandah, on west side making dimensions of back portion of house symmetrical.

**1944-** duplexed. Rear addition built to two storeys across entire rear of house. Small entrance porch at rear. Front porch may have been demolished. Garage on lot.

**1953 -** one storey addition. (This does not show on plan. It is likely that this is a permit for earlier rear addition).

**Present:** Original house has evolved significantly with later additions and detailing, on-going interest in decorative detailing.

Other (Structure, Interior, Building Type, Etc.):

Summary/Comments On Architectural Significance: Eclectic 19th century residence with extensive decorative wood detailing.

ENVIRONMENT

PREPARED BY: J. Smith

DATE: July 1992

Planning Area: Sandy Hill West

Heritage Conservation District Name: Sandy Hill West



PHOTO DATE: May 1992  
VIEW: NW  
SOURCE: K. Deevey  
NEGATIVE NUMBER: 120

Compatibility With Heritage Environs: Compatible heritage character of streetscape. provides variety in terms of massing, set back, and materials.

Community Context/Landmark Status: Notable residence because of its style and setting.

Summary/Comments On Environmental Significance: Significant contributing element to heritage character of the street.



PHASE TWO EVALUATION

CRITERIA SCORING

HISTORY CATEGORY	E	G	F	P	SCORE
1. Date of Construction		1			23
2. Trends			1		25
3. Persons/Institutions		1			7
HISTORY TOTAL	0	30	25	0	55

ARCHITECTURAL CATEGORY	E	G	F	P	SCORE
1. Design		1			20
2. Style		1			25
3. Designer/Builder				1	0
4. Architectural Integrity			1		15
ARCHITECTURAL TOTAL	0	45	15	0	60

ENVIRONMENT CATEGORY	E	G	F	P	SCORE
1. Design Compatibility		1			33
2. Landmark /			1		9
3. Community Context			1		13
ENVIRONMENT TOTAL	0	33	22	0	55

\*Date of Construction in Sandy Hill area.

Excellent ( Before 1880 ), Good ( 1880 to 1920 ), Fair ( 1920 to 1950 ), Poor ( After 1950 )

Criteria Scoring completed by: EVALUATION COMMITTEE

Date: Sept. 1992

DETERMINATION OF THE PHASE TWO TOTAL SCORE

CATEGORY SCORE	IN A POTENTIAL HERITAGE DISTRICT	NOT IN A POTENTIAL HERITAGE DISTRICT
History	55 x 45% = 25	X 40% =
Architecture	60 x 25% = 15	X 40% =
Environment	55 x 30% = 17	X 20% =
PHASE TWO TOTAL SCORE	57 /100	/100

HERITAGE CLASSIFICATION FOR THE SANDY HILL WEST AREA

Phase Two Total Score

Group	E	G	F	P
Group	0	2	0	0

PHASE TWO EVALUATION SUMMARY: 65 Stewart

## JARVIS HOUSE, 65 STEWART STREET

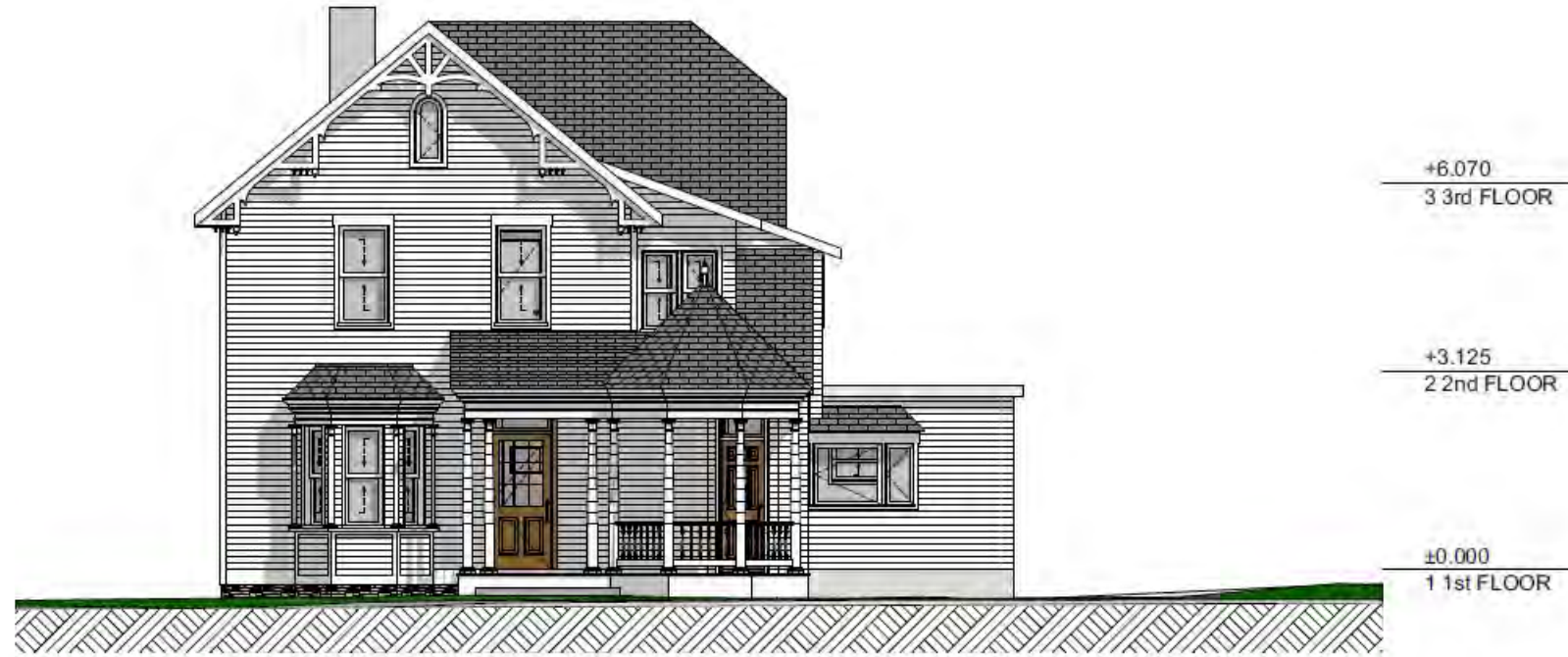
Bylaw 324-81

Plaque 1988

### Statement of Cultural Heritage Value- Jarvis House, 65 Stewart Street

The property at 65 Stewart Street is recommended for designation as being of architectural interest. Built in 1885, this Eclectic Victorian residence is set back from the street with a surrounding yard of suburban character. In overall appearance, a two storey clapboard frame house with gable end facing the street, it features a Picturesque bargeboard, Italianate window framing and a ground floor bay window. A delicate Beaux Arts polygonal verandah was added between 1901-1912. The original owner was Samuel Jarvis of Pittaway and Jarvis, Photographers.





2 SOUTH ELEVATION - EXISTING  
SD03 SCALE: 1:100



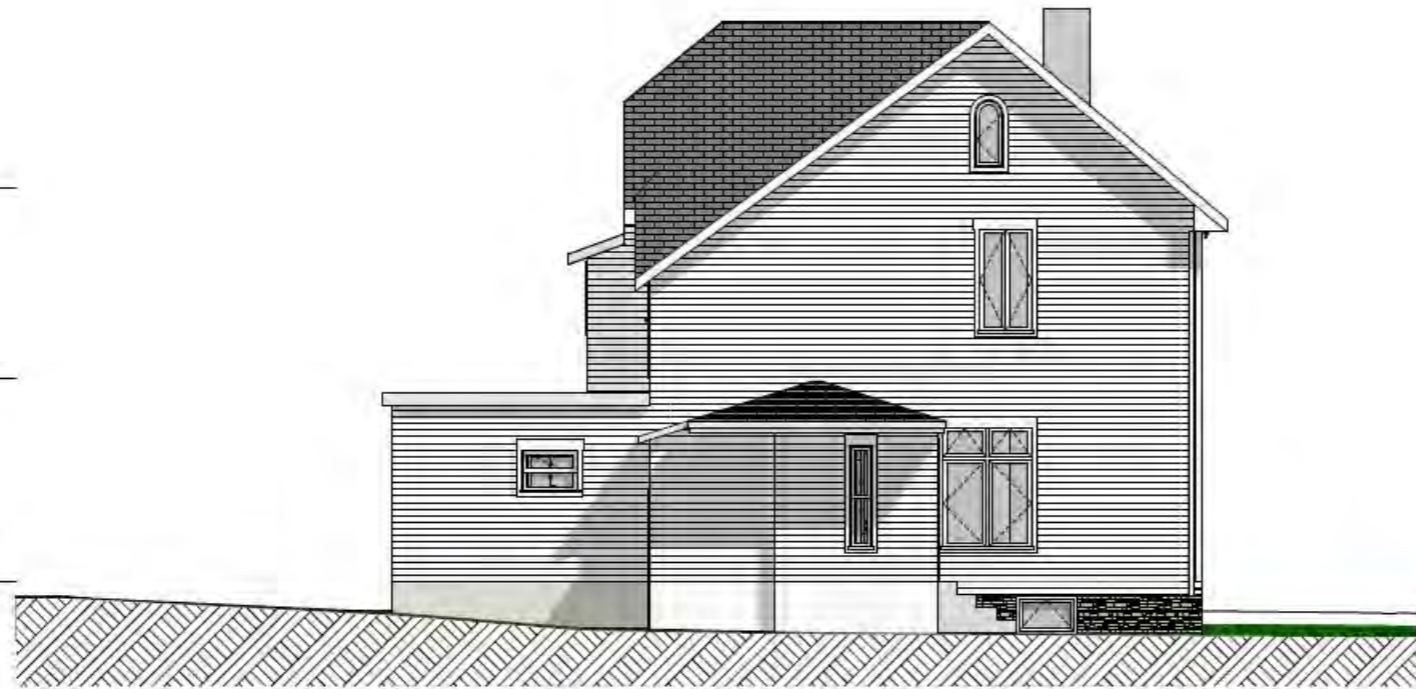
1 WEST ELEVATION - EXISTING  
SD03 SCALE: 1:100

SOUTH AND WEST ELEVATIONS - EXISTING  
JARVIS HOUSE

+6.070  
3 3rd FLOOR

+3.125  
2 2nd FLOOR

±0.000  
1 1st FLOOR

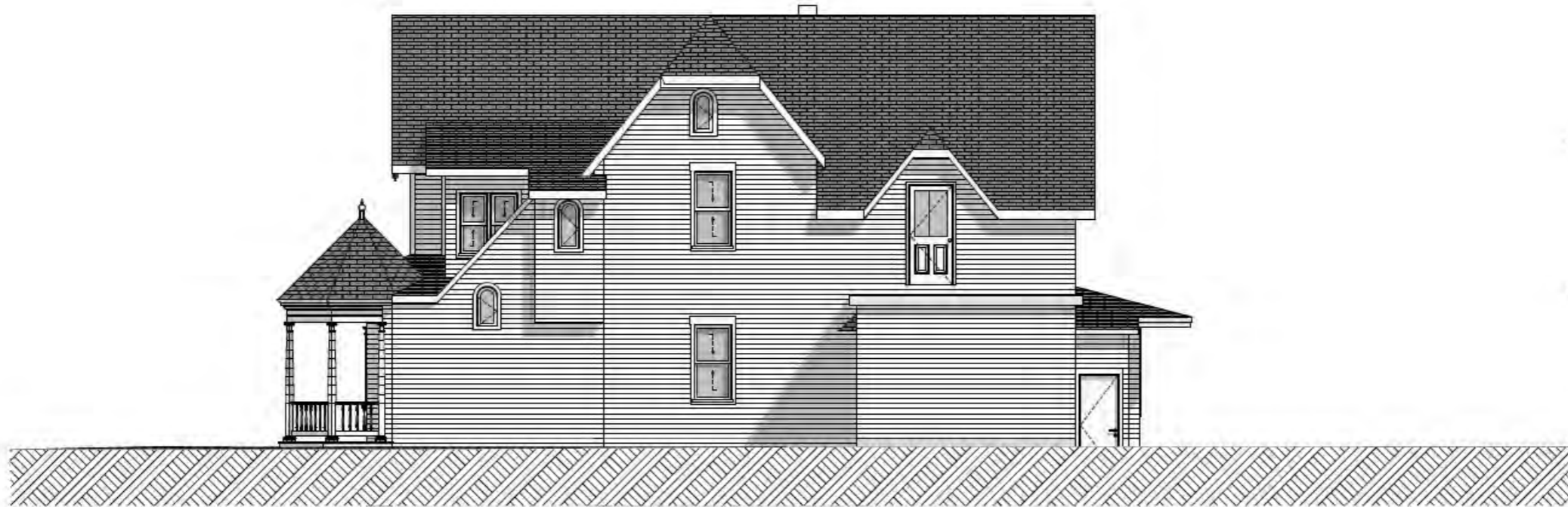


1 NORTH ELEVATION - EXISTING  
SD04 SCALE: 1:100

+6.070  
3 3rd FLOOR

+3.125  
2 2nd FLOOR

±0.000  
1 1st FLOOR



2 EAST ELEVATION - EXISTING  
SD04 SCALE: 1:100